



MURRAY
CITY COUNCIL

Council Meeting February 16, 2021



Murray City Municipal Council

Notice of Meeting

Murray City Center
5025 South State Street, Murray, Utah 84107

Electronic Meeting Only **February 16, 2021**

Public Notice is hereby given that this meeting will occur electronically without an anchor location in accordance with Utah Code 52-4-207(4), due to infectious disease COVID-19 Novel Coronavirus. The Council Chair has determined that conducting a meeting with an anchor location presents substantial risk to the health and safety of those who may be present at the anchor location because physical distancing measures may be difficult to maintain in the Murray City Council Chambers. (See attached Council Chair determination.)

The public may view the meeting via the live stream at www.murraycitylive.com or <https://www.facebook.com/Murraycityutah/>.

***Citizen comments or public hearing comments may be made as follows:**

- Live through the Zoom meeting process. Those wishing to speak during these portions of the meeting must send a request to city.council@murray.utah.gov by 3:00 p.m. on the meeting date. You will receive a confirmation email with instructions and a Zoom link to join the meeting.
- Read into the record by sending an email in advance or during the meeting to city.council@murray.utah.gov.
- Comments are limited to less than three minutes, include your name and contact information.

Meeting Agenda

5:15 p.m. Committee of the Whole

Diane Turner conducting.

Approval of Minutes

Committee of the Whole – January 19, 2021

Discussion Items

1. Fiscal Year 2019-2020 Budget Amendment – Brenda Moore (15 minutes)
2. Open and Public Meeting Act Training – G.L. Critchfield (15 minutes)
3. Harassment Training – G.L. Critchfield (15 minutes)
4. Legislative Updates – Kat Martinez (15 minutes)

Announcements

Adjournment

Break

6:30 p.m. Council Meeting

Rosalba Dominguez conducting.

Opening Ceremonies

Call to Order

Pledge of Allegiance

Approval of Minutes

Council Meeting – January 19, 2021

Special Recognition

1. Murray City Council Employee of the Month, Laura Lloyd, Executive Secretary – Brett Hales and Jon Harris presenting.

Citizen Comments

*See instructions above. Email to city.council@murray.utah.gov . Comments are limited to less than 3 minutes, include your name and contact information.

Consent Agenda

None scheduled.

Public Hearings

Staff and sponsor presentations, and public comment prior to Council action on the following matters.

1. Consider an ordinance related to land use; amends the General Plan to include a Small Area Plan for the Fashion Place West Area. – Melinda Greenwood and Jared Hall presenting.
2. Consider an ordinance enacting Section 15.20.145 of the Murray City Municipal Code relating to the establishment of public electric vehicle charging stations and rates and amending Section 10.08.020 of the Murray City Municipal Code relating to the regulation of parking at an electric vehicle charging station. – Blaine Haacke and Matt Youngs presenting.

Business Item

1. Consider an ordinance amending Sections 13.08.110 and 13.08.120 of the Murray City Municipal Code relating to city water user's responsibilities for service pipe repairs, and to allow the Mayor to develop guidelines for adjustments to high water bills due to water user waste. – Danny Astill and Cory Wells presenting.

Mayor's Report and Questions

Adjournment

NOTICE

Supporting materials are available for inspection on the Murray City website at www.murray.utah.gov.

Special accommodations for the hearing or visually impaired will be made upon a request to the office of the Murray City Recorder (801-264-2663). We would appreciate notification two working days prior to the meeting. TTY is Relay Utah at #711.

On Friday, February 12, at 10:00 a.m., a copy of the foregoing notice was posted in conspicuous view in the front foyer of the Murray City Center, Murray, Utah. Copies of this notice were provided for the news media in the Office of the City Recorder. A copy of this notice was posted on Murray City's internet website www.murray.utah.gov and the state noticing website at <http://pmn.utah.gov>.

A handwritten signature in black ink that reads "Jennifer Kennedy". The script is cursive and fluid, with the first name and last name clearly distinguishable.

Jennifer Kennedy
Council Executive Director
Murray City Municipal Council



MURRAY CITY CORPORATION
CITY COUNCIL

Kat Martinez, District 1

Dale M. Cox, District 2

Rosalba Dominguez, District 3

Diane Turner, District 4

Brett A. Hales, District 5

Janet M. Lopez
Council Executive Director

**Murray City Council Chair Determination
Open and Public Meeting Act
Utah State Code 52-4-207(4)
February 1, 2021**

In accordance with, Utah Code 52-4-207(4), due to infectious disease COVID-19 Novel Coronavirus, I have determined that meeting in an anchor location presents substantial risk to the health and safety of those who may be present at the anchor location because physical distancing measures may be difficult to maintain in the Murray City Council Chambers.

Federal, state and local leaders have all acknowledged the global pandemic. Salt Lake County Public Health Order 2020-15 dated October 26, 2020, recognizes that COVID-19 is a contagion that spreads from person to person and poses a continuing and immediate threat to the public health of Salt Lake County residents.

It is my intent to safeguard the lives of Murray residents, business owners, employees and elected officials by meeting remotely through electronic means without an anchor location.

The public may view the meeting via the live stream at www.murraycitylive.com or <https://www.facebook.com/Murraycityutah/>.

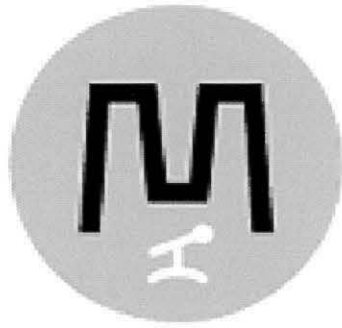
Citizen comments or public hearing comments may be made live through the Zoom meeting process or read into the record by sending an email to city.council@murray.utah.gov.

Diane Turner
Murray City Council Chair



MURRAY
CITY COUNCIL

Committee of the Whole



MURRAY
CITY COUNCIL

Committee of the Whole Minutes



MURRAY CITY MUNICIPAL COUNCIL COMMITTEE OF THE WHOLE

The Murray City Municipal Council met on Tuesday, January 19, 2021 for a meeting held electronically in accordance with the provisions of Utah Code 52-4-207(4), Open and Public Meeting Act, due to infectious disease COVID-19 Novel Coronavirus. Council Chair, Ms. Turner, determined that to protect the health and welfare of Murray citizens, an in-person City Council meeting, including attendance by the public and the City Council is not practical or prudent.

Council Members in Attendance:

Diane Turner – Chair	District #4
Brett Hales – Vice Chair	District #5
Kat Martinez	District #1
Dale Cox	District #2
Rosalba Dominguez	District #3

Others in Attendance:

Blair Camp	Mayor	Janet Lopez	City Council Director
Jennifer Heaps	Chief Communications Officer	Jennifer Kennedy	City Council Director
Brooke Smith	City Recorder	Pattie Johnson	City Council Office Admin.
Jared Hall	CED Division Supervisor	Danny Hansen	IT
G.L. Critchfield	City Attorney	Melinda Greenwood	CED Director
Bill Francis	The Imagination Company		

Ms. Turner called the meeting to order at 5:15 p.m.

Approval of Minutes – Ms. Turner asked for comments or a motion on the minutes from: Committee of the Whole – December 8, 2021. Mr. Hales moved approval. Mr. Cox seconded the motion. (Approved 5-0)

Discussion Items

2020 Moderate-Income Housing Report – Mr. Hall said the Moderate-Income Housing report was submitted to the State of Utah on December 1, 2020; and spoke about added requirements. He noted cities have always filed the report; however, changes were made last year. A rough outline of the report was given to highlight new required responses different from the past, which was due to the implementation of SB (Senate Bill) 34 that also changed the annual submission deadline to December 1.

He explained one change is that cities cannot have a Moderate-Income Housing plan separate from a GP (General Plan); and a GP must include the Moderate-Income Housing plan. Mr. Hall reviewed new

requirements that include having an updated projection of the City's 5-year affordable housing needs based on the following:

- Growth of households (demand)
- Housing stock (supply)
- Median housing costs
- Median household incomes

The report must also:

- Include findings of the Moderate-Income Housing element of the City's GP.
- Include the most recently adopted copy of the Moderate-Income Housing element of the GP.
- Be posted on the City's website.

Mr. Hall reviewed a *5-Year Projected Affordable Housing Needs* table to reflect affordable housing shortages in 2020 for three different moderate levels of median income in Salt Lake County, and Murray. An abundance of 140 units for the 80% moderate income level, and shortages of 2,500 units in two lower income categories were noted. He said compared to the nationwide challenge, Murray is doing well to have surplus in one category. He noted with seven existing strategies, Murray had already been in compliance for a number of years; however, due to the City being a transit City, two new strategies that meet affordable housing criteria were missing, which are now included to coincide with State Law.

A brief review of the existing strategies occurred. One City goal is to review all zoning ordinances and make modifications where necessary, to allow for various housing types, lot size, setbacks and other factors that limit the types of houses in a zone. He pointed out that SB-34 compliance affects whether cities attain transportation funds, so it is vital to provide a sufficient report to show that Murray is making a good faith effort to address modern income housing needs.

Ms. Greenwood said although the City is required to submit the annual Moderate Income Housing report, Murray is not required to perform. She agreed the City was further along in addressing the housing crisis than other communities; and confirmed legislators are more frequently bringing new requirements forward; many are anticipated this Legislative Session that effect how transportation and economic funds are dispersed. As a result, a pressure point is being applied to Murray and all transit cities. All in State Code, she expected the new requirements to become more stringent as the affordable housing crisis continues; so, she expected the situation to worsen.

Council Comments and Discussion

- Ms. Martinez asked how the City in its ability, could practically meet goals of the median income housing shortage.
- Mr. Hall replied existing strategies would provide that capability; for example, by allowing a variety of housing options like accessory dwellings that are more affordable than other types of housing. He believed unless densities are increased in certain areas, no impact could be made to the housing crisis in the current housing market. For example, near TRAX lines where the City is identified as a transit city with three TRAX stations.
- He felt Murray has the ability to place density where other cities cannot, which was the best way for the City to impact housing challenges; and if the City is going to impact affordability, we must recognize that increasing density and diversity must be done to show that Murray is doing all it can to address housing challenges. He noted Utah ranks 25 in top metro cities that lack affordability; and was placed in the top five for increased housing prices.

- Ms. Dominguez asked if the City was letting legislation dictate what should happen in Murray, and how the City should implement housing. She asked if the City could take the initiative by applying current Code to help developers, by incorporating NeighborWorks in with those relationships to help resolve housing challenges.
- Mr. Hall agreed another important relationship was with Rocky Mountain Housing. He said affordable housing should be implemented in areas where it can be utilized most effectively, with the least impact to other development patterns. He discussed the notion that single-family homes are extinct and pointed out that 90% of developed land in the valley is single-family homes; and 70% in Murray. He thought adding density to core areas was not going to cause the extinction of neighborhoods because it would only be added where able, and in leftover spaces.
- Ms. Turner studied the entire report and hoped to understand it more thoroughly; therefore, due to time restraints, she requested the conversation continue. She suggested a retreat for training about the GP process to ensure Council Members had specific clarity. All Council Members agreed.

General Plan and Zone Map Amendments 5283, 5157, 5217, & 5177 South and 151 East 5300 South –

Ms. Greenwood noted the subject property was located on the northeast corner of 5300 South and State Street; including Best Buy, Chick-fil-A, and Mimi's. She said property owners approached the City with a request to rezone the parcel to M-U (Mixed-Use).

Mr. Hall led the discussion and reported that Howland Partners own the 13.2-acre property; an aerial map was viewed to analyze current structures and the presence of a big box store. The current zone is C-D (Commercial Development) for most of the parcel; and the Future Land Use map suggests the property be categorized as General Commercial. Mr. Hall pointed out Professional Office categories across the street at the IMC (Intermountain Medical Center) campus; and a TRAX station to the west. He said it was not the best pedestrian environment, however, with the GP amendment, and proposed rezone to M-U, those challenges would be corrected. Good findings that support the amendments were noted as, proximity to State Street and the 5300 South corridor; proximity to TRAX, and it is also not far from the MCCD (Murray City Center District) where expansion of the downtown was expected.

Mr. Hall mentioned differences between the M-U zone, and the C-D zone as discussed in a previous Committee of the Whole, regarding height, parking, and setbacks. The significant difference being that the M-U allows higher density residential components; and in this case, he said the rezone would allow 80 units per acre. He said the Murray Planning Commission voted 7-0 to approve recommendation of approval to the Council based on several findings. One positive public comment was received about creating a unique walkable area, after 42 public notices were mailed out for the December 3, 2020 public hearing. He said staff offered the same recommendation of approval.

Council Comments and Discussion

- Mr. Hales asked if Best Buy was moving from the property.
- Ms. Turner asked if structures would be torn down.
- Ms. Greenwood was in contact with property owners for the last year, who conveyed the intent is to convert much of the existing commercial space into residential buildings; and add additional stories to existing structures. It is their belief that the future of retail is not sustainable without a residential component; so, it is thought that this location is perfect for being proactive in adding high density housing to sustain the retail located there.
- Ms. Dominguez asked if current commercial spaces on the property were leased; and if office space was fully occupied.

- Mr. Hall confirmed not all office space is utilized; there is a small parking structure on the site, but not large enough to handle a new development. Not every structure would be demolished because the complex is successful and active; adding a residential component is the overall desire.
- Ms. Dominguez asked the current height restriction for C-D. Mr. Hall confirmed 30 feet within 100 feet of a residential area; however, with no residential neighborhoods near the property, the component could be as much as 80 feet tall. Ms. Dominguez affirmed on 13 acres over 1,000 units would be possible on the parcel.
- Mr. Hall agreed residential housing could be over 800 units. He added that the area was identified on the GP for future study and consideration as a BRT (bus rapid transit) station village; once the bus transit was in place on State Street the corner was anticipated to be a station village area.
- Ms. Dominguez pointed out that the current city hall property is zoned as MCCD; she asked if it could also be rezoned to M-U in the future. Mr. Hall confirmed the six acres would be redeveloped once city hall is relocated.
- Ms. Dominguez thought parcels at 5300 South were well suited for an M-U development; but wondered about public services. Mr. Hall noted the Howland property sits outside the MCCD, where additional public services and facilities were planned for in a recent Sewer Master Plan public works study for capital improvements. Therefore, incorporating additional service units to areas on State Street were identified - and upgrades would be necessary as additional projects come about.
- Ms. Greenwood informed the Council that the application was received in September of 2020; however, it was not processed until after sewer capacity studies were completed to analyze new growth and the need for new infrastructure.
- Mr. Hall concluded staff believes these changes are appropriate, so they recommended approval to the planning commission, who had the same recommendation to the City Council.

Text Amendment for Residential Chicken Keeping – Ms. Greenwood said the Council requested the review about chicken keeping in late summer; since then, five years of history was researched. Mr. Hall discussed the text amendment that proposes chicken keeping now be allowed on residential properties.

(Attachment #1)

A timeline from 2012 to 2020 was presented to explain how reconsidering the ordinance came about. Mr. Hall explained it was after a public survey, and the number of responses that led to the proposal. New proposed standards were reviewed, such as maximum number of chickens allowed per lot size, and chicken coop requirements. Chicken keeping would only apply to single family detached dwellings and not townhomes or apartments. A comparison chart was shown that reflected how surrounding cities are allowing for chicken keeping. Average monthly code enforcement cases in cities were noted by population; and the survey was provided. Mr. Hall said staff moved forward with devising a draft ordinance due to good public response. The Murray Planning Commission recommended approval of the draft ordinance to the City Council with the addition of a requirement for those who want to keep chickens to register with the City.

Council Comments and Discussion

- Ms. Martinez visited neighbors who raise chickens; she reported many were unaware that chickens are not allowed in residential areas; and asked if there would be a fee associated with chicken keeping. She had concerns like smaller properties allowing up to six chickens and chickens not producing eggs for a certain time. She felt education about chicken keeping and egg laying cycles was important to provide good guidance. She asked why chicken coops are not allowed up against property lines and existing fencing.

- Mr. Hall replied there would be no fee, registration was only to obtain information; and if citizens do not voluntarily register with the City, it is not a violation. He was not familiar with egg production cycles; he said existing fences are viewed as accessory structures due to drainage issues onto neighboring properties, which is not allowed in City Code. All accessory structures must be one foot from a property line, and chicken coops are not large enough to require building permits.
- Ms. Greenwood affirmed that distancing chicken coops, and other animal structures like dog runs away from property lines was to help to avoid issues with neighbors and complies with building codes.
- Mr. Hales recalled concerns the Council had when considering chickens previously – related to issues like property lines and attracted pests like rats. He noted, after analyzing the recent survey results, there seemed to be a higher interest this time.
- Ms. Turner agreed a significant past concern was about ensuring proper space to house chickens. She supported chicken keeping and enjoyed hearing them in her neighborhood; she thought it was a good idea to recognize citizens who have them, to better regulate the practice.
- Mr. Hales asked Ms. Turner if chicken keeping was allowed in her condominium complex. Ms. Turner said no, but it was common in the surrounding area.
- Mr. Cox raised chickens in his youth and understood that pests like rats, skunks and racoons are drawn to chicken coops, due to chicken droppings and chicken feed. He shared more recently, due to neighboring chickens in his neighborhood that did not exist in previous years, he battled a costly situation with rats on his personal property. He explained the process to be rid of them, which was why he believed agricultural areas were best for chicken keeping. He shared Ms. Martinez's concerns about residents not understanding infrequent and little-to-no egg production cycles, related to the number of chickens allowed on small properties. He shared about a constituent who owns a pet chicken for an autistic child and felt this was a positive reason for having a chicken in a residential area, more so than raising chickens for fresh eggs, due to low egg productions. Mr. Cox was confident varmint challenges would occur more often with the new proposal; and stressed that with chickens comes this problem; for the record he wanted citizens to be aware of how costly it is to get rid of rats.

Announcements: Ms. Kennedy reminded the Council of Ms. Lopez's walk-through retirement reception on January 28, 2021 from 12-2 pm; masks and social distancing required.

Adjournment: 6:12 p.m.

**Pattie Johnson
Council Office Administrator II**

ATTACHMENT #1

Proposed Standards

Number of Chickens Allowed

Lot Size	Maximum Chickens Permitted
Less than 6,000 square foot lot	4
6,000 – 9,999 square foot lot	5
10,000 - 11,999 square foot lot	6
12,000 square foot lot or greater	8

Coop Standards

Standard	Requirement
Property line setback	5'
Adjacent property line setback	25'
Dwelling setback	10'
Coop height	7' maximum
Minimum area requirement	4 square feet per chicken



City	Number of Chickens	Permit Required?	Fee?
Cottonwood Heights	10	Yes	Yes
Draper	6	No	No
Herriman	1 – 10 based on lot size	No	No
Holladay	25 – 62 only on lots >10,000 square feet	Yes	No
North Salt Lake	6 – 30 based on lot size	No	No
Riverton	6, more allowed if lot is greater than ½ acre.	No	No
Sandy	Only in Agricultural Zone		
Salt Lake City	15	Yes	Yes
South Jordan	6	Yes	Yes
Taylorsville	2 – 10 based on lot size	Yes	No
West Jordan	5	Yes	Yes
West Valley City	Treated as pet up to 4 pets allowed	No	No
Midvale	2 – 8 based on lot size	Yes	Yes
Millcreek	Only in Agricultural Zone		
South Salt Lake	4 – 6 based on lot size	Yes	Yes
Salt Lake County	3 – 8 based on lot size	Yes	Yes



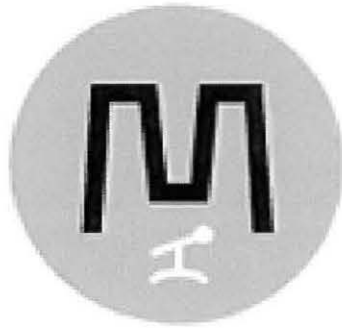
City	Setback for Coop	Area Per Chicken
Cottonwood Heights	40' from dwellings, 3' from property line	3 – 6 sq ft
Draper	50-75' from dwellings	N/A
Herriman	25' from all dwellings	N/A
Holladay	40' from dwellings and street	N/A
North Salt Lake	35' from dwellings, 5' from property line	N/A
Riverton	No standards found	N/A
Sandy	Only in Agricultural Zone	N/A
Salt Lake City	25' from adjacent dwelling	2 – 6 sq ft
South Jordan	40' from adjacent dwelling; 5' from property line; 10' from dwelling	N/A
Taylorsville	25' from adjacent dwelling; 3' from property line; 15' from dwelling	1.5 – 6 sq ft
West Jordan	20' from dwelling; 5' from property line	1.5 – 6 sq ft
West Valley City	No standards found	N/A
Midvale	30' from adjacent dwelling; 10' from dwelling	2.5 – 6 sq ft
Millcreek	Only in Agricultural Zone	N/A
South Salt Lake	50' from adjacent dwelling; 5' from property line; 25' from dwelling	N/A
Salt Lake County	40' from adjacent dwelling; 25' from dwelling	2 sq ft



Code Enforcement Cases

Municipality	2019 & 2020 Cases	Average Per Month	Population
West Valley City	77	1.6	136,401
Holladay City	3	0.06	30,697
Sandy City (not allowed)	10	0.21	96,901
South Jordan City	12	0.25	74,149
Taylorsville City	24	0.50	60,192
Midvale City	8	0.16	33,636
Millcreek City (not allowed)	28	0.59	61,270
South Salt Lake City	4	0.09	25,365
Ogden City	36	0.75	87,325





MURRAY
CITY COUNCIL

Discussion Item #1



MURRAY


Finance & Administration

FY 2020-2021 Budget Amendment

Council Action Request

Committee of the Whole

Meeting Date: February 16, 2021

Department Director Brenda Moore	Purpose of Proposal Amend the FY 2020-2021 budget
Phone # 801-264-2513	Action Requested Public hearing & consideration
Presenters Brenda Moore	Attachments Draft of the ordinance
	Budget Impact Budget Amendment
Required Time for Presentation 10 minutes	Description of this Item Requesting amendment of the FY2020-2021 budget for the following Grants and Donations: 1. Increase revenue and Victims Advocate emergency expenses for receipt of beard growing donations. \$1,000 2. Receive and allocate Utah Department of Health CARES grant \$3,600 for Kids Zone mobile room divider. 3. Receive and allocate an additional \$1,451 received from the EMS grant. 4. Receive and allocate an HHS provider relief grant \$11,787 for the purchase of a hands free CPR device. 5. Receive and allocate Create in Utah CARES grant \$5,000 for Arts programming
Is This Time Sensitive No	
Mayor's Approval 	
Date February 1, 2021	

Continued from Page 1:

In the General Fund, increase sales tax revenue budget by \$147,400 and appropriate the following expenditures:

1. Increase the City Council personnel budget \$36,900 for an employee payout, job overlap, and wage change.
2. Increase the IT small equipment budget \$100,000 for computer replacements.
3. Increase the Police travel and training budget \$7,000.
4. Increase the City Council professional services budget by \$2,500 for meeting broadcast service and miscellaneous expense budget by \$1,000 for parade taffy.

In the CIP Fund allocate from reserves the following expenditures:

1. Parks equipment \$40,000 for a truck.
2. Senior Recreation building for \$45,000 for HVAC system.
3. Police equipment \$292,000 for police cars and related equipment
4. Public Works \$25,000 for radar speed signs, \$20,000 to complete the transportation study and an additional \$200,000 to complete the Hanauer Street extension landscaping.
5. Transfer \$47,360 to the Golf Fund.

In the Golf Fund receive \$47,360 from the CIP Fund and allocate \$37,360 for two sand trap rakes, and \$10,000 for a golf cart lift. Also in the Golf Fund appropriate from reserves \$379,775 for the purchase of golf carts.

In the Risk Fund add \$100,000 in professional services for litigation expenses from reserves.

ORDINANCE NO.

AN ORDINANCE AMENDING THE CITY'S FISCAL YEAR 2020-2021 BUDGET

On June 16, 2020, the Murray City Municipal Council adopted the City's budget for Fiscal Year 2020-2021. It has been proposed that the Fiscal Year 2020-2021 budget be amended as follows:

1. Receive and appropriate the following grants and/or donations in the General Fund with no financial impact:
 - a. \$1,000 in donations from police officers beard growing donations for the victim advocates emergency funds, and;
 - b. \$3,600 Utah Department of Health CARES grant for a Kids Zone mobile room divider, and;
 - c. \$1,451 Utah Department of Health EMS grant for emergency medical equipment, and;
 - d. \$11,787 Health and Human Services provider relief grant for the purchase of a handsfree CPR device, and;
 - e. \$5,000 Create in Utah CARES grant for arts programming.
2. Receive and appropriate the following revenue & expenditures in the General Fund with no financial impact:
 - a. Receive \$147,400 from additional sales tax revenue, and;
 - b. Appropriate \$36,900 in the City Council personnel expenses, due to the council director personnel change, and;
 - c. Appropriate \$100,000 in the IT small equipment expenses, for computer replacements, and;
 - d. Appropriate \$7,000 in the Police travel and learning, for increased training needs, and;
 - e. Appropriate \$2,500 in the City Council professional services budget for public meeting broadcast services, and;
 - f. Appropriate \$1,000 in the City Council miscellaneous expense for Fun Days parade taffy.

3. Appropriate the following from Capital Improvement Projects Fund reserves:
 - a. \$40,000 for a truck for the Parks department, and;
 - b. \$45,000 for repair and replacement of the Senior Recreation center HVAC system, and;
 - c. \$292,000 for Police cars and related equipment, and;
 - d. \$25,000 for radar speed signs, and;
 - e. \$20,000 for professional services to complete the Cities transportation plan, and;
 - f. \$200,000 for the Hanauer street extension to complete landscaping and lighting, and;
 - g. \$47,360 transfer to the Golf Course Fund for equipment.
4. Receive and appropriate in the Golf Course Fund \$47,360 for 2 sand trap raking machines, and a golf cart lift.
5. Appropriate \$379,775 in the Golf Course Fund from reserves for Golf Carts.
6. Appropriate \$100,000 in the Risk Fund from reserves for professional services due to litigation expenses.
7. Authorize the Director of Finance and Administration to transfer any amount the transportation sales tax revenue is above budget to the Capital Projects fund at the close of the fiscal year 2020-2021 and adjust the budget accordingly.
8. Authorize the Director of Finance and Administration to transfer any amount from the General Fund to the Capital Projects fund at the close of fiscal year 2020-2021 any amount which exceeds the maximum fund balance as determined by Utah Code Ann section 10-6-116, and adjust the budget accordingly.

Section 10-6-128 of the Utah Code states that the budget for the City may be amended by the Murray City Municipal Council following a duly noticed public hearing. Pursuant to proper notice, the Murray City Municipal Council held a public hearing on March 2, 2021 to consider proposed amendments to the Fiscal Year 2020-2021 budget. After considering public comment, the Murray City Municipal Council wants to amend the Fiscal Year 2020-2021 budget.

Section 1. Enactment. The City's Fiscal Year 2020-2021 budget shall be amended as follows:

9. Receive and appropriate the following grants and/or donations in the General Fund with no financial impact:

- a. \$1,000 in donations from police officers beard growing donations for the victim advocates emergency funds, and;
- b. \$3,600 Utah Department of Health CARES grant for a Kids Zone mobile room divider, and;
- c. \$1,451 Utah Department of Health EMS grant for emergency medical equipment, and;
- d. \$11,787 Health and Human Services provider relief grant for the purchase of a handsfree CPR device, and;
- e. \$5,000 Create in Utah CARES grant for arts programming.

10. Receive and appropriate the following revenue & expenditures in the General Fund with no financial impact:

- a. Receive \$147,400 from additional sales tax revenue, and;
- b. Appropriate \$36,900 in the City Council personnel expenses, due to the council director personnel change, and;
- c. Appropriate \$100,000 in the IT small equipment expenses, for computer replacements, and;
- d. Appropriate \$7,000 in the Police travel and learning, for increased training needs, and;
- e. Appropriate \$2,500 in the City Council professional services budget for public meeting broadcast services, and;
- f. Appropriate \$1,000 in the City Council miscellaneous expense for Fun Days parade taffy.

11. Appropriate the following from Capital Improvement Projects Fund reserves:

- a. \$40,000 for a truck for the Parks department, and;
- b. \$45,000 for repair and replacement of the Senior Recreation center HVAC system, and;
- c. \$292,000 for Police cars and related equipment, and;

- d. \$25,000 for radar speed signs, and;
 - e. \$20,000 for professional services to complete the Cities transportation plan, and;
 - f. \$200,000 for the Hanauer street extension to complete landscaping and lighting, and;
 - g. \$47,360 transfer to the Golf Course Fund for equipment.
12. Receive and appropriate in the Golf Course Fund \$47,360 for 2 sand trap raking machines, and a golf cart lift.
13. Appropriate \$379,775 in the Golf Course Fund from reserves for Golf Carts.
14. Appropriate \$100,000 in the Risk Fund from reserves for professional services due to litigation expenses.
15. Authorize the Director of Finance and Administration to transfer any amount the transportation sales tax revenue is above budget to the Capital Projects fund at the close of the fiscal year 2020-2021 and adjust the budget accordingly.
16. Authorize the Director of Finance and Administration to transfer any amount from the General Fund to the Capital Projects fund at the close of fiscal year 2020-2021 any amount which exceeds the maximum fund balance as determined by Utah Code Ann section 10-6-116, and adjust the budget accordingly.

Section 2. Effective Date. This Ordinance shall take effect on first publication.

PASSED, APPROVED AND ADOPTED by the Murray City Municipal Council on this ____ day of _____, 2021.

MURRAY CITY MUNICIPAL COUNCIL

Diane Turner, Chair

ATTEST:

Brooke Smith, City Recorder

MAYOR'S ACTION: Approved

DATED this ____ day of _____, 2021.

D. Blair Camp, Mayor

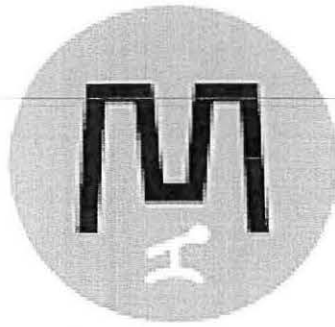
ATTEST:

Brooke Smith, City Recorder

CERTIFICATE OF PUBLICATION

I hereby certify that this Ordinance or a summary hereof was published according to law on the ____ day of _____, 2021.

Brooke Smith, City Recorder



MURRAY
CITY COUNCIL

Discussion Item #2



MURRAY

City Council

Open and Public Meeting Act Annual Training

Council Action Request

Committee of the Whole

Meeting Date: February 16, 2021

Department Director Jennifer Kennedy Phone # 801-264-2622 Presenters G.L. Critchfield, City Attorney Required Time for Presentation 10 Minutes Is This Time Sensitive Yes Mayor's Approval Date January 26, 2021	Purpose of Proposal The purpose is to review the Open and Public Meeting Act requirements as it applies to municipal government. Action Requested Informational only. Attachments Memo, summary of the Act, Open and Public Meetings Act, and an article related to elected officials and social media. Budget Impact None. Description of this Item This is an opportunity to review all aspects of the State of Utah Open and Public Meeting Act requirements as it applies to municipal government and elected officials. Much of the required criteria is fulfilled by Council staff and the City Recorder, however, elected officials have a responsibility to conduct business in an open and transparent manner and to abide by the spirit of the Open and Public Meeting Act.
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MEMORANDUM

TO: Murray City Council
CC: Mayor Blair Camp
FROM: G.L. Critchfield, City Attorney
DATE: January 26, 2021
RE: Open and Public Meetings Training

Section 52-4-104 of the Utah Open Public Meetings Act (the "Act") states that annual training should be provided to the City Council regarding the requirements of the Act. We would like to provide the mandated training for the City Council at its meeting on February 16, 2021. We anticipate that the training would not exceed ten (10) minutes. We are providing to you copies of the following:

- A summary of the Act.
- The Open Public Meetings Act - Title 52 Chapter 4 of the Utah Code.
- An article discussing elected officials and social media.

If you have any questions, please do not hesitate to contact us. We look forward to meeting with you on February 16th.

OPEN AND PUBLIC MEETINGS TRAINING

A. UTAH OPEN AND PUBLIC MEETINGS ACT

1. Attached is a copy of the Utah Open and Public Meetings Act (the "Act") Title 52, Chapter 4, Part 4 of the Utah Code.
2. Under Section 52-4-104 of the Act, "public bodies" covered by the Act must be provided annual training.
3. Stated Purpose of the Act:
 - a. You are to take your actions openly and deliberate openly.
 - b. You err on the side of public openness.
 - c. When in doubt, meetings are open.

B. ACT COVERS "MEETINGS"

1. A meeting means the convening of public body with quorum present.
2. Meetings include work sessions and site visits.
3. Meetings can be convened or conducted by means of a conference using electronic communications.
4. Meetings do not include chance or social meetings.
 - a. Meetings is a broad definition. The Act is intended to cover all gatherings of public bodies (Council and Boards). Chance or social meetings are construed narrowly and are not defined.
5. Separate discussion for public hearings.

C. NOTICE

1. Regular meeting schedule.
2. For each meeting you must give at least 24-hour notice of the meeting date, time, location and agenda. (Special notice requirements for public hearings.)
3. The notice must be posted in the City's principal offices, provided to at least one newspaper of general circulation within the City or local correspondent and published on the Utah Public Notice Website. (Minimum Notice)
4. There are specific and narrow rules to call an emergency meeting with best notice possible.

D. AGENDA

It is not enough to give notice. The agenda that is required for each public meeting must also provide enough detail to notify the public as to the topics to be discussed and the decisions that may be made. You should not discuss matters that have not been specified on the agenda. At the discretion of the Chair, matters brought up by the public in a meeting may be discussed but no action may be taken on the matter.

E. CLOSED MEETINGS

1. Meetings are open unless they are closed for only the reasons provided in the Act.
2. Before you close a meeting, you must follow the notice requirements, which includes an agenda identifying the subject matter and that the Council may take action to close the meeting. During the open meeting, the reason or reasons for holding a closed meeting need to be discussed. A vote by name of each member of the public body voting against or for the closed meeting must also be in the open meeting.
3. In order to close a meeting a quorum must be present and two-thirds of the members must vote for a closed meeting.

4. There needs to be a record of the closed portion of the meeting and detailed written minutes may be kept that disclose the contents of the closed portion of the meeting. The minutes are non-public records.
5. A closed meeting may be held only for the following reasons:
 - a. Discussion of the character, professional competence, or physical or mental health of an individual. This does not mean simply personnel matters.
 - b. Strategy sessions to discuss pending or reasonably imminent litigation. This does not mean just litigation.
 - c. Strategy sessions to discuss the purchase, exchange, or lease of real property.
 - d. Strategy sessions to discuss the sale of real property.
 - e. Discussion regarding the deployment of security personnel, devices, or systems.
 - f. Investigative proceedings regarding allegations of criminal misconduct.
6. If there is a closed meeting on same day as a regularly-scheduled meeting, the meeting must be held at the same location as the regularly-scheduled meeting with limited exception.

F. MINUTES

1. Written minutes must be kept of all meetings including closed meetings.
2. Minutes are public and are available to the public within a reasonable time after the meeting. Draft minutes are public when they are made available to members of the public body.
3. All open meetings must also be recorded. The public has the right to the record of the meeting.

4. Closed meetings are also recorded.
5. Limited exception to recording closed meeting:
 - a. Meetings in which the competence or physical or mental health of an individual is discussed or deployment of security devices.
 - b. An affidavit affirming the purpose is needed.
6. Tapes of Open meetings are public.

G. PENALTIES

It is a Class B Misdemeanor to knowingly and intentionally violate the Act.

H. ELECTRONIC COMMUNICATIONS THAT SUBVERT TRANSPARENCY AND ACCOUNTABILITY.

1. The challenge is to identify where civic social networks and other electronic communications such as e-mails, texting, tweets, blog posts and Facebook subvert the Open meetings laws.
2. Favor formality.

Chapter 4 Open and Public Meetings Act

Part 1 General Provisions

52-4-101 Title.

This chapter is known as the "Open and Public Meetings Act."

Enacted by Chapter 14, 2006 General Session

52-4-102 Declaration of public policy.

- (1) The Legislature finds and declares that the state, its agencies and political subdivisions, exist to aid in the conduct of the people's business.
- (2) It is the intent of the Legislature that the state, its agencies, and its political subdivisions:
 - (a) take their actions openly; and
 - (b) conduct their deliberations openly.

Renumbered and Amended by Chapter 14, 2006 General Session

52-4-103 Definitions.

As used in this chapter:

- (1) "Anchor location" means the physical location from which:
 - (a) an electronic meeting originates; or
 - (b) the participants are connected.
- (2) "Capitol hill complex" means the grounds and buildings within the area bounded by 300 North Street, Columbus Street, 500 North Street, and East Capitol Boulevard in Salt Lake City.
- (3)
 - (a) "Convening" means the calling together of a public body by a person authorized to do so for the express purpose of discussing or acting upon a subject over which that public body has jurisdiction or advisory power.
 - (b) "Convening" does not include the initiation of a routine conversation between members of a board of trustees of a large public transit district if the members involved in the conversation do not, during the conversation, take a tentative or final vote on the matter that is the subject of the conversation.
- (4) "Electronic meeting" means a public meeting convened or conducted by means of a conference using electronic communications.
- (5) "Electronic message" means a communication transmitted electronically, including:
 - (a) electronic mail;
 - (b) instant messaging;
 - (c) electronic chat;
 - (d) text messaging, as that term is defined in Section 76-4-401; or
 - (e) any other method that conveys a message or facilitates communication electronically.
- (6)
 - (a) "Meeting" means the convening of a public body or a specified body, with a quorum present, including a workshop or an executive session, whether in person or by means of electronic communications, for the purpose of discussing, receiving comments from the public about, or

acting upon a matter over which the public body or specific body has jurisdiction or advisory power.

- (b) "Meeting" does not mean:
 - (i) a chance gathering or social gathering;
 - (ii) a convening of the State Tax Commission to consider a confidential tax matter in accordance with Section 59-1-405; or
 - (iii) a convening of a three-member board of trustees of a large public transit district as defined in Section 17B-2a-802 if:
 - (A) the board members do not, during the conversation, take a tentative or final vote on the matter that is the subject of the conversation; or
 - (B) the conversation pertains only to day-to-day management and operation of the public transit district.
- (c) "Meeting" does not mean the convening of a public body that has both legislative and executive responsibilities if:
 - (i) no public funds are appropriated for expenditure during the time the public body is convened; and
 - (ii) the public body is convened solely for the discussion or implementation of administrative or operational matters:
 - (A) for which no formal action by the public body is required; or
 - (B) that would not come before the public body for discussion or action.
- (7) "Monitor" means to hear or observe, live, by audio or video equipment, all of the public statements of each member of the public body who is participating in a meeting.
- (8) "Participate" means the ability to communicate with all of the members of a public body, either verbally or electronically, so that each member of the public body can hear or observe the communication.
- (9)
 - (a) "Public body" means:
 - (i) any administrative, advisory, executive, or legislative body of the state or its political subdivisions that:
 - (A) is created by the Utah Constitution, statute, rule, ordinance, or resolution;
 - (B) consists of two or more persons;
 - (C) expends, disburses, or is supported in whole or in part by tax revenue; and
 - (D) is vested with the authority to make decisions regarding the public's business; or
 - (ii) any administrative, advisory, executive, or policymaking body of an association, as that term is defined in Section 53G-7-1101, that:
 - (A) consists of two or more persons;
 - (B) expends, disburses, or is supported in whole or in part by dues paid by a public school or whose employees participate in a benefit or program described in Title 49, Utah State Retirement and Insurance Benefit Act; and
 - (C) is vested with authority to make decisions regarding the participation of a public school or student in an interscholastic activity, as that term is defined in Section 53G-7-1101.
 - (b) "Public body" includes:
 - (i) an interlocal entity or joint or cooperative undertaking, as those terms are defined in Section 11-13-103;
 - (ii) a governmental nonprofit corporation as that term is defined in Section 11-13a-102; and
 - (iii) the Utah Independent Redistricting Commission.
 - (c) "Public body" does not include:
 - (i) a political party, a political group, or a political caucus;

- (ii) a conference committee, a rules committee, or a sifting committee of the Legislature;
- (iii) a school community council or charter trust land council, as that term is defined in Section 53G-7-1203;
- (iv) a taxed interlocal entity, as that term is defined in Section 11-13-602; or
- (v) the following Legislative Management subcommittees, which are established in Section 36-12-8, when meeting for the purpose of selecting or evaluating a candidate to recommend for employment, except that the meeting in which a subcommittee votes to recommend that a candidate be employed shall be subject to the provisions of this act:
 - (A) the Research and General Counsel Subcommittee;
 - (B) the Budget Subcommittee; and
 - (C) the Audit Subcommittee.
- (10) "Public statement" means a statement made in the ordinary course of business of the public body with the intent that all other members of the public body receive it.
- (11)
 - (a) "Quorum" means a simple majority of the membership of a public body, unless otherwise defined by applicable law.
 - (b) "Quorum" does not include a meeting of two elected officials by themselves when no action, either formal or informal, is taken.
- (12) "Recording" means an audio, or an audio and video, record of the proceedings of a meeting that can be used to review the proceedings of the meeting.
- (13) "Specified body":
 - (a) means an administrative, advisory, executive, or legislative body that:
 - (i) is not a public body;
 - (ii) consists of three or more members; and
 - (iii) includes at least one member who is:
 - (A) a legislator; and
 - (B) officially appointed to the body by the president of the Senate, speaker of the House of Representatives, or governor; and
 - (b) does not include a body listed in Subsection (9)(c)(ii) or (9)(c)(v).
- (14) "Transmit" means to send, convey, or communicate an electronic message by electronic means.

Amended by Chapter 25, 2019 General Session

Amended by Chapter 246, 2019 General Session

52-4-104 Training.

- (1) The presiding officer of the public body shall ensure that the members of the public body are provided with annual training on the requirements of this chapter.
- (2) The presiding officer shall ensure that any training described in Subsection (1) complies with Title 63G, Chapter 22, State Training and Certification Requirements.

Amended by Chapter 200, 2018 General Session

Part 2 Meetings

52-4-201 Meetings open to the public -- Exceptions.

- (1) A meeting is open to the public unless closed under Sections 52-4-204, 52-4-205, and 52-4-206.
- (2)
 - (a) A meeting that is open to the public includes a workshop or an executive session of a public body in which a quorum is present, unless closed in accordance with this chapter.
 - (b) A workshop or an executive session of a public body in which a quorum is present that is held on the same day as a regularly scheduled public meeting of the public body may only be held at the location where the public body is holding the regularly scheduled public meeting unless:
 - (i) the workshop or executive session is held at the location where the public body holds its regularly scheduled public meetings but, for that day, the regularly scheduled public meeting is being held at different location;
 - (ii) any of the meetings held on the same day is a site visit or a traveling tour and, in accordance with this chapter, public notice is given;
 - (iii) the workshop or executive session is an electronic meeting conducted according to the requirements of Section 52-4-207; or
 - (iv) it is not practicable to conduct the workshop or executive session at the regular location of the public body's open meetings due to an emergency or extraordinary circumstances.

Renumbered and Amended by Chapter 14, 2006 General Session
Amended by Chapter 263, 2006 General Session

52-4-202 Public notice of meetings -- Emergency meetings.

- (1)
 - (a)
 - (i) A public body shall give not less than 24 hours' public notice of each meeting.
 - (ii) A specified body shall give not less than 24 hours' public notice of each meeting that the specified body holds on the capitol hill complex.
 - (b) The public notice required under Subsection (1)(a) shall include the meeting:
 - (i) agenda;
 - (ii) date;
 - (iii) time; and
 - (iv) place.
- (2)
 - (a) In addition to the requirements under Subsection (1), a public body which holds regular meetings that are scheduled in advance over the course of a year shall give public notice at least once each year of its annual meeting schedule as provided in this section.
 - (b) The public notice under Subsection (2)(a) shall specify the date, time, and place of the scheduled meetings.
- (3)
 - (a) A public body or specified body satisfies a requirement for public notice by:
 - (i) posting written notice:
 - (A) except for an electronic meeting held without an anchor location under Subsection 52-4-207(4), at the principal office of the public body or specified body, or if no principal office exists, at the building where the meeting is to be held; and
 - (B) on the Utah Public Notice Website created under Section 63F-1-701; and
 - (ii) providing notice to:

- (A) at least one newspaper of general circulation within the geographic jurisdiction of the public body; or
- (B) a local media correspondent.
- (b) A public body or specified body is in compliance with the provisions of Subsection (3)(a)(ii) by providing notice to a newspaper or local media correspondent under the provisions of Subsection 63F-1-701(4)(d).
- (c) A public body whose limited resources make compliance with Subsection (3)(a)(i)(B) difficult may request the Division of Archives and Records Service, created in Section 63A-12-101, to provide technical assistance to help the public body in its effort to comply.
- (4) A public body and a specified body are encouraged to develop and use additional electronic means to provide notice of their meetings under Subsection (3).
- (5)
 - (a) The notice requirement of Subsection (1) may be disregarded if:
 - (i) because of unforeseen circumstances it is necessary for a public body or specified body to hold an emergency meeting to consider matters of an emergency or urgent nature; and
 - (ii) the public body or specified body gives the best notice practicable of:
 - (A) the time and place of the emergency meeting; and
 - (B) the topics to be considered at the emergency meeting.
 - (b) An emergency meeting of a public body may not be held unless:
 - (i) an attempt has been made to notify all the members of the public body; and
 - (ii) a majority of the members of the public body approve the meeting.
- (6)
 - (a) A public notice that is required to include an agenda under Subsection (1) shall provide reasonable specificity to notify the public as to the topics to be considered at the meeting. Each topic shall be listed under an agenda item on the meeting agenda.
 - (b) Subject to the provisions of Subsection (6)(c), and at the discretion of the presiding member of the public body, a topic raised by the public may be discussed during an open meeting, even if the topic raised by the public was not included in the agenda or advance public notice for the meeting.
 - (c) Except as provided in Subsection (5), relating to emergency meetings, a public body may not take final action on a topic in an open meeting unless the topic is:
 - (i) listed under an agenda item as required by Subsection (6)(a); and
 - (ii) included with the advance public notice required by this section.
- (7) Except as provided in this section, this chapter does not apply to a specified body.

Amended by Chapter 1, 2020 Special Session 5

52-4-203 Written minutes of open meetings -- Public records -- Recording of meetings.

- (1) Except as provided under Subsection (7), written minutes and a recording shall be kept of all open meetings.
- (2)
 - (a) Written minutes of an open meeting shall include:
 - (i) the date, time, and place of the meeting;
 - (ii) the names of members present and absent;
 - (iii) the substance of all matters proposed, discussed, or decided by the public body which may include a summary of comments made by members of the public body;
 - (iv) a record, by individual member, of each vote taken by the public body;
 - (v) the name of each person who:

- (A) is not a member of the public body; and
- (B) after being recognized by the presiding member of the public body, provided testimony or comments to the public body;
- (vi) the substance, in brief, of the testimony or comments provided by the public under Subsection (2)(a)(v); and
- (vii) any other information that is a record of the proceedings of the meeting that any member requests be entered in the minutes or recording.
- (b) A public body may satisfy the requirement under Subsection (2)(a)(iii) or (vi) that minutes include the substance of matters proposed, discussed, or decided or the substance of testimony or comments by maintaining a publicly available online version of the minutes that provides a link to the meeting recording at the place in the recording where the matter is proposed, discussed, or decided or the testimony or comments provided.
- (3) A recording of an open meeting shall:
 - (a) be a complete and unedited record of all open portions of the meeting from the commencement of the meeting through adjournment of the meeting; and
 - (b) be properly labeled or identified with the date, time, and place of the meeting.
- (4)
 - (a) As used in this Subsection (4):
 - (i) "Approved minutes" means written minutes:
 - (A) of an open meeting; and
 - (B) that have been approved by the public body that held the open meeting.
 - (ii) "Electronic information" means information presented or provided in an electronic format.
 - (iii) "Pending minutes" means written minutes:
 - (A) of an open meeting; and
 - (B) that have been prepared in draft form and are subject to change before being approved by the public body that held the open meeting.
 - (iv) "Specified local public body" means a legislative body of a county, city, town, or metro township.
 - (v) "State public body" means a public body that is an administrative, advisory, executive, or legislative body of the state.
 - (vi) "State website" means the Utah Public Notice Website created under Section 63F-1-701.
 - (b) Pending minutes, approved minutes, and a recording of a public meeting are public records under Title 63G, Chapter 2, Government Records Access and Management Act.
 - (c) Pending minutes shall contain a clear indication that the public body has not yet approved the minutes or that the minutes are subject to change until the public body approves them.
 - (d) A state public body and a specified local public body shall require an individual who, at an open meeting of the public body, publicly presents or provides electronic information, relating to an item on the public body's meeting agenda, to provide the public body, at the time of the meeting, an electronic or hard copy of the electronic information for inclusion in the public record.
 - (e) A state public body shall:
 - (i) make pending minutes available to the public within 30 days after holding the open meeting that is the subject of the pending minutes;
 - (ii) within three business days after approving written minutes of an open meeting:
 - (A) post to the state website a copy of the approved minutes and any public materials distributed at the meeting;
 - (B) make the approved minutes and public materials available to the public at the public body's primary office; and

- (C) if the public body provides online minutes under Subsection (2)(b), post approved minutes that comply with Subsection (2)(b) and the public materials on the public body's website; and
- (iii) within three business days after holding an open meeting, post on the state website an audio recording of the open meeting, or a link to the recording.
- (f) A specified local public body shall:
 - (i) make pending minutes available to the public within 30 days after holding the open meeting that is the subject of the pending minutes;
 - (ii) within three business days after approving written minutes of an open meeting, post and make available a copy of the approved minutes and any public materials distributed at the meeting, as provided in Subsection (4)(e)(ii); and
 - (iii) within three business days after holding an open meeting, make an audio recording of the open meeting available to the public for listening.
- (g) A public body that is not a state public body or a specified local public body shall:
 - (i) make pending minutes available to the public within a reasonable time after holding the open meeting that is the subject of the pending minutes;
 - (ii) within three business days after approving written minutes, make the approved minutes available to the public; and
 - (iii) within three business days after holding an open meeting, make an audio recording of the open meeting available to the public for listening.
- (h) A public body shall establish and implement procedures for the public body's approval of the written minutes of each meeting.
 - (i) Approved minutes of an open meeting are the official record of the meeting.
- (5) All or any part of an open meeting may be independently recorded by any person in attendance if the recording does not interfere with the conduct of the meeting.
- (6) The written minutes or recording of an open meeting that are required to be retained permanently shall be maintained in or converted to a format that meets long-term records storage requirements.
- (7) Notwithstanding Subsection (1), a recording is not required to be kept of:
 - (a) an open meeting that is a site visit or a traveling tour, if no vote or action is taken by the public body; or
 - (b) an open meeting of a local district under Title 17B, Limited Purpose Local Government Entities - Local Districts, or special service district under Title 17D, Chapter 1, Special Service District Act, if the district's annual budgeted expenditures for all funds, excluding capital expenditures and debt service, are \$50,000 or less.

Amended by Chapter 425, 2018 General Session

52-4-204 Closed meeting held upon vote of members -- Business -- Reasons for meeting recorded.

- (1) A closed meeting may be held if:
 - (a)
 - (i) a quorum is present;
 - (ii) the meeting is an open meeting for which notice has been given under Section 52-4-202; and
 - (iii)
 - (A) two-thirds of the members of the public body present at the open meeting vote to approve closing the meeting;

- (B) for a meeting that is required to be closed under Section 52-4-205, if a majority of the members of the public body present at an open meeting vote to approve closing the meeting;
 - (C) for an ethics committee of the Legislature that is conducting an open meeting for the purpose of reviewing an ethics complaint, a majority of the members present vote to approve closing the meeting for the purpose of seeking or obtaining legal advice on legal, evidentiary, or procedural matters, or for conducting deliberations to reach a decision on the complaint; or
 - (D) for the Political Subdivisions Ethics Review Commission established in Section 63A-15-201 that is conducting an open meeting for the purpose of reviewing an ethics complaint in accordance with Section 63A-15-701, a majority of the members present vote to approve closing the meeting for the purpose of seeking or obtaining legal advice on legal, evidentiary, or procedural matters, or for conducting deliberations to reach a decision on the complaint; or
- (b)
- (i) for the Independent Legislative Ethics Commission, the closed meeting is convened for the purpose of conducting business relating to the receipt or review of an ethics complaint, provided that public notice of the closed meeting is given under Section 52-4-202, with the agenda for the meeting stating that the meeting will be closed for the purpose of "conducting business relating to the receipt or review of ethics complaints";
 - (ii) for the Political Subdivisions Ethics Review Commission established in Section 63A-15-201, the closed meeting is convened for the purpose of conducting business relating to the preliminary review of an ethics complaint in accordance with Section 63A-15-602, provided that public notice of the closed meeting is given under Section 52-4-202, with the agenda for the meeting stating that the meeting will be closed for the purpose of "conducting business relating to the review of ethics complaints"; or
 - (iii) for the Independent Executive Branch Ethics Commission created in Section 63A-14-202, the closed meeting is convened for the purpose of conducting business relating to an ethics complaint, provided that public notice of the closed meeting is given under Section 52-4-202, with the agenda for the meeting stating that the meeting will be closed for the purpose of "conducting business relating to an ethics complaint."
- (2) A closed meeting is not allowed unless each matter discussed in the closed meeting is permitted under Section 52-4-205.
 - (3) An ordinance, resolution, rule, regulation, contract, or appointment may not be approved at a closed meeting.
 - (4) The following information shall be publicly announced and entered on the minutes of the open meeting at which the closed meeting was approved:
 - (a) the reason or reasons for holding the closed meeting;
 - (b) the location where the closed meeting will be held; and
 - (c) the vote by name, of each member of the public body, either for or against the motion to hold the closed meeting.
 - (5) Except as provided in Subsection 52-4-205(2), nothing in this chapter shall be construed to require any meeting to be closed to the public.

Amended by Chapter 461, 2018 General Session

52-4-205 Purposes of closed meetings -- Certain issues prohibited in closed meetings.

- (1) A closed meeting described under Section 52-4-204 may only be held for:

- (a) except as provided in Subsection (3), discussion of the character, professional competence, or physical or mental health of an individual;
- (b) strategy sessions to discuss collective bargaining;
- (c) strategy sessions to discuss pending or reasonably imminent litigation;
- (d) strategy sessions to discuss the purchase, exchange, or lease of real property, including any form of a water right or water shares, if public discussion of the transaction would:
 - (i) disclose the appraisal or estimated value of the property under consideration; or
 - (ii) prevent the public body from completing the transaction on the best possible terms;
- (e) strategy sessions to discuss the sale of real property, including any form of a water right or water shares, if:
 - (i) public discussion of the transaction would:
 - (A) disclose the appraisal or estimated value of the property under consideration; or
 - (B) prevent the public body from completing the transaction on the best possible terms;
 - (ii) the public body previously gave public notice that the property would be offered for sale; and
 - (iii) the terms of the sale are publicly disclosed before the public body approves the sale;
- (f) discussion regarding deployment of security personnel, devices, or systems;
- (g) investigative proceedings regarding allegations of criminal misconduct;
- (h) as relates to the Independent Legislative Ethics Commission, conducting business relating to the receipt or review of ethics complaints;
- (i) as relates to an ethics committee of the Legislature, a purpose permitted under Subsection 52-4-204(1)(a)(iii)(C);
- (j) as relates to the Independent Executive Branch Ethics Commission created in Section 63A-14-202, conducting business relating to an ethics complaint;
- (k) as relates to a county legislative body, discussing commercial information as defined in Section 59-1-404;
- (l) as relates to the Utah Higher Education Assistance Authority and its appointed board of directors, discussing fiduciary or commercial information as defined in Section 53B-12-102;
- (m) deliberations, not including any information gathering activities, of a public body acting in the capacity of:
 - (i) an evaluation committee under Title 63G, Chapter 6a, Utah Procurement Code, during the process of evaluating responses to a solicitation, as defined in Section 63G-6a-103;
 - (ii) a protest officer, defined in Section 63G-6a-103, during the process of making a decision on a protest under Title 63G, Chapter 6a, Part 16, Protests; or
 - (iii) a procurement appeals panel under Title 63G, Chapter 6a, Utah Procurement Code, during the process of deciding an appeal under Title 63G, Chapter 6a, Part 17, Procurement Appeals Board;
- (n) the purpose of considering information that is designated as a trade secret, as defined in Section 13-24-2, if the public body's consideration of the information is necessary in order to properly conduct a procurement under Title 63G, Chapter 6a, Utah Procurement Code;
- (o) the purpose of discussing information provided to the public body during the procurement process under Title 63G, Chapter 6a, Utah Procurement Code, if, at the time of the meeting:
 - (i) the information may not, under Title 63G, Chapter 6a, Utah Procurement Code, be disclosed to a member of the public or to a participant in the procurement process; and
 - (ii) the public body needs to review or discuss the information in order to properly fulfill its role and responsibilities in the procurement process;
- (p) as relates to the governing board of a governmental nonprofit corporation, as that term is defined in Section 11-13a-102, the purpose of discussing information that is designated as a trade secret, as that term is defined in Section 13-24-2, if:

- (i) public knowledge of the discussion would reasonably be expected to result in injury to the owner of the trade secret; and
 - (ii) discussion of the information is necessary for the governing board to properly discharge the board's duties and conduct the board's business; or
 - (q) a purpose for which a meeting is required to be closed under Subsection (2).
- (2) The following meetings shall be closed:
- (a) a meeting of the Health and Human Services Interim Committee to review a fatality review report described in Subsection 62A-16-301(1)(a), and the responses to the report described in Subsections 62A-16-301(2) and (4);
 - (b) a meeting of the Child Welfare Legislative Oversight Panel to:
 - (i) review a fatality review report described in Subsection 62A-16-301(1)(a), and the responses to the report described in Subsections 62A-16-301(2) and (4); or
 - (ii) review and discuss an individual case, as described in Subsection 62A-4a-207(5);
 - (c) a meeting of the Opioid and Overdose Fatality Review Committee, created in Section 26-7-13, to review and discuss an individual case, as described in Subsection 26-7-13(10); and
 - (d) a meeting of a conservation district as defined in Section 17D-3-102 for the purpose of advising the Natural Resource Conservation Service of the United States Department of Agriculture on a farm improvement project if the discussed information is protected information under federal law; and
 - (e) a meeting of the Compassionate Use Board established in Section 26-61a-105 for the purpose of reviewing petitions for a medical cannabis card in accordance with Section 26-61a-105.
- (3) In a closed meeting, a public body may not:
- (a) interview a person applying to fill an elected position;
 - (b) discuss filling a midterm vacancy or temporary absence governed by Title 20A, Chapter 1, Part 5, Candidate Vacancy and Vacancy and Temporary Absence in Elected Office; or
 - (c) discuss the character, professional competence, or physical or mental health of the person whose name was submitted for consideration to fill a midterm vacancy or temporary absence governed by Title 20A, Chapter 1, Part 5, Candidate Vacancy and Vacancy and Temporary Absence in Elected Office.

Amended by Chapter 12, 2020 General Session

Amended by Chapter 201, 2020 General Session

52-4-206 Record of closed meetings.

- (1) Except as provided under Subsection (6), if a public body closes a meeting under Subsection 52-4-205(1), the public body:
 - (a) shall make a recording of the closed portion of the meeting; and
 - (b) may keep detailed written minutes that disclose the content of the closed portion of the meeting.
- (2) A recording of a closed meeting shall be complete and unedited from the commencement of the closed meeting through adjournment of the closed meeting.
- (3) The recording and any minutes of a closed meeting shall include:
 - (a) the date, time, and place of the meeting;
 - (b) the names of members present and absent; and
 - (c) the names of all others present except where the disclosure would infringe on the confidentiality necessary to fulfill the original purpose of closing the meeting.

- (4) Minutes or recordings of a closed meeting that are required to be retained permanently shall be maintained in or converted to a format that meets long-term records storage requirements.
- (5) A recording, transcript, report, and written minutes of a closed meeting are protected records under Title 63G, Chapter 2, Government Records Access and Management Act, except that the records may be disclosed under a court order only as provided under Section 52-4-304.
- (6) If a public body closes a meeting exclusively for the purposes described under Subsection 52-4-205(1)(a), (1)(f), or (2):
 - (a) the person presiding shall sign a sworn statement affirming that the sole purpose for closing the meeting was to discuss the purposes described under Subsection 52-4-205(1)(a),(1)(f), or (2); and
 - (b) the provisions of Subsection (1) of this section do not apply.

Amended by Chapter 425, 2018 General Session

52-4-207 Electronic meetings -- Authorization -- Requirements.

- (1) Except as otherwise provided for a charter school in Section 52-4-209, a public body may convene and conduct an electronic meeting in accordance with this section.
- (2)
 - (a) A public body may not hold an electronic meeting unless the public body has adopted a resolution, rule, or ordinance governing the use of electronic meetings.
 - (b) The resolution, rule, or ordinance may:
 - (i) prohibit or limit electronic meetings based on budget, public policy, or logistical considerations;
 - (ii) require a quorum of the public body to:
 - (A) be present at a single anchor location for the meeting; and
 - (B) vote to approve establishment of an electronic meeting in order to include other members of the public body through an electronic connection;
 - (iii) require a request for an electronic meeting to be made by a member of a public body up to three days prior to the meeting to allow for arrangements to be made for the electronic meeting;
 - (iv) restrict the number of separate connections for members of the public body that are allowed for an electronic meeting based on available equipment capability; or
 - (v) establish other procedures, limitations, or conditions governing electronic meetings not in conflict with this section.
- (3) A public body that convenes or conducts an electronic meeting shall:
 - (a) give public notice of the meeting:
 - (i) in accordance with Section 52-4-202; and
 - (ii) except for an electronic meeting held without an anchor location under Subsection (4), post written notice at the anchor location;
 - (b) in addition to giving public notice required by Subsection (3)(a), provide:
 - (i) notice of the electronic meeting to the members of the public body at least 24 hours before the meeting so that they may participate in and be counted as present for all purposes, including the determination that a quorum is present; and
 - (ii) a description of how the members will be connected to the electronic meeting;
 - (c) except for an electronic meeting held without an anchor location under Subsection (4), establish one or more anchor locations for the public meeting, at least one of which is in the building and political subdivision where the public body would normally meet if they were not holding an electronic meeting;

- (d)
 - (i) provide space and facilities at the anchor location so that interested persons and the public may attend and monitor the open portions of the meeting; or
 - (ii) for an electronic meeting held without an anchor location under Subsection (4), provide means by which the public may hear, or view and hear, the open portions of the meeting; and
- (e) if comments from the public will be accepted during the electronic meeting:
 - (i) provide space and facilities at the anchor location so that interested persons and the public may attend, monitor, and participate in the open portions of the meeting; or
 - (ii) for an electronic meeting held without an anchor location under Subsection (4), provide means by which members of the public may provide comments by electronic means to the public body.
- (4) A public body may convene and conduct an electronic meeting without an anchor location if the chair of the public body:
 - (a) makes a written determination that conducting the meeting with an anchor location presents a substantial risk to the health and safety of those who may be present at the anchor location;
 - (b) states in the written determination described in Subsection (4)(a) the facts upon which the determination is based;
 - (c) includes in the public notice for the meeting, and reads at the beginning of the meeting, the information described in Subsections (4)(a) and (b); and
 - (d) includes in the public notice information on how a member of the public may view or make a comment at the meeting.
- (5) A written determination described in Subsections (4)(a) and (b) expires 30 days after the day on which the chair of the public body makes the determination.
- (6) Compliance with the provisions of this section by a public body constitutes full and complete compliance by the public body with the corresponding provisions of Sections 52-4-201 and 52-4-202.

Amended by Chapter 1, 2020 Special Session 5

52-4-208 Chance or social meetings.

- (1) This chapter does not apply to any chance meeting or a social meeting.
- (2) A chance meeting or social meeting may not be used to circumvent the provisions of this chapter.

Enacted by Chapter 14, 2006 General Session

52-4-209 Electronic meetings for charter school board.

- (1) Notwithstanding the definitions provided in Section 52-4-103 for this chapter, as used in this section:
 - (a) "Anchor location" means a physical location where:
 - (i) the charter school board would normally meet if the charter school board were not holding an electronic meeting; and
 - (ii) space, a facility, and technology are provided to the public to monitor and, if public comment is allowed, to participate in an electronic meeting during regular business hours.
 - (b) "Charter school board" means the governing board of a school created under Title 53G, Chapter 5, Charter Schools.
 - (c) "Meeting" means the convening of a charter school board:

- (i) with a quorum who:
 - (A) monitors a website at least once during the electronic meeting; and
 - (B) casts a vote on a website, if a vote is taken; and
 - (ii) for the purpose of discussing, receiving comments from the public about, or acting upon a matter over which the charter school board has jurisdiction or advisory power.
 - (d) "Monitor" means to:
 - (i) read all the content added to a website by the public or a charter school board member; and
 - (ii) view a vote cast by a charter school board member on a website.
 - (e) "Participate" means to add content to a website.
- (2)
- (a) A charter school board may convene and conduct an electronic meeting in accordance with Section 52-4-207.
 - (b) A charter school board may convene and conduct an electronic meeting in accordance with this section that is in writing on a website if:
 - (i) the chair verifies that a quorum monitors the website;
 - (ii) the content of the website is available to the public;
 - (iii) the chair controls the times in which a charter school board member or the public participates; and
 - (iv) the chair requires a person to identify himself or herself if the person:
 - (A) participates; or
 - (B) casts a vote as a charter school board member.
- (3) A charter school that conducts an electronic meeting under this section shall:
- (a) give public notice of the electronic meeting:
 - (i) in accordance with Section 52-4-202; and
 - (ii) by posting written notice at the anchor location as required under Section 52-4-207;
 - (b) in addition to giving public notice required by Subsection (3)(a), provide:
 - (i) notice of the electronic meeting to the members of the charter school board at least 24 hours before the meeting so that they may participate in and be counted as present for all purposes, including the determination that a quorum is present;
 - (ii) a description of how the members and the public may be connected to the electronic meeting;
 - (iii) a start and end time for the meeting, which shall be no longer than 5 days; and
 - (iv) a start and end time for when a vote will be taken in an electronic meeting, which shall be no longer than four hours; and
 - (c) provide an anchor location.
- (4) The chair shall:
- (a) not allow anyone to participate from the time the notice described in Subsection (3)(b)(iv) is given until the end time for when a vote will be taken; and
 - (b) allow a charter school board member to change a vote until the end time for when a vote will be taken.
- (5) During the time in which a vote may be taken, a charter school board member may not communicate in any way with any person regarding an issue over which the charter school board has jurisdiction.
- (6) A charter school conducting an electronic meeting under this section may not close a meeting as otherwise allowed under this part.
- (7)
- (a) Written minutes shall be kept of an electronic meeting conducted as required in Section 52-4-203.

- (b)
 - (i) Notwithstanding Section 52-4-203, a recording is not required of an electronic meeting described in Subsection (2)(b).
 - (ii) All of the content of the website shall be kept for an electronic meeting conducted under this section.
- (c) Written minutes are the official record of action taken at an electronic meeting as required in Section 52-4-203.
- (8)
 - (a) A charter school board shall ensure that the website used to conduct an electronic meeting:
 - (i) is secure; and
 - (ii) provides with reasonably certainty the identity of a charter school board member who logs on, adds content, or casts a vote on the website.
 - (b) A person is guilty of a class B misdemeanor if the person falsely identifies himself or herself as required by Subsection (2)(b)(iv).
- (9) Compliance with the provisions of this section by a charter school constitutes full and complete compliance by the public body with the corresponding provisions of Sections 52-4-201 and 52-4-202.

Amended by Chapter 415, 2018 General Session

52-4-210 Electronic message transmissions.

Nothing in this chapter shall be construed to restrict a member of a public body from transmitting an electronic message to other members of the public body at a time when the public body is not convened in an open meeting.

Enacted by Chapter 25, 2011 General Session

Part 3 Enforcement

52-4-301 Disruption of meetings.

This chapter does not prohibit the removal of any person from a meeting, if the person willfully disrupts the meeting to the extent that orderly conduct is seriously compromised.

Enacted by Chapter 14, 2006 General Session

52-4-302 Suit to void final action -- Limitation -- Exceptions.

- (1)
 - (a) Any final action taken in violation of Section 52-4-201, 52-4-202, 52-4-207, or 52-4-209 is voidable by a court of competent jurisdiction.
 - (b) A court may not void a final action taken by a public body for failure to comply with the posting written notice requirements under Subsection 52-4-202(3)(a)(i)(B) if:
 - (i) the posting is made for a meeting that is held before April 1, 2009; or
 - (ii)
 - (A) the public body otherwise complies with the provisions of Section 52-4-202; and

(B) the failure was a result of unforeseen Internet hosting or communication technology failure.

- (2) Except as provided under Subsection (3), a suit to void final action shall be commenced within 90 days after the date of the action.
- (3) A suit to void final action concerning the issuance of bonds, notes, or other evidences of indebtedness shall be commenced within 30 days after the date of the action.

Amended by Chapter 403, 2012 General Session

52-4-303 Enforcement of chapter -- Suit to compel compliance.

- (1) The attorney general and county attorneys of the state shall enforce this chapter.
- (2) The attorney general shall, on at least a yearly basis, provide notice to all public bodies that are subject to this chapter of any material changes to the requirements for the conduct of meetings under this chapter.
- (3) A person denied any right under this chapter may commence suit in a court of competent jurisdiction to:
 - (a) compel compliance with or enjoin violations of this chapter; or
 - (b) determine the chapter's applicability to discussions or decisions of a public body.
- (4) The court may award reasonable attorney fees and court costs to a successful plaintiff.

Renumbered and Amended by Chapter 14, 2006 General Session

Amended by Chapter 263, 2006 General Session

52-4-304 Action challenging closed meeting.

- (1) Notwithstanding the procedure established under Subsection 63G-2-202(7), in any action brought under the authority of this chapter to challenge the legality of a closed meeting held by a public body, the court shall:
 - (a) review the recording or written minutes of the closed meeting in camera; and
 - (b) decide the legality of the closed meeting.
- (2)
 - (a) If the judge determines that the public body did not violate Section 52-4-204, 52-4-205, or 52-4-206 regarding closed meetings, the judge shall dismiss the case without disclosing or revealing any information from the recording or minutes of the closed meeting.
 - (b) If the judge determines that the public body violated Section 52-4-204, 52-4-205, or 52-4-206 regarding closed meetings, the judge shall publicly disclose or reveal from the recording or minutes of the closed meeting all information about the portion of the meeting that was illegally closed.
- (3) Nothing in this section may be construed to affect the ability of a public body to reclassify a record, as defined in Section 63G-2-103, as provided in Section 63G-2-307.

Amended by Chapter 425, 2018 General Session

52-4-305 Criminal penalty for closed meeting violation.

In addition to any other penalty under this chapter, a member of a public body who knowingly or intentionally violates or who knowingly or intentionally abets or advises a violation of any of the closed meeting provisions of this chapter is guilty of a class B misdemeanor.

Enacted by Chapter 263, 2006 General Session

Social Media And Elected Officials: Navigating Uncharted Territory

By Stephanie York/Hennes Communications

Q. I work for a government agency and am an elected official. Our office maintains a social media presence, my campaign maintains a social media presence, and I also have a personal Facebook page. What should I do to be sure my “personal” pages are not subject to public records law?

Laws across the nation are evolving regarding a public official's right to maintain personal social media pages and the obligation of that official to abide by public records laws. Even though new cases are playing out daily, there are some rules and best practices you can adopt to help ensure your personal pages remain personal – without violating the public record laws of your state.

Here's the first rule: This is not legal advice and public officials should ultimately bring questions about what's appropriate for their personal social media pages to an attorney.

That said, broad guidelines to help public officials navigate these waters – and help the public hold their elected and appointed officials accountable on social media – have emerged.

First, make a clear distinction between official accounts, campaign accounts and personal accounts. Elected officials can clearly distinguish private social media accounts by adding disclaimers or explanations to describe what type of page it is and is not. And then build a high wall between any government page and campaign or personal pages.

The law is clear on this: Government-sponsored accounts may not be used for campaign-related or personal purposes. And personal and campaign accounts should not be used for government related business. Period.

Second, understand and use privacy settings to manage your personal accounts. Set your privacy settings to reflect your personal audience, such as “friends only”.

Third, establish a process to follow if you receive a government-related question or comment on your personal accounts. Specifically, have a plan in place for how you'll forward government-related comments or questions to the appropriate government social media page. Briefly explain to the commenter how the question or comment will be addressed, and how the record will be retained.

Fourth, do not write posts on personal accounts that would fit within the scope of your employment as a government official. And if you do, understand your personal account now may have morphed into a public account.

On July 9, a federal appeals court ruled that President Donald Trump has been violating the Constitution by blocking people from his Twitter account because he uses the account to conduct government business and, therefore, can't exclude some Americans from reading his posts.

Similarly, in January, a federal appeals court ruled that an elected official in Virginia violated the First Amendment when she temporarily blocked a constituent on Facebook. The Washington Post called it “a novel case with implications for how government officials nationwide interact with constituents on social media.”

The official, in a separate court filing, contended that her account on a privately owned digital platform is personal and she should be able to restrict who gets a chance to speak there without crossing constitutional lines. In this case, she considered a community activist’s posts on her site to be slanderous.

Who’s right? That’s being determined. And these cases will continue winding through the courts, possibly all the way to the U.S. Supreme Court.

Fifth, do not discuss your private accounts in public meetings or documents. For example, do not direct people to follow your personal page during a city council or other public meeting.

Sixth, do not link to your private accounts from an official government account.

Seventh, do not use government devices to maintain your private accounts.

Eighth, do not allow social media chatter to become a public meeting. According to the National Conference of State Legislatures, you have a public meeting:

- If you have a quorum discussing or commenting on an issue.
- If any official business, policy or public matter is formulated, presented, discussed or, of course, voted.

That’s a potentially broad definition. Court guidance is still evolving here as well, but the bottom line is to avoid inadvertent exchanges that could violate notice and public meeting requirements.

Got a question about crisis communications, issues management or reputation management? We’ve got the answers. Send your question to info@crisiscommunications.com



MURRAY
CITY COUNCIL

Discussion Item #3



MURRAY

City Council

Annual Training on Harassment

Council Action Request

Committee of the Whole

Meeting Date: February 16, 2021

Department Director Jennifer Kennedy Phone # 801-264-2622 Presenters G.L. Critchfield, City Attorney Required Time for Presentation 10 Minutes Is This Time Sensitive Yes Mayor's Approval Date January 26, 2021	Purpose of Proposal Annual training on Harassment Action Requested Informational only. Attachments Memo and Article IX of the Rules of the Murray City Municipal Council. Budget Impact None. Description of this Item This is an opportunity to review the City's Anti-Harassment Policy and how someone should report discrimination or harassment.
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MEMORANDUM

TO: Murray City Council

CC: Mayor Blair Camp

FROM: G.L. Critchfield, City Attorney

DATE: January 26, 2021

RE: Harassment Training

The City Council has requested that annual training on Harassment should be provided to the City Council. We would like to provide the requested training for the City Council at its meeting on February 16, 2021. We anticipate that the training would not exceed ten (10) minutes. We are providing to you copies of Article IX of the Rules of the Murray City Municipal Council.

If you have any questions, please do not hesitate to contact us. We look forward to meeting with you on February 16th.

IX. COUNCIL RELATIONS

A. Anti-Harassment Policy

1. It is City policy to foster and maintain a work environment that is free from discrimination and intimidation. Toward this end, the City will not tolerate harassment of any kind that is made by City Councilmembers toward fellow Councilmembers, City Staff or members of the public. City Councilmembers are expected to show respect for one another and the public at all times, despite individual differences.
2. Harassment is unwelcome conduct that is based on race, color, religion, sex (including pregnancy, childbirth and related medical conditions), national origin, age (40 or older), disability, genetic information, marital status, sexual orientation, honorably discharged veteran or military status or the presence of any sensory, mental or physical disability or the use of a trained dog guide or service animal by a person with a disability. In some circumstances, it can be deemed unlawful discrimination that violates federal laws and/or state laws. The making of demeaning comments, whether verbally or in writing, or use of unwelcome epithets, gestures or other physical conduct, based on the above-referenced protected classes, toward employees, Councilmembers or members of the public are prohibited. Councilmembers are strongly urged to report all incidents of harassment.
3. Sexual harassment is a form of unlawful discrimination.

B. Reporting Discrimination or Harassment

1. If the incident involves a city employee, or an appointee to an advisory board or a commission, the incident should be reported as soon as possible to the Mayor.
2. If the incident involves a Councilmember, the incident should be reported as soon as possible to the City Attorney.
3. All complaints will be investigated promptly. Upon receiving a complaint, an investigation shall be initiated within 24 hours, or as reasonably practicable, usually, by the end of the next business day.
4. All complaints will be kept confidential to the fullest extent possible and will be disclosed only as necessary to allow an investigation and response to the complaint. No one will be involved in the investigation or response except those with a need to know. Any special concerns about confidentiality will be addressed at the time they are raised.
5. Anyone who is found to have violated this policy is subject to corrective action. Corrective action will depend on the gravity of the offense. The City Council will take whatever action it deems necessary to prevent an offense from being repeated.
6. The City Council will not permit retaliation against anyone who makes a complaint or who cooperates in an investigation.
7. Both the person filing the complaint and the alleged offender shall receive a written response that contains the findings of the investigation and any action taken. Unless extra time is needed for a thorough investigation, the response will normally be given within thirty (30) days of when the complaint was received. All parties will be notified of an extended investigation if such an extension is necessary to complete the findings.



MURRAY
CITY COUNCIL

Discussion Item #4



MURRAY

City Council

Legislative Updates

Council Action Request

Committee of the Whole

Meeting Date: February 16, 2021

Department Director Jennifer Kennedy Phone # 801-264-2622 Presenters Kat Martinez Required Time for Presentation 15 Minutes Is This Time Sensitive No Mayor's Approval Date 1/26/21	Purpose of Proposal An update on the 2021 Legislative Session Action Requested Attachments None. Budget Impact None. Description of this Item
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MURRAY
CITY COUNCIL

Adjournment

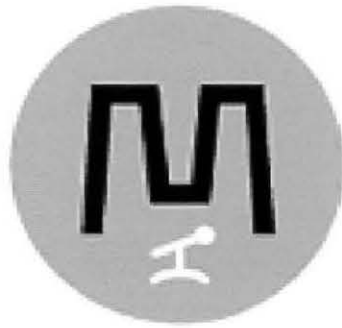


MURRAY
CITY COUNCIL

Council Meeting 6:30 p.m.

Call to Order

Pledge of Allegiance



MURRAY
CITY COUNCIL

Council Meeting Minutes

Murray City Municipal Council Chambers Murray City, Utah

The Murray City Municipal Council met on Tuesday, December 1, 2020 at 6:30 p.m. for a meeting held electronically without an anchor location in accordance with Utah Code 52-4-207(4), due to infectious disease COVID-19 Novel Coronavirus. The Council Chair determined that conducting a meeting with an anchor location presents substantial risk to the health and safety of those who may be present at the anchor location because physical distancing measures may be difficult to maintain in the Murray City Council Chambers.

The public was able to view the meeting via the live stream at www.murraycitylive.com or <https://www.facebook.com/Murraycityutah/>.

Council Members in Attendance:

Kat Martinez	District #1
Dale Cox	District #2
Rosalba Dominguez	District #3
Diane Turner	District #4 – Council Chair
Brett Hales	District #5 – Council Vice-Chair

Others in Attendance:

Blair Camp	Mayor	Jan Lopez	Council Director
G.L. Critchfield	City Attorney	Jennifer Kennedy	Council Director
Robert White	IT Director	Jennifer Heaps	Chief Communications Officer
Jared Hall	Community & Economic Development (CED)	Melinda Greenwood	Community & Economic Development (CED) Director
Danny Hansen	IT	Brooke Smith	City Recorder
Bill Strong	Murray Resident	Pattie Johnson	Council Office Administrator III
Saeld Ahar	Owner of Sew N Fit	Bill Francis	Utah VOD
Kyleigh Cooper	Miss. Murray	Karl Schatten	Senior Rec Center Board Appointee

Opening Ceremonies

Call to Order – Councilmember Cox called the meeting to order at 6:30 p.m.

Pledge of Allegiance – The Pledge of Allegiance was led by Councilmember Cox.

Special Recognition

1. Murray City Council Employee of the Month, Danny Hansen, Senior IT Technician

Staff Presentation: Brett Hales, Councilmember and Rob White, IT Director

Councilmember Hales said the Council started the Employee of the Month Program because they felt it was important to recognize the City's employees. He stated that Mr. Hansen would receive a certificate, a \$50 gift card and told him that his name would appear on the plaque located in the Council Chambers. He expressed his appreciation to Mr. Hansen for all he does for the City.

Mr. White spoke about the work Mr. Hansen has done during his time with the City.

Mr. Hansen expressed his appreciation for the recognition and thanked individuals who have helped him along the way.

The Councilmembers thanked Mr. Hansen for his hard work.

2. Consider a Joint Resolution of Appreciation to Janet M. Lopez, Executive Director for the Murray City Municipal Council. R21-05

Council Presentation: Diane Turner

Councilmember Turner read Resolution number 21-05 into the record.

Mayor Camp wished Ms. Lopez the best in her endeavors in the future.

Ms. Lopez expressed her thanks to the Mayor, city council, department directors, administrative staff, city employees, and the people she had the opportunity to work with and for all their hard work and support. Ms. Lopez wishes Jennifer Kennedy the best and shares that she has great confidence in Ms. Kennedy and knows she will continue to take the city council forward. Ms. Lopez looks forward to traveling with her husband and extended family.

MOTION: Councilmember Hales moved to adopt the Joint Resolution. The motion was SECONDED by Councilmember Dominguez.

Council roll call vote:

Ayes: Councilmember Dominguez, Councilmember Turner, Councilmember Hales, Councilmember Martinez, Councilmember Cox

Nays: None

Abstentions: None

Motion passed 5-0

The Councilmembers thanked Ms. Lopez for the tremendous help she provided to the city council during her tenure.

Citizen Comments – Comments are limited to 3 minutes unless otherwise approved by the Council. Two comments were shared in-person via zoom and 15 were read by city council staff.

Bill Strong (chicken ordinance)

Mr. Strong expressed concerns with the ordinance that the city council is thinking about modifying. Mr. Strong thinks it is a bad idea and is surprised the council is revisiting it again since it was voted down two to three years ago. Mr. Strong thinks it is a bad idea because one, the lot sizes in Murray are small so whatever someone does on their land, it will effect their neighbors. Secondly, chickens in the summer stink a lot. Mr. Strong knows this because from personal experience when he worked on his uncle's farm in the summer and he knows what it is like to have stinky chickens around. He has concerns that if some of his neighbors have chickens in the lots next to him, it will negatively effect him and he thinks it would be best if the ordinance is left the way it is and he doesn't see a need to modify it moving forward.

Kyleigh Cooper, 2020 Miss Murray (Diversity and Inclusion Task Force)

Hello, first off, I want to thank you for the opportunity to comment this evening. Ms. Cooper is in favor of the creation of the Diversity and Inclusion Task Force. It has been inspiring to watch this project unfold. Now more than ever I think we are all dreaming of living in a unified country. But that sense of unification and community must start in our neighborhoods and our cities. I believe that everyone here in Murray City deserves a voice and through the creation of a Murray City Diversity and Inclusion Task Force I believe that we can make Murray City feel like a true and safe home to all. Through my efforts as Miss Murray to implement my social impact initiative, which is "End the Culture War" through promoting Unity, I have seen and felt the support of the citizens in our community. They are ready for this. I look forward to seeing what this Task Force will do for our community if voted for in favor, tonight.

Sherm and Marianne Ross (RC Willey) Read by Jan Lopez

During the past few years we have experienced an increase in property crimes in our neighborhood.

The Extended Stay Motel on Winchester, along with the James Point apartments and the Crystal Inn bring in a more transient population which increases the local property crime.

We are not opposed to the mixed use zoning change, but would request an exception be made to the Master Plan to allow condominiums rather than apartments. This would be an upgrade to the property along with lessening the transient population that apartments cater to.

Your consideration of this request would be greatly appreciated.

G. Scott Reading (High density housing, taxes, utility rates) Read by Pattie Johnson

I am writing this email to each of you to express my concerns about the direction Murray City seems to be taking. It seems the Murray I grew up with is rapidly disappearing.

I am referring to the development of high density housing, and, if rumors are true, the soon to be development of high-rise apartments in the city. It seems that if

there's a vacant piece of land more apartments and/or condos are being built. I also have to wonder where all the water is coming from to supply all the high density housing? (Oh yeah, have you seen your water bill lately)?

Equally concerning is the rate at which utilities and taxes seem to be rising. Yes, I know you will probably respond by saying we are still lower than surrounding areas, (haven't fact checked that), but when it comes to taxes and utility rates maybe it's not such a good idea to "keep up with the Jones's."

Quite frankly, I am very concerned at the direction this administration and council seems to be taking Murray City and it's residence. I have spoken with a few others (not many, but a few) who have the same concerns.

I'm quite certain this will fall on deaf ears as each administration and council have their own agendas, but thanks for letting me vent.

A life-long Murray resident.

Doug Brown (Rezoning) Read by Jennifer Kennedy

I am concerned about rezoning to M-U.

My questions and concerns are as follows:

In light of all of the calls to the Police department for many types of problems At the Extended stay motel, Crystal Inn, and even the James Point apartments, And the Increased break ins in our neighborhood adjacent to them. Why do you want to change zoning to be able to build more low income apartments and bring more problems to our corner of Murray? How many Police calls does Murray Police get called on around the Fire Clay apartments, And the areas around them?

How do we as the long term residents stop the building of these apartment complexes that Will increases not only traffic on an already busy street, but will bring in unwanted crime.? I do not want this here. This will drop the value of my property, increase crime, and cause more Pollution, and problems here.

You will have to hire more bus drivers to move kids to school in the mornings and evenings. I have spent my whole life working on to keep my neighborhood clean and safe for my family and those of my neighbors around me.

I am not against progress, I am against reckless progress, and City managers not looking out for all of your fellow Murray residents.

The tax revenue will not out way the cost of getting the tax revenue. This is a lose, lose, choice.

Ryan Pollick (Proposed zoning change at 861 E. Winchester Street) Read by Jan Lopez

My name is Ryan Pollick and I am the President & CEO of Utah Power Credit Union, whose headquarters building has been located at 957 E. 6600 S. for just over 10 years.

I appreciate the efforts of the Economic Development Department and the Council in bringing new businesses to the City while considering impacts to residents and businesses.

My comments are regarding the proposed change in zoning from General Commercial to Mixed Use for properties located at 861 E. Winchester Street and 6520,6550,6580 S. 900 E.

- The current traffic in and leading to the intersection of 900 E. and Winchester St. / 6600 S. is severe. Over the last few years, particularly during afternoon rush hour, cars accessing 1215 can back up on 6600 S. nearly to 900 E. There are safety issues as a result of this traffic both for pedestrians and motorists. Increased traffic will significantly affect the accessibility to businesses in this corridor.*
- There has also been an increase in crime in this area, mostly due to the Studio 6 Motel east of our building. We have had to call the police countless times in the last year, and while I am very pleased with the police department and the officers once they respond, there have been many times when an officer is not available to respond during a security incident. I am concerned that adding this many housing units will stretch the resources of the police department even further and negatively impact the overall safety and security of current residents and businesses.*

Before a decision is made on this amendment, I urge you to conduct extensive analysis regarding the impact this development will have on traffic, security, safety, other City services and resources, and create a thorough action plan to mitigate these risks.

Thank you for all you do in making Murray City a very favorable place to do business.

Verl Greenhall (Amending General Plan for RC Willey property) Read by Pattie Johnson

Please do not change or amend the General Plan from Commercial to Mixed Use nor amend the Zoning Map from C-D to M-U concerning the property known as RC Willey Properties located at 561 E. Winchester St and the adjacent property along 900 E.

I recognize the Planning Commission's concern over having another "Big Box" building setting vacant, and finding another tenant using the same building(s) could prove futile. However, by changing the General Plan and Zoning map to the Mixed Use, the potential of medium to high density housing being placed there-on is probable.

With a M-U zone, multi-family housing could be placed within 10' of adjacent properties and be 45' high. That would cause a great disadvantage to the Murray City residences that have homes along the North and West boundaries of this property. Our home would lose all view of the mountains and very little sky...like maybe 5'. (I realize that may sound like an exaggeration, but please take a site visit to our home to verify how realistic 5' is.)

As Murray City Public Works will verify, the Storm Water drainage system is insufficient to handle the existing load, let alone any additional pressure put on the system. As is, when there is a high rate per minute rain storm, the RC Willey parking lot acts like a catch basin and absorbs the shock to the system. With small, medium and high density

housing, that detention effect would be lost.

Leaving the Mater Plan as is would still allow an alternate development of the said property, using the "Conditional Use" alternative. By changing the plan, as long as a developer meets the conditions of the changed zoning, they would have a legal right to build whatever the zone allows. That takes all citizen input away.

I think if there is at least 75' of buffer from the existing homes and commercial properties and the storm water system is addressed, some multi-family housing could be located on this property. However, if the code is changed, any chance for public comment on the future development would be lost.

As an alternative, a nice neighborhood park...graded to perform the storm water detention element, and some joint tenant fooderies along 900 East and Winchester Blvd would add to the neighborhood and Murray City as a community. A concept that is now showing some very big promise is to have several small attached commercial units where Mobile Food Trucks can bring their cuisine to sell to the public. This kind of concept would blend very well with the exiting zoning, and provide greater tax dollars to Murray City than property tax from multi-family housing.

Please vote no on the proposed re-zoning.

Joe Hillock (Re-zoning RC Willey) Read by Pattie Johnson

As a home owner on the northwest corner of the RC Willey property I would like to state as public comment for this Murray City Council agenda item that I agree with Verl Greenhalgh's attached comments.

Carrie Roberts (Residential Chickens) Read by Jennifer Kennedy

I would like my comments added to the record regarding residential chickens.

Residential chickens benefit our communities in many ways. Chickens are opportunistic omnivores which is great for eating both weeds in yards, invasive bugs, and mice, moles and voles. The cute noises that chickens make are no more invasive than any other wild bird found in our neighborhoods. Hens rarely make loud noises and mostly make quieter clucks. Louder noises made by a hen would be an alarm for predators and the owner would want to check on their flock and ensure that enclosures are predator proof. Residential chickens produce eggs with increased nutrition value when compared to factory eggs. Chicken excrement is amazing for composting and is a nutrient rich source for gardens and flower beds. A well kept chicken area shouldn't cause a smell for neighbors. Chicken coops have an odor but only when approaching the coop or harvesting eggs. There are many natural and chemical products to be added to coops to scent the coop and get rid of mites. Chickens do not draw insects or pests, due to their opportunistic omnivores nature. In other words bugs or mice that try to eat chicken feed will be eaten. Wild birds may snack on chicken feed however chickens will rarely mess with other flighted birds as they are not seen as a threat or food source.

Murray neighbors looking to get chickens need to see this opportunity as having a great outdoor

pet that makes breakfast and not as a business opportunity or food supply. No one wants a chicken factory next door and the proposed ordinance covers the approved numbers per square foot. I would recommend people do their research and look at chickens that are best suited for their situation. People with children want to consider more domestic breeds like Plymouth Rock, Cochin or Brahma. People need to understand that their pet chickens have personalities and will bond with them. Chickens who no longer produce eggs should not be released or surrendered to an animal facility. Just like the family dog or cat we have the responsibility to protect and care for these animals throughout their lifespan.

I vote in support of responsible residential chickens and I hope anti chicken residents will see that the chickens will not disturb their property.

Jessica Kyle (Zoning change for Sports Mall, RC Willey, and 5300 South State Street) Read by Jan Lopez

I am writing regarding the zoning change requests for the Sports Mall, RC Willey, and 5300 South State Street Properties. I do not live directly next to these locations but have lived in Murray city for over 40 years so I am familiar with the properties. I understand the need to update these locations to allow for different types of commercial business because Murray City has an abundance of big box store locations that are closing. The city needs to adapt to changing economic conditions that no longer support such store locations.

The purpose of my letter is not to be in favor or against these zoning changes. I have two questions for the economic development staff and the Council;

First, what is the purpose of the Murray City Master plan if it is rarely followed? All three of these properties are zoned commercial in the Master Plan, not mixed use. I have followed planning commission meetings quite closely over the last few years and it is quite apparent that whenever there is a zoning change request, whether in alignment with the master plan or not, it gets approved. It gives the appearance that the city is chasing money rather than making changes for the long-term benefit of the community.

Second, what is the long-term plan for high density/mixed use housing? Again, based upon changes recommended by economic development staff, almost every time a piece of land opens up that is large enough for a high-density condo complex or mixed-use development, it gets approved. There does not appear to be any long-term strategy on where these types of developments should be in the city or how many. Again, it creates the appearance that the city is chasing money.

Finally, it is frustrating that necessary data about these developments is not provided to the Council prior to voting. For example, in the previous Committee of the Whole meeting, Council members asked about the impact to traffic for these developments. The response was, we do not know, we assess that after the fact. This is like putting the cart before the horse. Bad information, or lack of information equals poor city planning.

I appreciate your service to the Murray City.

Jessica Lucero (Diversity and Inclusion ad hoc advisory task force) Read by Pattie Johnson

I am in favor of a Resolution Establishing the Diversity and Inclusion Ad Hoc Advisory Task Force. Our city and state's demographics are changing. In order to be more inclusive and provide equitable access, Murray needs to examine its programming, policies, and procedures. Creating this Task Force will establish that Murray cares for all its citizens and that members of the Board are working to understand the differing needs of the constituency in its entirety, especially those underrepresented and who do not often have a seat at the table. Examining policies and actively creating opportunities to engage with and listen to constituents is vital to the future of our community. Please vote yes to this resolution.

Tim Richardson (Re-zoning RC Willey) Read by Jennifer Kennedy

Please vote no on the proposed re-zoning for the land located at the RC Willey location.

My obvious concern is because I live next to the property under review (behind Make A Wish).

While my experience with Make A Wish was overall positive and felt the city was concerned about the residential effect, there was still some things that slipped through the cracks of what was agreed on and then implemented. So this is a concern for me for the RC Willey space.

I would ask this council to think first of the people instead of the money aspect for the city. People and residents make Murray of what it is and has become. There are so many people that return to Murray because of its value and people that reside here. If the decision makers for Murray are concerned only for money then we will be one of those cities that has its heyday and then people will move on because the value of home and quality of living decreases.

I am one of those people that has lived here mainly my whole life because of the tight knit community. If this decision goes the wrong way in pursuit of financial goals instead of community and residential goals I guarantee this will impact the long term commitment of the residents and eventually lead into high turnover residents.

I am directly concerned about my view and would have you refer to Verl Greenhalgh's document where he so eloquently explained the distance desired. Also I am very concerned with making my street a through street instead of dead end. This will definitely impact my decision as a long term resident.

Thanks for listening to my concerns and would ask you to think sincerely about the best for the community and not for the money. Take a second to think if this change is something you would not mind being your neighbor.

William Paul Miller (Diversity and Inclusion Task Force) Read by Jan Lopez

My name is William Paul Miller and I live in Murray. I just wanted to voice my support for the resolution adopting the Diversity and Inclusion Task Force. Upon moving to Utah, my wife and I were warned by former residents of Utah that we may not feel accepted or welcome in Utah. Thankfully, my family has not felt any sort of exclusion since moving to Utah and I believe that is due in large part to the broader Murray community. I want ALL of Murray's residents to feel included and have access to services that I have enjoyed as a Murray resident.

Beau Pili (Murray City Center District) Read by Jan Lopez

As I have looked through the proposal I have a few questions that you might have answered in past meetings, but would like some clarification.

Ms. Lopez clarified that this has to do with the Murray City Center District.

*What is the percentage of Affordable housing units?
What is the impact on the surrounding schools? Fire Dept.? Police Dept?
What is the impact on the flow of traffic?
What is the appeal to higher net worth individuals?
Why are we using an out of state developer and not a local?*

Due to time constraints today, I am not able to provide some ideas and thoughts with my questions, but will at a later time.

Thank you for your time and I look forward to the responses.

Ms. Lopez has forwarded this to Melinda Greenwood who will follow-up with Mr. Pili.

Richard Seiger (Re-zoning on Winchester and 9th) Read by Jennifer Kennedy

Many of the properties on Winchester and 9th East surrounding the RC Willey properties have been re-zoned as RNB from residential and the resulting buildings have mostly been a net positive for the area. Changing the properties in question from C-D to M-U is a little different, but still asks the same question. What type of zoning and buildings do we want to provide as a buffer to residential zoning. M-U zoning has the potential to offer a lot to the surrounding residences, but also can result in a lot of harm. I'm sure many of my neighbors will bring up issues such as the problematic storm water drainage between Labrum Ave and RC Willey and the fact that the surrounding houses are generally lower in elevation than the RC Willey property. The elevation difference has real potential for light, noise, and visual pollution from any new structures. The elevation difference will also exacerbate the minimal stand-off distance required between the zoning transition. Ten feet or 25 feet will probably not seem adequate when you're towering over the adjacent houses. Traffic congestion/safety is also a concern when you transition from one lightly trafficked furniture store to a mixed use zoning with the potential for high density housing.

I realize that this meeting is merely to discuss the zoning and not the design of any proposed structures, however the location and size of the properties require us to think of the potential impact of this change and the vision of the involved land developers.

Finally, I noted in 17.161.060 for mixed use height regulations that the document does not limit the authority of the planning commission or community and economic development staff to place additional restrictions on the building material, design, etc. based on the surrounding land use. I would urge those bodies to take that ability into consideration with this proposed

land development.

Moses Rogers Read by Jan Lopez

*We own one of the houses directly East of the sports mall's bubble.
We have been given no information on the plans for the property or how it will be used or changed with the new zoning, and how that will ultimately affect our property values and enjoyment of our property.*

What is being proposed that would require the zoning change?

Ms. Lopez indicated she would follow-up with Mr. Rogers.

Amanda Rogers (Sports Mall) Read by Jennifer Kennedy

My family and I live in a house on Revere Drive, directly behind the Sports Mall. We have lived here for 18 years.

If the Sports Mall property is sold to developers for apartments, how large will the complex be, ie how many stories tall?

Will people be able to peer down into our yards and invade our privacy?

Will this development block my view of the sun in the evening?

Cast unwanted shadows on my property?

Light pollution in the night?

How will all of this affect the homes directly behind the land?

Consent Agenda

1. Consider confirmation of the Mayor's re-appointment of Todd Allen to the Murray City Ethics Commission for a three-year term beginning February 19, 2021 to expire February 19, 2024.
2. Consider confirmation of the Mayor's re-appointment of Susan Gregory to the Murray City Ethics Commission for a three-year term beginning February 19, 2021 to expire February 19, 2024.
3. Consider confirmation of the Mayor's re-appointment of Richard Clark to the Murray Senior Recreation Center for a three-year term beginning February 1, 2021 to expire January 30, 2024.
4. Consider confirmation of the Mayor's re-appointment of Sandra Jones to the Murray Senior Recreation Center for a three-year term beginning February 1, 2021 to expire January 30, 2024.
5. Consider confirmation of the Mayor's appointment of Karl Schatten to the Murray Senior Recreation Center for a three-year term beginning February 1, 2021 to expire January 30, 2024. Karl will replace Jenny Martin.

Presenting: Mayor Camp

Mayor Camp expressed his appreciate for the four re-appointed individuals and the one new appointee. All re-appointed individuals are starting their second term and Mr. Schatten will be replacing Jenny Martin who served on the Senior Recreation Board for two terms.

Councilmember Cox turned the time over to Karl.

Mr. Schatten shared that he has been interested in what's going on in the city and he sees this appointment as an opportunity to give back to the Senior Center, which he has been a member of for the last six years.

MOTION: Councilmember Turner moved to approve the Consent Agenda. The motion was SECONDED by Councilmember Martinez.

Council roll call vote:

Ayes: Councilmember Dominguez, Councilmember Turner, Councilmember Hales, Councilmember Martinez, Councilmember Cox

Nays: None

Abstentions: None

Motion passed 5-0

Public Hearings

Staff and sponsor presentations and public comment will be given prior to Council action on the following matters.

1. Consider an ordinance relating to land use; amends the Zoning Map from G-O to C-D for the properties located at approximately 192 East 4500 South, Murray City, Utah.
(Sew N Fit applicant.)

Staff Presentation: Melinda Greenwood and Jared Hall presenting

Mr. Hall shared information about Saeld Ahar with Sew-N-Fit's application to amend the Zoning Map for the property located at 192 East 4500 south and change from a G-O (General Office) to a C-D (Commercial Development). The zone change is supported by the General Plans Future Land Use Map and on November 19, 2020 there was a public hearing Planning Commission meeting held on behalf of the application. Based on the findings of that meeting, the city staff and Planning Commission recommend the City Council approve the request.

The public hearing was open for public comments. No comments were given, and the public hearing was closed.

Mr. Ahar joined the meeting and asked if the council has any questions for the applicant. No questions were asked.

MOTION: Councilmember Hales moved to adopt the ordinance. The motion was SECONDED by Councilmember Turner.

Council roll call vote:

Ayes: Councilmember Dominguez, Councilmember Turner, Councilmember Hales, Councilmember Martinez, Councilmember Cox

Nays: None

Abstentions: None

Motion passed 5-0

2. **POSTPONED TO MARCH 2, 2021.** Consider an ordinance relating to land use; amends the General Plan from General Commercial to Mixed Use and amends the Zoning Map from C-D to M-U for the properties located at approximately 861 East Winchester Street and 6520, 6550, 6580 South and 900 East, Murray City, Utah. (Boyer Company, applicant.)
3. **POSTPONED.** Consider an ordinance relating to land use; amends the General Plan from General Commercial to Mixed Use and amends the Zoning Map from C-D to M-U for the property located at 5445 South 900 East, Murray City, Utah. (Sports Mall, applicant.)

Business Items

1. Consider a resolution approving the City Council's appointment of representatives to boards and committees.

Staff Presentation: Diane Turner

Councilmember Turner reported:

- 1) Councilmember Rosalba Dominguez appointed to the Association of Municipal Councils.
- 2) Councilmember Dale Cox and Councilmember Brett Hales to the Capital Improvement Program.
- 3) Councilmember Kat Martinez to the Utah League of Cities and Towns Legislative Policy Committee.
- 4) Councilmember Dale Cox to the Chamber of Commerce Board.
- 5) Councilmember Kat Martinez to the Economic Task Force.

MOTION: Councilmember Hales moved to adopt the resolution. The motion was SECONDED by Councilmember Dominguez.

Council roll call vote:

Ayes: Councilmember Dominguez, Councilmember Turner, Councilmember Hales, Councilmember Martinez, Councilmember Cox

Nays: None

Abstentions: None

Motion passed 5-0

2. Consider a resolution establishing the Diversity and Inclusion Ad Hoc Advisory Task Force.

Presentation: Kat Martinez

Councilmember Martinez shared two quotes; The first one was from Martin Luther King, ""Darkness cannot drive out darkness, only light can do that. Hate cannot drive out hate, only love can do that" and Congress women Ayanna Pressley, "Policy is your love language". Ms. Martinez shared these quotes because we need to put our love into action.

Ms. Martinez shared that this task force will examine and research the cities current processes, practices, and policies to ensure all residences have equal access to Murrays diverse range of services and community celebrations and events. This task force hopes to provide community members seat at the table and a forum to share their experience and offer suggestions to the council and mayor moving forward.

The floor was opened to questions. Councilmembers commended Kat for creating the Ad Hoc Advisory Task Force.

MOTION: Councilmember Hales moved to adopt the ordinance. The motion was SECONDED by Councilmember Turner.

Council roll call vote:

Ayes: Councilmember Dominguez, Councilmember Turner, Councilmember Hales, Councilmember Martinez, Councilmember Cox

Nays: None

Abstentions: None

Motion passed 5-0

Mayor's Report and Questions

Presentation: Mayor Camp

- Road construction projects have continued with this dry/warmer weather has allowed crews to sealing road cracks and sidewalk repairs.
- In Murray Park the crews will be pouring footings for Pavilion 5 construction project at the end of the week.
- The City is working on an agreement with Test Utah to set up a drive thru COVID testing location in the city. This will be set up in the Senior Parking Lot.
- Salt Lake County is planning on stabilizing the bank of the Jordan River near the Nature Center, this project has been postponed for a few weeks due to permit issues.
- Salt Lake County is dredging the creek in Murray Park near the amphitheater. This is done every few years to remove sand that builds up during the spring runoff and will continue through the week.

- Cultural Arts and History Advisory Board presented a zoom webinar last Thursday, about "Preservation it Doesn't Cost, It Pays" there were 45 participates. There is another workshop on February 17 at 6:00 pm about Energy Efficiently for Old Houses and Buildings. This webinars are limited to the first 100 participates and the workshops are advertised on our website and Facebook page.

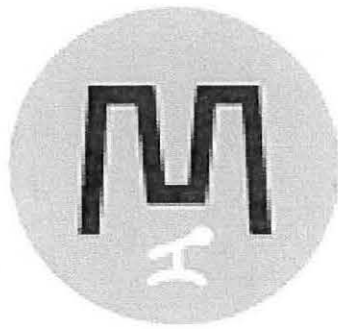
The floor was opened for questions, none were asked.

Councilmembers expressed his thanks to Jan Lopez and wished her the best of luck in her retirement.

Adjournment

The meeting was adjourned at 7:52 p.m.

Brooke Smith, City Recorder



MURRAY
CITY COUNCIL

Special Recognition #1



MURRAY

Fire Department

**Employee of the Month,
Laura Lloyd, Executive Secretary**

Council Action Request

Council Meeting

Meeting Date: February 16, 2021

Department Director Jon Harris	Purpose of Proposal Recognition of the Murray City Council Employee of the Month, Laura Lloyd, Executive Secretary
Phone # 801-264-2774	Action Requested Recognition for February of 2021.
Presenters Brett Hales and Jon Harris	Attachments Recognition form attached.
	Budget Impact None.
Required Time for Presentation 10 Minutes	Description of this Item Outstanding performance Has worked through 4 fire chiefs - trained them all! Tremendous help in budget preparation and maintenance All accounts receivable and payable Creates numerous reports including the annual report Helps proofread numerous documents
Is This Time Sensitive No	
Mayor's Approval	
Date 1/26/21	



EMPLOYEE OF THE MONTH RECOGNITION

DEPARTMENT:

Fire

DATE:

1/26/21

NAME of person to be recognized:

Laura Lloyd

Submitted by:

DIVISION AND JOB TITLE:

Executive Secretary

YEARS OF SERVICE:

26

REASON FOR RECOGNITION:

Outstanding performance
Has worked through 4 fire chiefs - trained them all!
Tremendous help in budget preparation and maintenance
All accounts receivable and payable
Creates numerous reports including the annual report
Helps proofread numerous documents

COUNCIL USE:

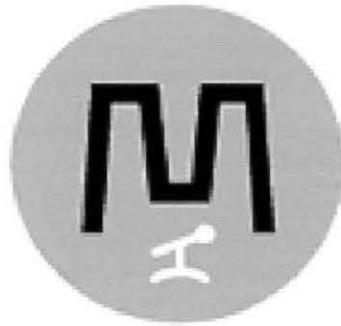
MONTH/YEAR HONORED



MURRAY
CITY COUNCIL

Citizen Comments

Limited to three minutes, unless otherwise approved by Council



MURRAY
CITY COUNCIL

Public Hearings



MURRAY
CITY COUNCIL

Public Hearing #1



MURRAY


Community & Economic Development

General Plan Amendment Fashion Place West Small Area Plan

Council Action Request

Committee of the Whole

Meeting Date: February 2, 2021

Department Director Melinda Greenwood Phone # 801-270-2428 Presenters Melinda Greenwood Jared Hall Required Time for Presentation 15 Is This Time Sensitive No Mayor's Approval  Date January 19, 2020	Purpose of Proposal General Plan Amendment to adopt a Small Area Plan for the Fashion Place West TRAX station and surrounding area. Action Requested Adoption of the Fashion Place West Small Area Plan as an amendment to the 2017 Murray City General Plan. Attachments Presentation Slides Budget Impact None. Description of this Item Background Early in 2019 Murray City was awarded a grant from the Wasatch Front Regional Council (WFRC) to study the area around the Fashion Place West TRAX Station and develop a Small Area Plan. Small Area Plans are documents intended to help guide growth and inform land use decisions within a specific area. Document Organization The document is divided into six (6) chapters: <ol style="list-style-type: none">1. Executive Summary and Implementation - This chapter highlights the goals, existing conditions, strategies for housing and connectivity, and includes a framework for implementation.
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Continued from Page 1:

2. Existing Conditions - This chapter outlines the current conditions and challenges that the neighborhood faces, including barriers to potential development along core streets.
3. Housing - This chapter divides the study area into 4 sub-areas including to carefully identify the different neighborhoods and help the City tailor any approach to redevelopment and reinvestment.
4. Connectivity - This chapter focuses on opportunities for future improvements to the connections between the Fashion Place West Station and the retail center around the Fashion Place Mall, and on better pedestrian and bicycle facilities throughout the study area.
5. Design Guidelines - This chapter provides simple guidelines to establish an appropriate and human scaled development pattern as ordinances change and redevelopment opportunities come.
6. Appendix - The appendix section addresses the public engagement and provides some case studies.

Public Notice and Planning Commission

A total of 897 notices were sent to all property owners within the proposed Small Area Plan study area, property owners within 500' of the proposed plan, and affected entities. A number of comments were received expressing concerns about additional density, height, and traffic resulting from potential changes to the area. The Planning Commission held a public hearing for this item for this item on December 17, 2020. The Planning Commission voted 7-0 to forward a recommendation of approval to the City Council based on the findings below.

1. The Murray City General Plan provides direction in implementation through five key initiatives.
2. The requested General Plan amendment has been carefully considered based on public input and review or city planning best practices.
3. Chapter 3, Framework for the Future, of the Murray City General Plan calls for the development of Small Area Planning projects along rail transit-oriented developments.
4. The proposed General Plan amendment is consistent with the Goals & Initiatives of the Murray City General Plan
5. The proposed small area plan will provide Murray residents, staff, elected officials, and the development community clear guidance as to how the City anticipates development within the subject area.

Recommendation

Based on the background, analysis, and the findings within this report, Staff and the Planning Commission recommend that the City Council adopt the Fashion Place West Small Area Plan as an amendment to the 2017 Murray City General Plan.

Fashion Place West Small Area Plan

General Plan Amendment to Adopt the Plan

Roughly 6100 South to 6790 South and I-15 to State Street



SMALL AREA PLANNING PROJECTS



REGIONAL CENTERS

Located at existing or future regional retail or employment centers and their surrounding context. Including:

- 4500 South/State Street
- IMC/Murray High
- I-15/5300 South
- Fashion Place Mall



COMMUNITY CENTERS/NODES

Located at existing or future city, retail, or employment centers. Including:

- Downtown Murray/City Center
- TOSH
- 4500 South/500 West
- 4500 South/700 East
- 4800 South/900 East
- 900 East/5600 South
- 900 East/5900 South
- 900 East/Winchester



NEIGHBORHOOD CENTERS/NODES

Located at existing or future key intersections within neighborhoods. Including:

- 1300 East/5600 South
- 1300 East/5900 South
- 600 East/Creekview Cr.
- Vine St/Glenn St
- 700 West/5900 South
- 700 West/Winchester St
- Jordan River Parkway/5300 South
- Jordan River Parkway/Winchester St



RAIL TRANSIT ORIENTED DEVELOPMENTS

Located at TRAX and FrontRunner Stations and up to 1 mile around. Including:

- Murray North
- Murray Central
- Fashion Place West



BUS RAPID TRANSIT VILLAGE NODES

Located at major intersections along State Street. Including:

- 4500 South
- 4800 South
- Vine Street
- 5300 South
- 5600 South
- 5900 South
- Winchester Street



WASATCH FRONT REGIONAL COUNCIL



TRANSPORTATION AND LAND USE CONNECTION

The Transportation and Land Use Connection (TLC) program is a partnership between the Wasatch Front Regional Council (WFRC), Salt Lake County, Utah Department of Transportation (UDOT), and Utah Transit Authority (UTA).

The TLC program provides technical assistance to local communities to help them achieve their goals and plan for growth. The program helps communities implement changes to the built environment that reduce traffic on roads and enable more people to easily walk, bike, and use transit. This approach is consistent with the Wasatch Choice Vision and helps residents living throughout the region enjoy a high quality of life through enhanced mobility, better air quality, and improved economic opportunities.



VODA

landscape +
planning



Millcreek City Center Masterplan



Regent Street Urban Design & Placemaking





Mark Morris, PLA,
LEED-AP, ASLA
Founding Partner



Annaliese Eichelberger
Project Manager



FINDINGS

Based on the analysis of the proposed small area plan and review of the Murray City General Plan, staff concludes the following:

1. The Murray City General Plan provides direction in implementation through five key initiatives.
2. The requested General Plan amendment has been carefully considered based on public input and review of city planning best practices.
3. Chapter 3, Framework for the Future, of the Murray City General Plan calls for the development of Small Area Planning Project along rail transit-oriented developments.
4. The proposed General Plan amendment is consistent with the Goals & Initiatives of the Murray City General Plan.
5. The proposed small area plan will provide Murray City residents, staff, elected officials, and the development community clear guidance as to how the City anticipates development within the subject area.



Planning Commission Meeting

December 17, 2020

- **1,000 public notices mailed to all property owners within the study area and within 500'.**
- **Planning Commission voted 7-0 to recommend APPROVAL based on the findings:**
 - ✓ The Murray City General Plan provides direction for implementation through five key initiatives.
 - ✓ The requested General Plan amendment has been carefully considered based on public input and review of city planning best practices.
 - ✓ Chapter 3, Framework for the Future, of the Murray City General Plan calls for the development of Small Area Planning Project along rail transit-oriented developments.
 - ✓ The proposed General Plan amendment is consistent with the Goals & Initiatives of the Murray City General Plan.
 - ✓ The proposed small area plan will provide Murray City residents, staff, elected officials, and the development community clear guidance as to how the City anticipates development within the subject area.



Recommendation

Staff and the Planning Commission recommend the City Council **APPROVE** the adoption of the Fashion Place West Small Area Plan as an amendment to the 2017 Murray City General Plan.





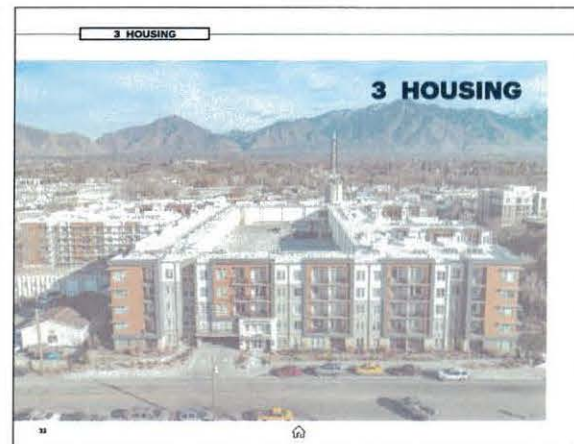
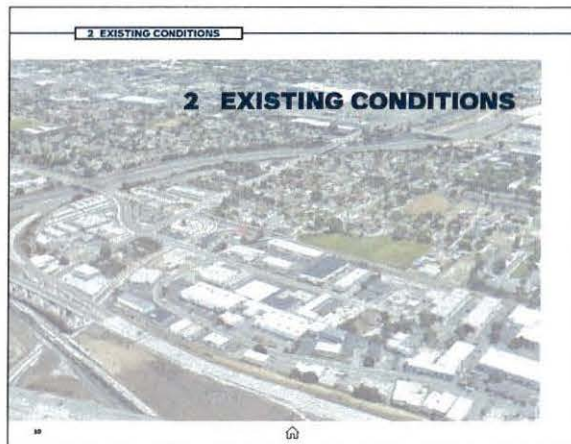
FASHION PLACE WEST SMALL AREA PLAN

Final Draft Discussion

2 EXISTING CONDITIONS



DOCUMENT STRUCTURE



SUMMARY

Planning Take-aways from the Small Area Plan:

1. Neighborhood changes must be context sensitive.
2. City does not own significant land in the area, all development will be a partnership with landowners or involve property acquisition.
3. Infrastructure improvements needed along Winchester to improve walkability and active transportation uses.
4. Concentration of new development near TRAX station will create more neighborhood-scale services, housing, and public spaces.
5. Begin conversation about conversion of Fashion Place Mall to a mixed-use center, with housing, jobs, and office uses.
6. Housing demand in the region is going to continue to increase, and locations with quality transit service near jobs are the right place to locate more housing options.

GOALS & VISION

The following goals for the study area were established through the small area planning process:

- A. Strengthen relationship between TRAX station and Fashion Place Mall.
- B. Improve transportation connectivity for the neighborhood.
- C. Improve overall neighborhood quality.
- D. Promote transit use and active transportation.

6 APPENDIX

6.1 PUBLIC ENGAGEMENT

6.1.1 OPEN HOUSE

On February 12, 2020 Murray City along with the consultant team, held a public open house at the Clark Cushing Senior Center, located within the northern portion of the study area. The objective of the open house was to educate the public about existing conditions in the area and the goals of the Fashion Place West Small Area Plan, as well as to gain feedback and insight from the participants about many key components. A series of ten boards and individual questionnaires were used to inform, and gather feedback.

Among the approximately 35 individuals that participated, half said that they lived in the study area, and the other half were commuters or Murray residents. Most participants had positive reactions to the planning process, while also expressing their desire for better connectivity in the area, which aligns well with the City's vision for the Small Area Plan.

The most frequently asked question from participants was, "What development is being proposed?" Staff and the consultant team educated residents about the need for a long range plan for this area, even though there was no development proposed, or on the horizon.

When participants were asked which of Murray's five key initiatives (established in the General Plan) seem most related to this neighborhood, many felt that Livable and Vibrant Neighborhoods and Multi-Modality were most applicable.

The questionnaire asked respondents about their impressions of the study area and what they have experienced, and would like to see changed.



Approximately 35 individuals participated in the Open House at the Clark Cushing Senior Center.



When asked what types of destinations are most desired in the neighborhood, the most common responses were:

- Public space/parks
- Dining
- Grocery/market

When asked what type of housing is most desired, the majority of respondents answered:

- Single-Family Home
- Townhome
- Accessory Dwelling Unit (ADU)



6.1.2 SURVEY

While originally scheduled to hold a second open house, due to safety concerns related to the COVID-19 pandemic, City staff and the consultant team conducted an online survey from May 20th through June 20th. Residents, commuters, shoppers, and other interested parties were invited to participate by answering a series of 18 questions. The survey was advertised through social media channels and received over 130 responses.

The goal of the survey was to gauge respondents' understanding of the components of the Small Area Plan, and attitude for more specific recommendations dealing with connectivity expansion, housing options, and design guidelines.

A number of survey questions stood out as good indicators of concerns that residents have and what they would like to see more of. Those included:

- What four words would you use to describe the attributes of the Fashion Place West neighborhood?
- What is your primary destination when you visit the neighborhood?
- What do you see as challenges facing the neighborhood?
- What types of housing do you wish were available?
- What housing issues do you feel exist in the neighborhood?

A majority of respondents appreciate the convenient and central location of the Fashion Place West neighborhood. When asked questions regarding access for bicycles and pedestrians, many respondents expressed desire for better sidewalks and more bicycle lanes. A common concern throughout the survey responses was around traffic in the Fashion Place West neighborhood, and the area becoming busier. Because of this concern, staff and the consultant team felt it important to address the effects of future growth on



Question 4: What four words would you use to describe the attributes of the Fashion Place West neighborhood?

When respondents were asked about the types of housing that they wished were in the neighborhood, many felt that mid-density housing types such as cottage clusters, ADUs, and duplex/triplex units would make a good addition. When asked about housing issues they felt the study area faced, many respondents expressed the need for more housing affordability, and construction quality.

Overall, the survey was a key component to the public engagement approach, giving residents a safe and healthy avenue to express their concerns and ideas about the future of the neighborhood.



3 HOUSING

INCOME NEEDED TO BUY A \$400,000 HOME IN SALT LAKE COUNTY 2020



Source: UtahRealEstate.com | Ridge Home Loans

Figure 3.6: Graphic showing necessary household income to purchase a home in Salt Lake County.

study also shows that new construction decreases the average income of people moving to the area by approximately 2 percent, as well as the number of people moving to the area who are from very low income neighborhoods by almost 3 percent. This is due to the fact that new buildings reduce costs in lower segments of the housing market.

Another misconception about the construction of new market-rate housing in a lower income neighborhood is that this development contributes to or initiates gentrification. The Upjohn Institute study found that new construction actually tends to occur after a neighborhood has already begun to change, or gentrify. The end result is the eventual accommodation of pre-existing demand, diverting high-income households from nearby units and reducing rents, instead of signaling that a neighborhood is now desirable.

Murray City should adopt strategies that encourage housing



Figure 3.7: With the projected increase in population over the next 20 years, market-rate and more income-dependent housing options will be important to maintaining affordability.

development. Regulatory restrictions on housing development can lead to higher rents, and faster home price growth. This leads to fewer people moving into economically successful areas. Strategies that promote residential construction foster more economically integrated neighborhoods, which also promotes economic mobility and housing options for low income residents. Market-rate housing construction not only improves regional affordability, but also neighborhood affordability.

3.2.2.4 ENERGY PRICES

In a world of higher energy costs, it will be essential to consider the combined costs of housing, transportation, and utilities—to ensure that families have adequate residual incomes to afford other necessities. This in turn suggests the importance of policies and practices that help to reduce these combined costs, for example, by ensuring the availability of affordable homes near public transit and job and retail centers—so



HOUSING ANALYSIS

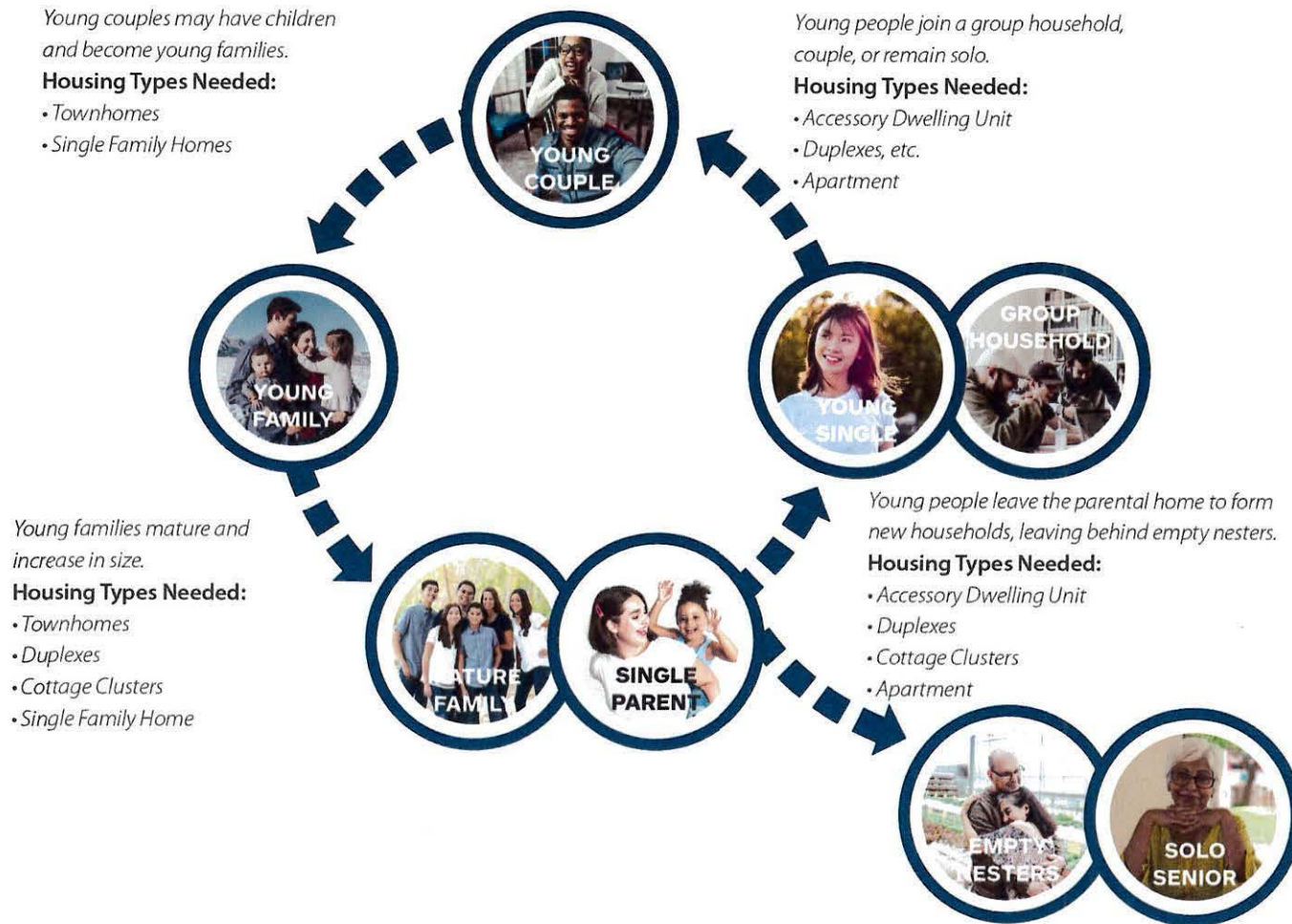
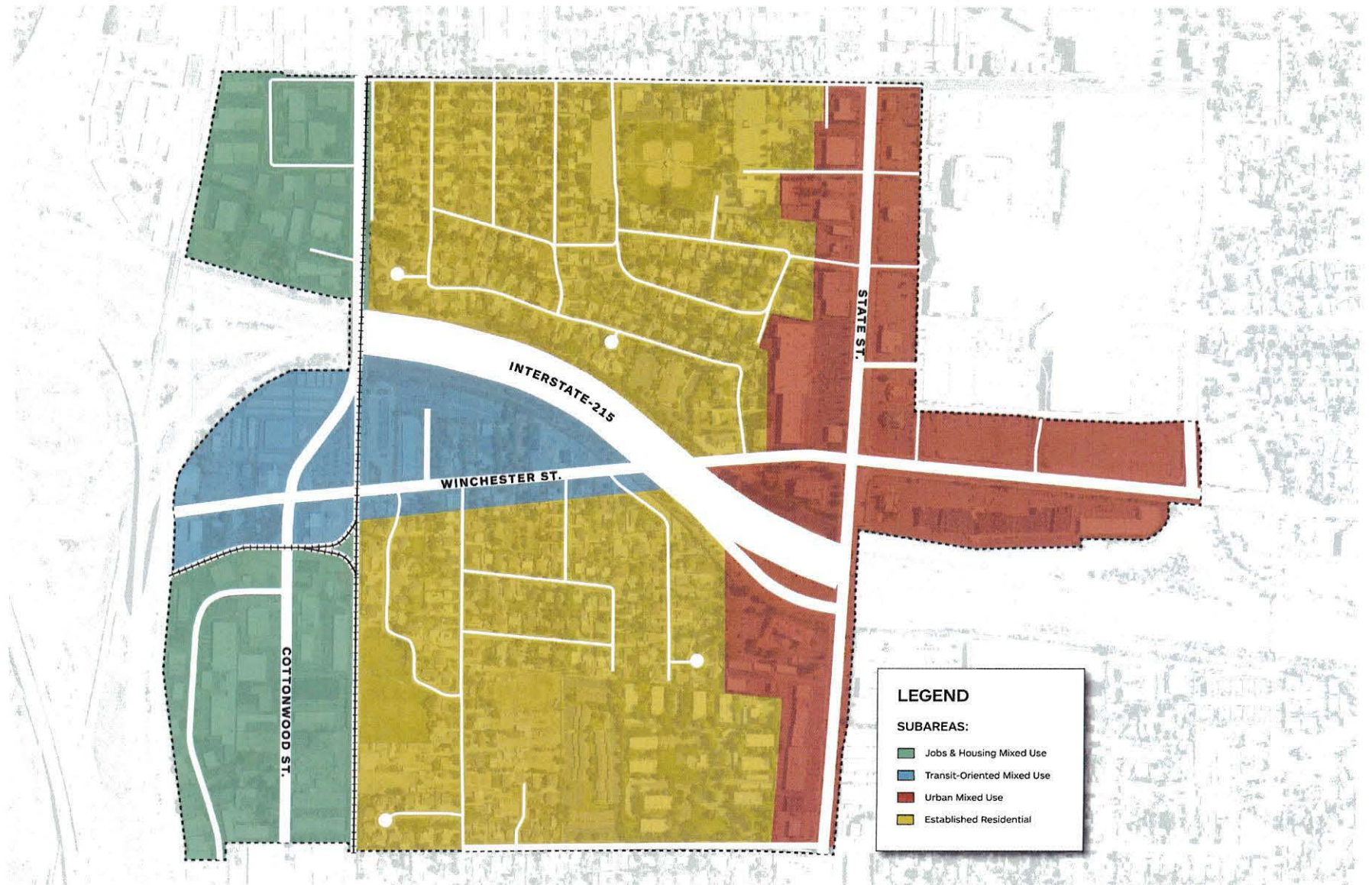


Figure 3.8 Life cycle housing is a strategy to ensure that all households have access to housing choice in their neighborhood throughout their lifetime.

SUBAREAS





3.8 HOUSING IMPLEMENTATION STRATEGY

This implementation strategy weighs current market conditions, regulations, and best practices. These important factors help to identify and outline clear priorities and policy amendments that will improve housing development and opportunity within the study area.

3.8.1 HOUSING PRIORITIES

In order to expand housing choice in the study area, the following priorities have been identified:

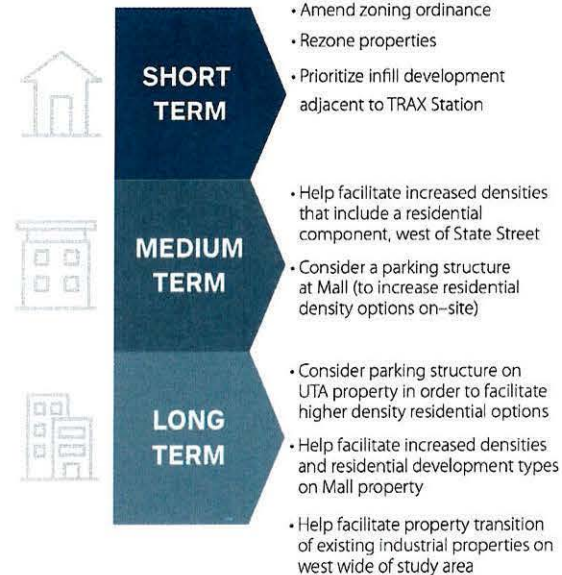
1. Offer services and amenities near housing.
2. Provide housing for all stages of life.
3. Create a walkable neighborhood.
4. Increase residential allowable densities for development along and adjacent to the Fashion Place West TRAX station, I-15, and State Street, by increasing parking densities using structured parking in conjunction with mixed-use developments.
5. Address established residential neighborhoods by creating responsible transitions between existing residential and new, higher density developments.
6. Incorporate a mix of uses into new residential developments as well as existing single-use zone districts.

3.8.2 POLICY UPDATES AND LAND USE AMENDMENTS

Policy changes the City can implement will begin the process of change for the study area, including the following:

1. Create new Fashion Place West zone district (FPW) modeled off existing TOD zone with the following revisions:
 - (a) Parking
 - (i) Include shared parking provision.

- (ii) Reduce residential requirements contingent upon proximity to TRAX station, shared parking calculation, etc.
 - (iii) Implement parking maximums.
 - (b) Reduce front yard setback from 15 feet and 25 feet, to 0 feet
 - (c) Implement maximum setback requirements.
 - (d) Consider a decrease of open space percentage requirements from 20 percent to 10 percent.
 - (e) Ground floor activation, requirements, and language.
2. Re-zone areas within the study area per recommendations of the General Plan.



CONNECTIVITY ANALYSIS

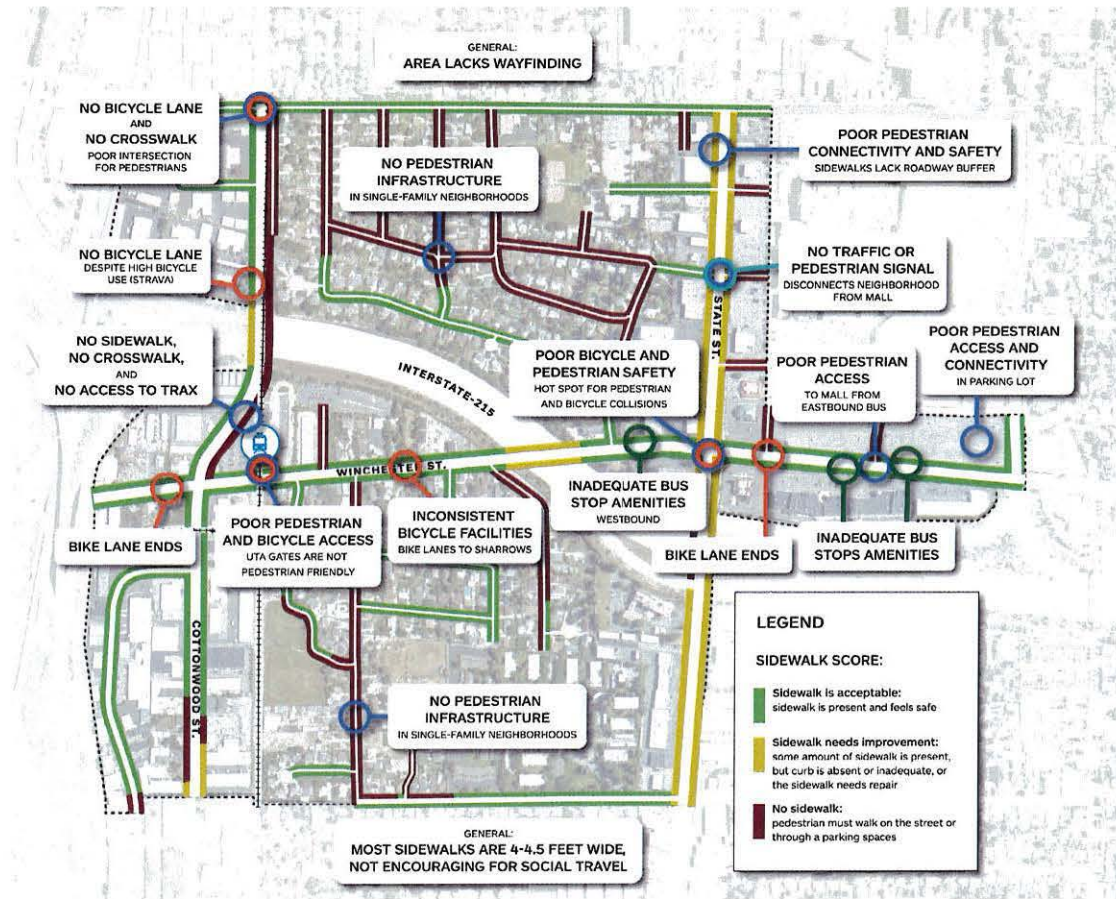


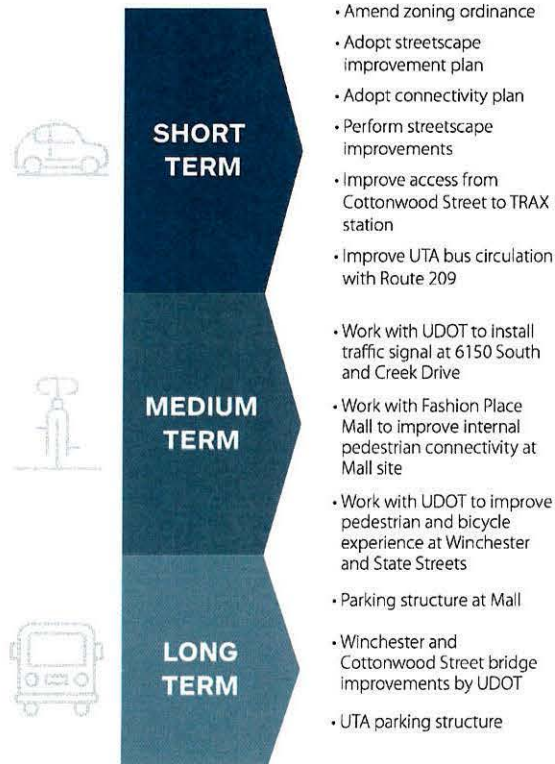
Figure 2.22 Walkability infrastructure scoring for the study area.



4 CONNECTIVITY

4.7 CONNECTIVITY SUMMARY AND IMPLEMENTATION STRATEGY

The Connectivity section of the Small Area Plan considers current transportation and mobility in the study area, planned improvements, and best practices. These factors were used to identify and outline clear priorities and policy amendments to improve future transportation within the study area.



4.7.1. CONNECTIVITY PRIORITIES

1. Improve overall active transportation connectivity between residential neighborhoods, TRAX station, and Fashion Place Mall
2. Modify UTA Bus route 209 to be a circulator between the TRAX station and Fashion Place Mall
3. Develop parking strategy
4. Adopt streetscape improvement plan to ensure future connectivity in key areas:
 - (a) Winchester
 - (b) Cottonwood
 - (c) Intersections
 - (d) Fashion Place Mall access

4.7.2. POLICY UPDATES AND LAND USE AMENDMENTS

1. Create new Fashion Place West zone district modeled off of existing TOD zone with the following revisions:
 - (a) Parking
 - (i) Include shared parking provision
 - (ii) Reduce residential requirements contingent upon proximity to TRAX station, shared parking calculation, etc.
 - (iii) Implement parking maximums
 - (b) Reduce front yard setback from 15 feet to 25 feet, to 0 feet
 - (c) Implement maximum setback requirements
 - (d) Decrease open space percentage requirements from 20 percent to 10 percent
 - (e) Ground floor activation, requirements, and language
2. Re-zone areas within the study area per recommendations of the General Plan



5 DESIGN GUIDELINES

5.4.3. PROMINENT ENTRANCES

Building entrances should be designed to readily inform people of their access and use.

- The primary (third) building facade and main entry of nonresidential buildings should be well-marked, articulated and oriented and facing the primary public street.
- Consider placing the main building entrance at a street corner.
- Entries should be lighted and protected from weather.
- Entries facing public streets should be made visually prominent and receive architectural emphasis. A variety of techniques to accomplish this include:
 - Recessed entries
 - Projecting entries
 - Elevated entries with call ways for residential uses
 - Entry-related cover or roof-line articulation (such as canopy, articulated pendant-roof articulation)
 - Arched entries
 - Decorative linels of masonry above doorways
 - Landscaping treatment and emphasis
 - Surface treatment (such as paving or tiles)
 - Entry courtyard
 - Transoms windows
 - Signage
 - Other techniques as appropriate



Figure 5.4.3.1: Building with a prominent entrance featuring a recessed canopy and a small courtyard.



Figure 5.4.3.2: Building with a prominent entrance featuring a projecting canopy and a small courtyard.

FASHION PLACE WEST SMALL AREA PLAN

5.3.4. PEDESTRIAN CONNECTIONS

Safe pedestrian passage should be provided through any large blocks of parking lots to provide convenient and direct pedestrian connections, and to provide neighborhood-scale open space.

- Formalized mid-block pedestrian corridors or connections between public rights-of-way through the blocks and redevelopment sites on 100-150 foot intervals are highly encouraged, with at least one through-block connection for any block face longer than 500 feet.
- All new mid-block connections and new ones should include:
 - A 5 foot minimum building setback on either side of the connection, which could include landscaping, lighting, and other pedestrian amenities.
 - A 6 foot 7 inch minimum sidewalk and
 - Appropriately scaled pedestrian lighting.
- Walkways should be paved with a differentiated pavement surface treatment to alert drivers to the pedestrian right-of-way and potential presence of pedestrians. Speed tables may be installed as appropriate to further calm vehicle traffic.
- Alternative building entrances are encouraged to be located on pedestrian connections and alley ways to provide a building face along such pathways.
- Access from the street should include wayfinding signage to nearby pedestrian or the facility.



Figure 5.3.4.1: Mid-block pedestrian connection with a building setback and landscaping.



Figure 5.3.4.2: Mid-block pedestrian connection with a building setback and landscaping.

FASHION PLACE WEST SMALL AREA PLAN

5 DESIGN GUIDELINES

5.3.3. LIGHTING

Lighting should ensure a contribution to the character and safety of the streetscape and public spaces, but not disturb adjacent developments and residences.

- Use City-approved standardized fixtures for sidewalk lighting.
- Fixtures should be consistent with adopted light fixture for the study area.
- Lighting elements throughout and surrounding the site should be complementary, including pedestrian pathway, access, parking lot lighting, lighting of adjacent developments, and the public right-of-way.
- All lighting should be shielded from the sky and adjacent properties and structures, either through exterior full cut-off shields or through optics within the fixture.
- Lighting used in parking lots should not exceed a maximum of 16 feet in height. Pedestrian-scale lighting should be a maximum of 16 feet in height.
- Parking lot lighting should be appropriate to create adequate visibility at night and evenly distributed to increase security.



Figure 5.3.3.1: A street scene showing various lighting fixtures, including streetlights and building lighting.

FASHION PLACE WEST SMALL AREA PLAN

5 DESIGN GUIDELINES

5.3 SITE DESIGN

5.3.1. BUILDING PLACEMENT

To support and encourage pedestrian comfort, convenience and activity, buildings should create a sense of enclosure within the street corridor by establishing a direct relationship between buildings and sidewalk.

- Commercial and mixed-use buildings should be built along the back of the sidewalk on all Type I and Type II streets, adjacent to any public plaza, courtyard, seating area, or other space intended for public use.
- Multi-family buildings may include a modest front setback (3-7 feet) to create a transition area between the public and private space. Street wall-reinforcing elements are encouraged frontage in this setback, such as:
 - Porches and stoops
 - Landscaping
 - 3 foot maximum fence height
- Single-family and lower-density residential structures on Type II streets may have a front setback of 20-25 feet (a percentage of lot area) as long as they are designed to maintain the existing character.
- Detailed accessory residential structures, such as accessory dwelling units or detached garages, should be set 10 feet from the back lot line.



Figure 5.3.1.1: A street scene showing building placement and setbacks.

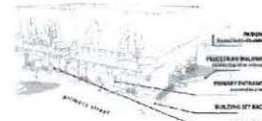


Figure 5.3.1.2: A diagram showing building placement and setbacks.

FASHION PLACE WEST SMALL AREA PLAN

6 APPENDIX

6.2.3. CATALYTIC PROJECT: TRAX STATION AREA REDEVELOPMENT



The area around the Fashion Place West TRAX station is ripe for redevelopment over the next 20 years.

In order to encourage this increase in density of uses such as commercial, residential, and office, the area must be rezoned to decrease parking requirements and increase density allowances.

SHORT TERM

- Amend zoning ordinance, rezone properties
- Improve access from Cottonwood Street to TRAX station with Cottonwood bridge reconstruction
- Prioritize residential infill development adjacent to TRAX station

- Perform streetscape improvements
- Improve UTA bus circulation and frequency with Route 20

MEDIUM TERM

- Help facilitate increased densities that includes residential

LONG TERM

- UTA Parking structure
- Help facilitate property transition of existing industrial property side of study area.



6.2.4. CASE STUDY: MEADOWBROOK 188 WEST 3900 SOUTH, SOUTH SALT LAKE

The Meadowbrook station is located in an older industrial area near the center of the Salt Lake Valley. Upon the construction of TRAX, the surrounding parcels were primarily industrial and underutilized parcels. Some office space, Hammer Park, and single-family homes inhabited the area, as well.

Once the Meadowbrook station was built, the surrounding community leveraged Eviction Utah and the Watchdog 2040 toolkit for future development around the station. South Salt Lake and Salt Lake County have employed a Form Based Code and other policies to remove barriers and encourage the kind of growth the community envisions.



TIME TO GET TO... VIA TRAX



NEIGHBORHOOD DATA



Viewing Trax in Meadowbrook and Downtown, built in 2017. (Source: Urban Design Partners, 2017)



Construction of the Hub of Opportunity, expected completion in 2021. The Hub will include a 10-story building, 100,000 sq ft, 100 units in office space, 100 units in residential space, 100 units in retail space, 100 units in parking structure, and 100 units in community space. The Hub will provide a central location for community building and development.



Renovation of the new Hub of Opportunity, located on the corner of 1000 S. West Temple. The Hub will include a 10-story building, 100,000 sq ft, 100 units in office space, 100 units in residential space, 100 units in retail space, 100 units in parking structure, and 100 units in community space. The Hub will provide a central location for community building and development.



6 APPENDIX

6.2.1. CATALYTIC PROJECT: STATE STREET/ WINCHESTER INTERSECTION



The future success of Fashion Place Mall and the surrounding area hinges on the ability to develop more densely where properties meet State Street (and Winchester Street). To make this future development possible the following regulations should be reviewed and revised:

1. Create new Fashion Place West zone district (FPW) that includes:

- Shared parking provision
- Implementation of parking maximums
- Reduced front yard setback
- Ground Floor activation recommendations

SHORT TERM

- Amend zoning ordinance, rezone properties
- Prioritize residential and office use infill development adjacent to State and Winchester Streets

- Perform streetscape improvements

MEDIUM TERM

- Work with Fashion Place Mall to improve internal pedestrian and pedestrian access to mall site
- Work with UDCT to improve pedestrian and bicycle exp Winchester and State Street intersection
- Parking structure at mall

LONG TERM

- Help facilitate increased densities and residential development within mall property, especially adjacent to State Street.



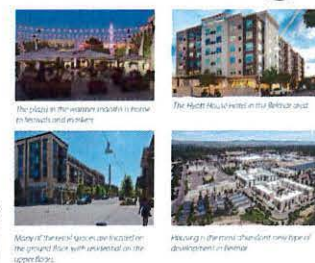
6.2.2. CASE STUDY: BELMAR LAKEWOOD, COLORADO

After more than 15 years of proactive redevelopment efforts by the City of Lakewood and private developers, Belmar is considered to be the new downtown. The total amount of retail area was reduced considerably but the developers added housing, office, lodging, and healthcare to the mix to create a mixed-use place.

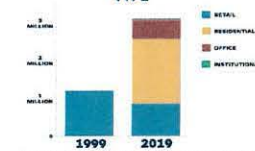
In 1986, the Villa Italia, a regional mall was built. It was a 104-acre site with 1.2 million square feet of commercial space. The mall closed in 2001 due to increasing competition and changing retail formats. The City of Lakewood began to re-evaluate how Villa Italia could be renovated or redeveloped.



NEIGHBORHOOD DATA



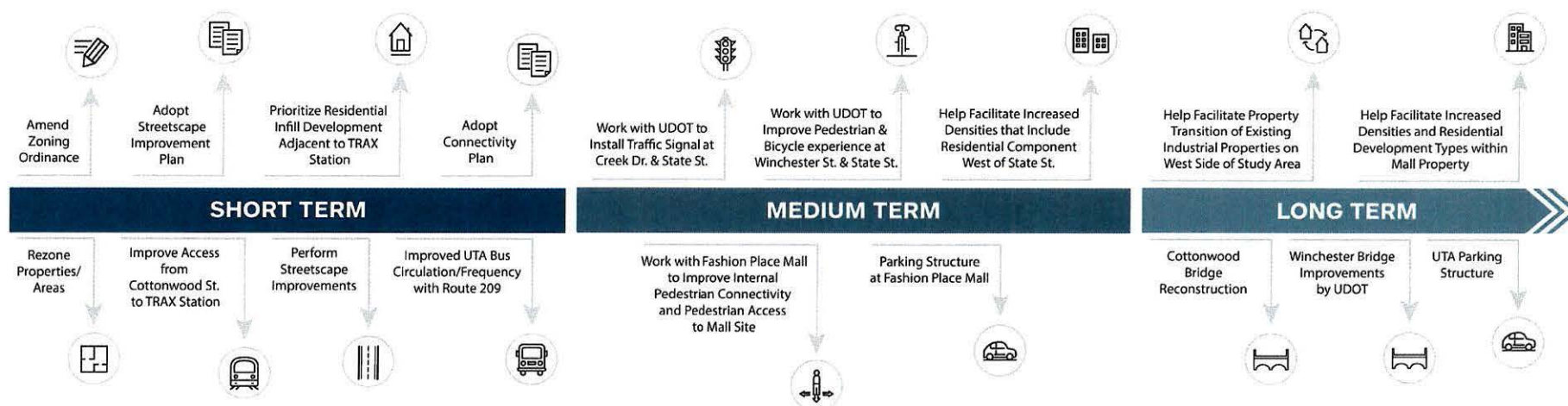
SQUARE FEET OF DEVELOPMENT BY TYPE



The largest catalyst for redevelopment of the Belmar area is the mix of uses, which is a reflection of the overall trend of retail space being replaced.



IMPLEMENTATION





FASHION PLACE WEST SMALL AREA PLAN

Thank you!

Murray City Corporation

NOTICE OF PUBLIC HEARING

NOTICE IS HEREBY GIVEN that on the 16th day of February, 2021, at the hour of 6:30 p.m. of said day the Murray City Municipal Council will hold and conduct a Public Hearing on and pertaining to the consideration of adopting the Fashion Place West Small Area Plan as an Amendment to the General Plan, for the properties generally bounded from 6100 South Street to Lester Avenue (6790 South) and from State Street to the Frontrunner line that is generally along 400 West; also properties abutting Winchester Street from State Street to Fashion Blvd.

The purpose of this public hearing is to receive public comment concerning the proposed action.

Public Notice is hereby given that this meeting will occur electronically without an anchor location in accordance with Utah Code 52-4-207(4), due to infectious disease COVID-19 Novel Coronavirus. The Council Chair has determined that conducting a meeting with an anchor location presents substantial risk to the health and safety of those who may be present at the anchor location because physical distancing measures may be difficult to maintain in the Murray City Council Chambers.

The public may view the meeting via the live stream at www.murraycitylive.com or <https://www.facebook.com/Murraycityutah/>.

***Citizen comments or public hearing comments may be made as follows:**

- Live through the Zoom meeting process. Those wishing to speak during these portions of the meeting must send a request to city.council@murray.utah.gov by 3:00 p.m. on the meeting date. You will receive a confirmation email with instructions and a Zoom link to join the meeting.
- Read into the record by sending an email in advance or during the meeting to city.council@murray.utah.gov.
- Comments are limited to less than three minutes, include your name and contact information.

DATED this _____ day of January, 2021.

MURRAY CITY CORPORATION

Brooke Smith, City Recorder

DATES OF PUBLICATION: January 31, 2021

ORDINANCE NO. _____

AN ORDINANCE RELATED TO LAND USE; AMENDS THE GENERAL PLAN TO INCLUDE A SMALL AREA PLAN FOR THE FASHION PLACE WEST AREA.

Background

Chapter 3 of the City's 2017 General Plan (the "General Plan") identifies recommended "Small Area Planning Projects." The Fashion Place West area was identified among such projects, and in early 2019, the City was awarded a grant from the Transportation & Land Use Connection (TLC) program administered by the Wasatch Front Regional Council to study the area around develop a Small Area Plan for the Fashion Place West area. The City worked with a consultant to conduct the study in developing the Small Area Plan.

The study area comprised of a large area surrounding the Fashion Place West area, and was an area identified in the General Plan as an area that could benefit from more in-depth study. This area comprises approximately 245 acres, which includes aging light industrial uses, the Fashion Place Mall, two multi-family developments, and a stable residential neighborhood bisected by the I-215 interchange. The guiding principal that resulted from the study is to align the planning and design of the small area plan with the overall vision of the General Plan.

Notices were sent to 897 property owners in the vicinity to attend the Planning Commission to make public comment. After hearing the matter and citizen comments, the Planning Commission forwarded to the Council a favorable recommendation.

NOW, THEREFORE, be it enacted by the Municipal Council of Murray City as follows:

Section 1. Purpose. The purpose of this ordinance is to adopt amendments to the General Plan.

Section 2. Amendment. The attached amendment to the General Plan, specifically the Fashion Place West Small Area Plan, is hereby adopted as part of the Murray City General Plan.

Section 3. Effective date. This Ordinance shall take effect upon first publication and filing of copy thereof in the office of the City Recorder of Murray City, Utah.

PASSED, APPROVED AND ADOPTED by the Murray City Municipal Council on this ____ day of _____, 2021.

MURRAY CITY MUNICIPAL COUNCIL

Diane Turner, Chair

ATTEST:

Brooke Smith, City Recorder

Transmitted to the Office of the Mayor of Murray City on this ____ day of _____, 2021.

MAYOR'S ACTION: Approved

DATED this ____ day of _____, 2021

D. Blair Camp, Mayor

ATTEST:

Brooke Smith, City Recorder

CERTIFICATE OF PUBLICATION

I hereby certify that this Ordinance was published according to law on the ____ day of _____, 2021.

Brooke Smith, City Recorder

A motion was made by Travis Nay to forward a recommendation of approval to the City Council for the requested amendment to the Zoning Map designation of the property located at 5283, 5157, 5217, and 5177 South State Street, and 151 East 5300 from C-D, Commercial Development to M-U, Mixed Use.

Seconded by Ned Hacker.

Call vote recorded by Mr. Hall.

 A Ned Hacker
 A Lisa Milkavich
 A Travis Nay
 A Sue Wilson
 A Maren Patterson
 A Phil Markham
 A Scot Woodbury

Motion passed 7-0.

FASHION PLACE WEST SMALL AREA PLAN – Project #20-001

Zac Smallwood reviewed the General Plan Amendment to adopt the Fashion Place West Small Area Plan that roughly encompasses 6100 South to 6790 South and I-15 to just east of State Street. The 2017 General Plan calls for certain areas to be further researched and developed. Fashion Place West, as well as all the transit stations, are areas needing further research and development.

The City obtained a grant from the Wasatch Front Regional Council's (WFRC) Transportation and Land Use Connection (TLC) program. The TLC program is a partnership between WFRC, Salt Lake County, the Utah Department of Transportation (UDOT), and Utah Transit Authority (UTA). The TLC program provides technical assistance to local communities to help them achieve their goals and plan for growth. The City put out a Request for Proposal (RFP) to find the most qualified consultant to help with this project. The City selected VODA Landscape and Planning.

Mark Morris, VODA, said in planning for development, they looked at what is feasible and what investments the City needs to plan for. One of the key objectives of this plan is to try to improve the connection between the Trax Station at Fashion Place West on Winchester Street and the Fashion Place Mall. He reviewed the sections of the plan.

The Fashion Place West Small Area Plan includes sections related to existing conditions, housing, connectivity, and design guidelines. The following goals for the study area were established through the small area planning process:

- Strengthen relationship between the TRAX Station and Fashion Place Mall.
- Improve connectivity for the neighborhood.
- Improve the overall neighborhood quality.
- Promote transit use and active transportation.

Mr. Morris went over the public outreach that was done for this project. One open house was

held and one survey was conducted.

Housing is going to be an issue for the Wasatch Front in the foreseeable future. The City can help with the housing supply by building more housing in key areas such as the Fashion Place West neighborhood. The plan divided Fashion Place West into subareas based on the housing types that were appropriate for each area. The subarea categories are established residential, urban mixed-use, transit-oriented mixed-use, and jobs and housing mixed-use. The largest amount of housing surrounding the Fashion Place West area consists of single-family neighborhoods.

A big piece of this study was the connectivity analysis where they looked at the gaps and challenges the connectivity network has. There are parts of the neighborhood that have good pedestrian infrastructure and parts where an investment needs to be made in pedestrian infrastructure.

The Design Guidelines section was broken down into key urban design elements that the City could look at adopting. Building placement and the quality of the pedestrian space is important as development comes in along Winchester Street.

The plan includes a section of catalytic projects which are projects that could happen in key areas that could change the Fashion Place West neighborhood. Trax station area redevelopment and the State Street/Winchester Street Intersection were two catalytic projects noted in the plan.

Mr. Markham asked how often the City looks at revising the General Plan or Future Land Use Map. Mr. Smallwood replied a General Plan should be looked at every five years, however it usually only happens about every ten years. Mr. Markham said it is hard to plan things out for 25 years. Things will change in the future and this plan has the potential to be changed down the road.

Mr. Woodbury noted that comments from the following individuals were provided to the Planning Commission prior to the meeting: Heydon Kaddas, Matthew Schneider, Nicolle Stookey, and Kristi Miller.

The meeting was open for public comment. The following comments were read into the record:

Madison Smith – 6152 South Clear Street, Murray City

Wow let me first off start by saying that, I just moved into a home in your city... and boy, do I really love it. It's extremely convenient (which you have noticed) and my neighborhood is quiet and calm. I sure do enjoy my lovely neighbors who recently welcomed me with open arms.

I've been living at 6152 Clear St. since May of 2020, but it goes a lot farther back than that. My parents bought this home when they first got married, about 30 years ago. I grew up in this house for a part of my childhood. My first dogs were here, my first sand box, my name is written in the cement out back, est. 1993. It has been a pretty sentimental opportunity to now live here with the love of my life, Riley and our dog Roby. In March we chose to gut this home completely and renovated everything. We rewired the whole home, all new plumbing, ripped out walls, and installed new floors.. we had cupboards handmade and drywall installed. The list goes on, but you should see the before and after photos, it is something to be proud of! We dug all the sand out of my old sand box, about four tons... and replaced it with nourishing dirt where I was able to

grow a beautiful garden last summer. Moving here has been such a wonderful step in my life, and it has brought me, my parents, and Riley closer than ever before. For some reason I had this feeling, maybe I would be able to grow a family here sometime in the future to. A lot of blood sweat and tears have been poured into 6152 Clear... my dog sure loves chasing the gophers. I guess I feel slightly desperate asking to not take away our neighborhood... a huge part of my life, and a huge part of everyone else's life in this area. I know so many people would be sad and left with nothing, displaced... during a global pandemic. Times are hard enough right now, it's a shame that Murray City would impose such an awful Christmas gift for everyone to worry about. This proposition is absolutely not in the best interest of anyone who is actually involved. I hope that to whomever is reading this has a kind, compassionate heart.

Timothy Schomburg – 66 West Lester Avenue, Murray City

I live in the South 67 Condos. I've lived here since 2000 when I moved back from L.A. Why do all you politicians in Salt Lake County want to make Salt Lake County like L.A.? Look at the south west part of Salt Lake County. It looks like L.A. and the county wants to add even more high density with the Olympus project. So why do you in Murray want to change the zoning to allow more high density residential/commercial development? It's one thing to fix sidewalks, add a traffic light, but no high density.

1.2.3 BARRIERS TO DEVELOPMENT

Barriers to development within the study area include: Lack of City owned land that could spur private development. Current zoning regulations prohibiting density and growth including front yard setbacks, height limits, open space requirements, and parking requirements.

EXACTLY. Prohibiting high density growth. No high density growth.

1.6.4 POLICY UPDATES AND LAND USE AMENDMENTS

(d) Decrease open space requirements from 20 percent to 10 percent

WHAT? Decrease open space. I say expand open space or at least leave it the same. If you want L.A. high density, then move to L.A.

Carla Clark – Murray City

As a resident in the Fashion Place West area, I am concerned that major issues in the area plan were addressed insufficiently or not at all. Before any zoning changes are enacted, the plan should fully investigate these concerns:

- 1. Address traffic congestion and backup on Winchester*
 - a. Accessing Winchester from the neighborhood is already difficult due to back up from TRAX and increasingly heavy commuter traffic.*
 - b. Customers accessing proposed businesses will have the same issue and likely compound it.*

- c. *Since the elimination of buses, elementary students must cross an already dangerous Winchester. Their safety should be of top concern when making decisions that will increase traffic.*
2. *Validate expectation of conversion to foot/bike traffic*
 - a. *Due to easy freeway access, how will traffic be affected by:*
 - i. *Increased traffic from customers of these new businesses.*
 - ii. *New residents who commute via car.*
 - iii. *Transportation limited to TRAX and foot/bike is still constrictive for most lifestyles. How will these assumed non-vehicle owning residents access areas outside of TRAX and Fashion Place.*
 - b. *How will shoppers transport large purchases (including more than a few groceries) without a vehicle?*

Sub-area 3

Parking - The plan indicated that proximity to TRAX would reduce the need for parking but provides no evidence for that rational. There should be enough high-density housing in the area to provide data, but nothing was included.

1. *Parking for small businesses is limited, so how could there possibly be space for high-density structures?*
 - a. *What would a minimum ratio of parking per resident/business size look like?*
2. *The report mentions street parking on Winchester (Pg. 25 Section 2.10.1.1), but with bike lanes and a high level of traffic, street parking is already dangerous. Is street parking an option and what rationale will support this as safe?*
3. *I would also like to see a crime analysis for high-density neighborhoods. Just this week a murder was reported in the TRAX Fireclay high-density area and that is not the first time that area has made the news.*

I am also concerned about the narrow strip on the south side of Winchester included with Sub Area 3. High density is not suitable in what is essentially my neighbor's backyard. A buffer of smaller homes and businesses would be more appropriate.

Sub-Area 1

1. *Parking – Accessory Dwelling Units (pg. 47 Figure 3.21) should include a requirement for off-street parking spaces.*
 - a. *Due to narrow roads, people parking on the street often reduce sections to single lane. ADU's would only make this worse if they don't have sufficient parking.*

I understand that the area should be carefully developed, but it seems obvious that the high density plan for area 3 is questionable and needs further study and proof of rationale. Additionally, the connected neighborhood needs to be protected from associated problems of insufficient planning.

I hope you will wisely make further investigation and provide the applicable information before making any changes.

Teresa Long – Murray City

I purchased my home on Creek Dr. less than 2 years ago. I lived outside Murray, however, sent my kids to Murray schools, shopped here and couldn't wait to move to Murray. I have/had plans of refinishing my basement and having this be my forever home. It has been a very safe neighborhood and I have great neighbors. It is mainly single women with kids or elderly. That is great that you want to push vulnerable populations out of their homes. I vehemently oppose this change!

State street has many areas that are vacant and it seems like a much more logical choice. Every time I drive down State in Murray I think there is nothing here. Plenty of 7-Elevens and dealerships but that is about it aside from mall. Last year I heard about this, the word then was that you want more traffic to the mall from Trax. Just drive by the mall, or go inside, it is always busy now that it is open again. If someone on Trax wants to go there they will. And they do, I see people walking there all the time. Having a bunch of large office commercial buildings won't do it. I sincerely hope that this does not pass. If so I will definitely not relocate in Murray.

Jill Rhead – Murray City

I am writing to you in response to the public hearing scheduled for tonight at 6:30 pm to discuss the proposal for the Fashion Place West Small Area Plan.

I have read your plan and I have many concerns. One of which is the proximity of the TRAX station to the Fashion Place Mall. The average American walks 3000 steps per day. The distance from the Murray TRAX station to the nearest mall entrance is over half a mile or about 1000 average steps. A round trip on foot from that station to the mall and back would burn over 2/3 of the average person's steps per day. Do you have any statistics on how many people presently use the Murray TRAX station to frequent the mall now? My guess is it is very few.

I feel as if this plan has little to do with its stated goals and more to do with rushing an opportunity to redevelop an area that is currently home to established and thriving business. And, I am very concerned about your tactics - a few thoughts on that:

- Holding a public hearing the week before Christmas seems very suspicious since most people are too busy to think about this kind of thing right now.*
- We are under a statewide mask mandate, is it even legal to gather in large groups? And if it is, is it a prudent move? Your timing does not seem appropriate.*
- Yesterday, it was announced on the national news that one American is dying of Covid every 30 seconds. Jeopardizing public safety by holding a public meeting during a pandemic is reckless.*

It is becoming more and more evident to me that the good of the people and businesses in the area is not the goal of the Murray City Planning department. If you truly want public input, I would suggest that you wait a while to push this through so the voices of those this will affect can be fairly heard.

Brandon Tiedt – Murray City

I am a property owner off of Malstrom Lane. Me and my family are strongly opposed to high density apartment buildings being built, along with all the other issues this project would bring.

Derek Tiedt – Murray City

I am a home owner in Murray on Krista Ct. and am strongly opposed to this project. Adding hundreds of apartments/condos to the proposed area will over burden the infrastructure in place and cause major delays to anyone who lives in the area. Rush hour traffic anywhere near Winchester is already heavy without the addition of a few hundred new cars. I am strongly opposed to the idea of my property taxes going up to fund this project and only make things harder for the people in the neighborhoods surrounding the proposed project.

Ian George – Murray City

Will the new proposed bike trail that parallels the Trax line be using part of people's yards, and will it be on the East or West side of the Trax lines. Is there enough room in the Trax corridor to safely allow a bike trail? Will you be removing the concrete walls that are existing? Would those concrete walls be replaced with concrete if they come down? How will you guarantee the safety of the resident's homes that but up against Trax?

Matt Newland – 6199 Valley Drive, Murray City

My family and I live at 6199 Valley Drive. In the proposed plan, we find a map that shows our home as being zoned to commercial property. Is the plan to take our home?

Joe Silverzweig –Murray City

I want to make comments in support of the development plans in these items, as they are parts of the city I live near and frequent. I'm really excited for the changes to this area; the additional density makes sense in that part of our city and will help alleviate the drive through strip mall feel of that part of State Street. I think the plan is too optimistic about the current state of Winchester sidewalks- it's a long, exposed walk on a high speed road and there's a lot of construction, narrow spots, and other unpleasantness to evade. It would be worthwhile to explore a small shuttle or other transit solution from Trax to the mall, at least while improvements to Winchester Bridge and the sidewalks have yet to take effect. I also hope we'll work hard to preserve the relative affordability of housing in this area so that we can invite long-term residents who will contribute to a vibrant community and build wealth that is reinvested in Murray.

The following citizens spoke during public comments:

Timothy Schomburg – 66 West Lester Avenue, Murray City

Mr. Schomburg said he grew up in Sugarhouse. He knows the Planning Commission is trying to do the best they can with the growth of Salt Lake County and Murray. He does not want to see Murray City turn into L.A.

Jon Boettcher – Murray City

Mr. Boettcher said he has lived in his neighborhood, east of the Trax lines off of 6400 South, for over 40 years. Since Trax has come in, crime has steadily increased. The higher the housing density becomes, the higher crime is. You can't even drive on the streets over at Fireclay at night because there are cars parked all over the street. If the City is planning higher density housing, there has to be adequate parking. Things like public safety need to be considered when it comes to high density housing.

The public comment portion for this agenda item was closed.

Mr. Morris said the introduction of housing density is never popular. Any housing considered high density would be happening in the subarea near the mall. Redevelopment of underutilized parcels near the Trax station would be more mid-rise. Buffering is being recommended for anything backing up into single-family neighborhoods. The places where this type of development is most appropriate is where you have transit service. He knows not everyone will utilize public transportation but making the experience of getting around the neighborhood better and improving the infrastructure will make the area more peaceful.

Traffic congestion is an issue that is everywhere. As far as transportation planning goes, the intent is to make it more feasible for people to get around their neighborhood without having to get into their car for every trip. Children today will not be able to afford a home in this valley unless the supply of homes is increased.

Additional information can be added to the plan regarding the parking demand in transit- oriented areas. There is data out there showing that people who choose to live in these types of communities on average own fewer cars or use them for fewer trips. None of this plan is rezoning anything, it's looking at the potential in the future. Some of this is not economically feasible and won't happen for years.

Mr. Smallwood said the City is not rezoning anyone's property and is not proposing to take property from anyone. This is a visioning document that guides the planning staff in how they approach future land use applications. The plan also allows the City more bargaining power with UDOT. The plan will be looked at in more detail.

The City is aware of the traffic and parking concerns in the Fireclay area and is working on that problem.

There are standards for Accessory Dwelling Units (ADU) in Chapter 17.78 of the Land Use Ordinance.

Mr. Smallwood said crime has been increasing everywhere in Salt Lake County. It's unfortunate, but he doesn't have any statistics relating the rising crime rate to high density housing.

Ms. Wilson said her biggest concern is keeping the residential anchors in this area. She doesn't feel like high density housing is a good fit for this area. She thinks it's better to concentrate on installing sidewalks, pedestrian access, and bike lanes. Murray City needs more owner-occupied housing. An apartment building won't meet the needs of the City's core citizens. A lot of people that can't afford a \$500,000 house could afford a \$250,000 condo. Owner occupied units help

keep the neighborhoods stable, safer and is more Murray City's vibe. She loves the sense of community in Murray and would like to preserve that.

Ms. Patterson said she thinks this plan is well thought out. There are certain areas in the plan that could support high density housing. The plan also notes certain areas where single-family neighborhoods should be protected. It's not a one size fits all plan. Ms. Wilson said she thinks there are areas that would be perfect for medium density. She doesn't think high density is what the City is looking for. The plan is well thought out, but she wants to be careful about adding high density rather than medium density. Ms. Patterson said she agrees with Ms. Wilson, but she thinks a great place for a high-density development would be on State Street next to the freeway. Ms. Wilson say she agrees with that, she just wants to make sure the single-family neighborhoods are being buffered. Ms. Patterson said she feels protecting neighborhoods is a top priority.

Ms. Milkavich said there is a need for high-density housing, but it's important to be selective of where high-density housing is put and they are trying to do that.

Mr. Nay said he thinks the areas that will have the most intense development will be along the State Street frontage or directly adjacent to the Trax Station. There was a tremendous amount of investment that went into Trax in this area and it is the type of area where you want to see density increase. Building single-family homes around Trax is not a practical solution going forward. Mr. Nay asked Mr. Morris to clarify the statement about a reduction of open space. Mr. Morris replied on any particular site, the City requires a certain percentage of the site to be open space. The neighborhood has a big open space that the plan recommends improving. They are not recommending eliminating park space for housing.

Ms. Milkavich said the City has been trying to keep up with local and national trends on housing costs versus income. She thinks the City is at the turning point of meeting the need for affordable housing.

Mr. Hacker reiterated that this is not a zoning change. Anything that comes to the City for redevelopment in this area will also likely come to the Planning Commission where they can look at the plan and make sure it fits within the area.

Mr. Markham said going forward, the Planning Commission, City Council and City Government in general need to regain the trust of the residents. There were a lot of comments tonight from people who don't believe what the Commission is saying. There has been a serious erosion of trust in the process and it's crucial to restore the trust from the residents.

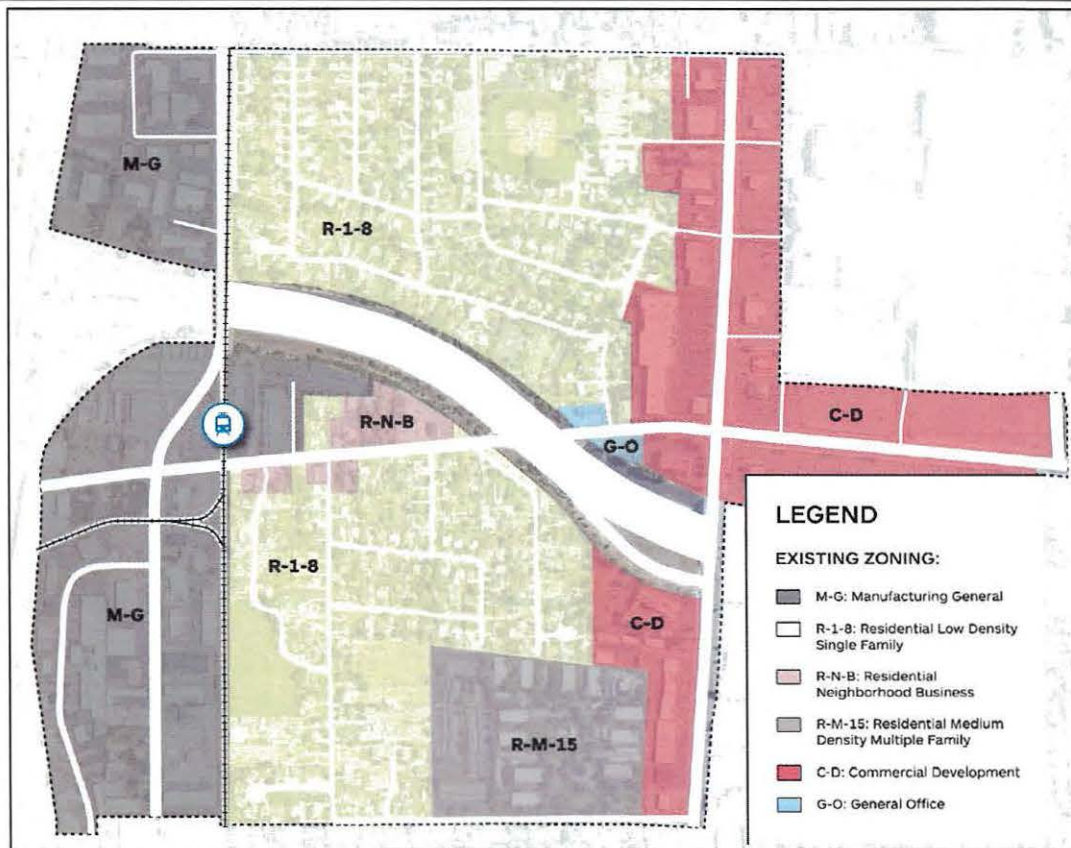
Mr. Woodbury said staff tries to establish framework that will balance both the rights of the residents and the property owners or developers. Staff tries to be responsive to the market conditions. There are a lot of projects coming that will provide middle ground housing. The reality of the Wasatch Front is that it is not going to be what it once was. This document, and the General Plan that was adopted a few years ago, does its best it can to provide a framework to balance what may happen. He agreed that the resident comments should be taken seriously. He added that this is a virtual public meeting and the Commission is not meeting together in person.

Ms. Patterson said one thing she likes about this plan is that this area has been underserved. You can't walk down Winchester Street or get from neighborhood to neighborhood. She hopes



AGENDA ITEM #8

ITEM TYPE:	General Plan Amendment		
ADDRESS:	Roughly 6100 S to 6790 S and I-15 to State Street	MEETING DATE:	December 17, 2020
APPLICANT:	Murray City Planning Division & VODA Landscape + Planning	STAFF:	Zachary Smallwood, Associate Planner
PARCEL ID:	N/A	PROJECT NUMBER:	20-001
SIZE:	Approximately 245 Acres		
REQUEST:	The Murray City Planning Division is requesting the adoption of the Fashion Place West Small Area Plan as an Amendment to the General Plan.		



I. STAFF REVIEW & ANALYSIS

Purpose

Small Area Plans are documents that help guide growth and decision making within an area. They are not to be used as ordinances or standards that require strict adherence. Small Area Plans can help inform the ordinance writing process. As an example, Murray City allows accessory dwelling units within single-family zones. There are regulations that dictate size, parking, and a number of other things. This small area plan suggests that accessory dwelling units be expanded in the single-family residential areas. This means that if the Planning Division were to look at amending the text of the accessory dwelling unit ordinance, some of the suggestions that are within the plan should be considered.

The proposed plan does not change the zoning, or character of the area. Its purpose is to inform the Public, Staff, and Elected Officials as to how the area could develop in the future and to provide a framework for those groups to prioritize infrastructure improvements, zone changes, and ordinance updates.

Background

Early in 2019 Murray City was awarded a grant from the Wasatch Front Regional Council (WFRC) to study the area around the Fashion Place West TRAX Station. The grant was awarded from the Transportation and Land Use Connection (TLC) Program administered through the WFRC who partners with Salt Lake County, the Utah Department of Transportation (UDOT), and the Utah Transit Authority (UTA). On the WFRC's website it states "The TLC program provides technical assistance to local communities to help them achieve their goals and plan for growth. The program helps communities implement changes to the built environment that reduce traffic on roads and enable more people to easily walk, bike, and use transit."

Throughout 2020, the Planning Commission and City Council have received multiple presentations on the progress of the Fashion Place West Small Area Plan. Including reports on the public engagement process, existing conditions, connectivity, and design guidelines. The last update was given in July of this year. Since then, the Planning Division Staff and the contracted consultant, VODA Landscape + Planning have been working on crafting the final Small Area Plan document.

Review

Planning Division Staff are requesting that the Planning Commission review the proposed small area plan and forward a recommendation of approval to the City Council. Staff has provided a summary of the document below. It is intended to help guide the commissioners in reviewing the plan.

The following is only a very basic summation of the small area plan, intended to assist in reading the proposed document.

Document Organization

The proposed small area plan is divided into six (6) chapters.

Executive Summary and Implementation

The executive summary and implementation chapter highlights the area plan's goals, existing conditions, housing and connectivity strategies, and lastly helps provide a framework for implementation of the plan. This section is used to provide an introduction and primer for what will be discussed in depth within the plan itself.

Existing Conditions

This chapter outlines the current situation of the study area. It builds upon the history of area as the Fashion Place Mall developed and turned this area from a small post-war suburban neighborhood to an economic center. This section also calls out community assets such as Grant Park, the Senior Recreation Center, the two elementary schools within the area, and the TRAX station itself.

The existing conditions chapter further outlines the challenges that the neighborhood faces and barriers to potential development along the core streets. Of note is the review of the Land Use Conditions that mentions that current zoning does not address the opportunities that the light rail station could provide.

This section also outlines existing economic and housing conditions. The Fashion Place West area trends slightly younger than Salt Lake County and significantly younger than the rest of Murray City. The housing cost in the area trends lower than most of the city and county with the median home value at \$239,474. Providing opportunities for people who currently reside to reinvest in their property is encouraged.

One way to increase the livability of a neighborhood is by making access to services more widely available through encouraging different modes of transportation. Opportunities for infrastructure improvements to increase walkability are called out on page 31. This includes items such as installing sidewalks in the single-family residential areas, better pedestrian access on the Fashion Place Mall site, and improvements to bus stop amenities.

Housing

It is no secret that housing across the Salt Lake Valley is in high demand. This chapter takes an in depth look into the study area and finds that housing demand is at an all-time high with vacancy rates (homes and rental units that are available) at 5.5%. This leads to increased prices for homes and rental units.

Providing a diversity of housing options allows for people to stay in their neighborhoods longer. Residents of Murray City are proud to live here and encouraging opportunities to allow for someone to cycle through different types of housing while staying in the city allows residents to remain. Planning Division Staff consistently hears from residents that used to live in Murray, moved away, and now have come back. If more opportunities for housing are provided people would not have to leave as often.

The small area plan calls for the study area to be divided into four (4) subareas. These subareas allow for focused growth and improvements depending on ability to provide new services, service existing residents, and foster future growth. Each subarea is briefly outlined below.

Subarea 1 “Established Residential” recommends that this area largely remain as it currently is. The plan suggests that infrastructure improvements like sidewalks and bike lanes be installed in these areas. To address additional housing, this area should look into housing reinvestment such as additions to existing homes or adding an accessory dwelling unit.

Subarea 2 “Urban Mixed-Use” recommends that higher density and height be allowed along the State Street corridor. Transitioning the corridor from strip commercial to a more urban style mixing of uses will strengthen connections to the mall. The plan recommends that higher residential densities be placed along the street and step down towards the existing single-family neighborhoods.

Subarea 3 “Transit Oriented Development” recommends that this area focus on smaller scale, service-oriented uses with housing mixed in at a smaller scale than the State Street corridor. Housing options such as townhouses, row housing, and smaller scaled apartments are encouraged.

Subarea 4 “Jobs and Housing Mixed-Use” recommends this area become more mixed with office-oriented jobs and mixing residential within. This area is largely industrial in use and the plan calls for it to transition to more of a mix of jobs and housing.

To implement these subareas the Planning Division will need to work on drafting new zoning ordinances to lay out the specifics of each subarea. This would occur after the potential

adoption of the plan. It would include additional public outreach and working with the public to craft ordinances that reflect the community.

Connectivity

The Fashion Place West study area is complex in its network of connections. The area is bounded on the west by I-15 on the west, and I-215 runs through the middle of the district. State Street on the east is a major, regional arterial road that handles thousands of cars a day and also creates a difficult barrier to access to Fashion Place Mall. These three corridors carve up the district into three distinct areas with little connection between them. The connectivity section provides opportunities for future improvements to help those that are biking and walking in the area to have greater access to the services that are near.

Design Guidelines

The intent of including design guidelines within this plan is to help guide those involved in developing ordinances to shape the look and feel of each subarea. The guidelines help to establish a more friendly environment for walking and biking and create a human-scaled development pattern to encourage people to get out of their vehicles and into the district itself.

Appendix

The appendix section addressed the public engagement that was conducted, including the open house in February of 2020 and the online survey that was distributed on May 20th and ran through June 20th.

This section also provides a preliminary look at what some catalytic projects could look like in the future. This is in no way meant to suggest that the area will develop to look exactly this way, rather to suggest what may be possible in the future. Each catalytic project also includes a case study where a project similar to the catalytic project occurred. Of particular note is the Jefferson Detention Basin. This area plan and the recently adopted Parks and Recreation Masterplan call for this to be adapted to an active park area while maintaining its use as a detention basin.

General Plan Consideration

The primary goal of the 2017 General Plan is to “guide growth to promote prosperity and sustain a high quality of life for those who live, work, shop, and recreate in Murray”. Based on that primary goal, five Key Initiatives were identified through the public process in developing the General Plan. Four of the five initiatives directly tie into development of the proposed small area plan. “Create Office/Employment Centers”, the second initiative, prescribes the

importance of creating new opportunities for office and employment. The proposed plan should help to make creating office space easier. The area surrounding the TRAX station should be a wholly contained neighborhood (initiative 3, Livable + Vibrant Neighborhoods) where people can access all their daily needs but should also generate visitors from other neighborhoods in Murray. Initiatives 4, Linking Centers/Districts to Surrounding Context and 5, A City Geared Toward Multi-Modality are tentpoles as the Planning Division and consultant worked to develop the small area plan.

II. PUBLIC INPUT

A total of 897 notices were sent to all property owners within the proposed Small Area Plan, property owners within 500' of the proposed plan and affected entities. There have been a number of e-mails sent in with concerns about the proposed plan. They have been included as attachments to this staff report for the Planning Commission to review.

III. FINDINGS

Based on the analysis of the proposed small area plan and review of the Murray City General Plan, staff concludes the following:

1. The Murray City General Plan provides direction in implementation through five key initiatives.
2. The requested General Plan amendment has been carefully considered based on public input and review of city planning best practices.
3. Chapter 3, Framework for the Future, of the Murray City General Plan calls for the development of Small Area Planning Project along rail transit-oriented developments.
4. The proposed General Plan amendment is consistent with the Goals & Initiatives of the Murray City General Plan.
5. The proposed small area plan will provide Murray City residents, staff, elected officials, and the development community clear guidance as to how the City anticipates development within the subject area.

IV. STAFF RECOMMENDATION

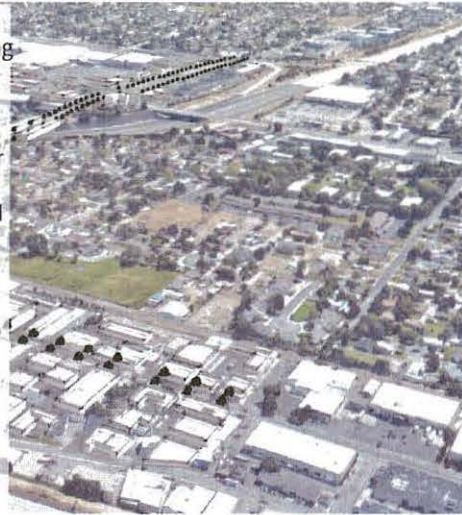
Based on the background, analysis, and the findings within this report, Staff recommends that the Planning Commission **forward a recommendation of APPROVAL to the City Council to adopt the Fashion Place West Small Area Plan as an amendment to the 2017 Murray City General Plan.**

Attachments

NOTICE IS HEREBY GIVEN that on December 17th, 2020, at 6:30 p.m. the Murray City Planning Commission will conduct a Public Hearing for the purpose of receiving public comment on the Fashion Place West Small Area Plan as an amendment to the Murray City General Plan for the properties generally bounded from 6100 South Street to Lester Avenue (6790 South) and from State Street to the Frontrunner line that is generally along 400 West; also properties abutting Winchester Street from State Street to Fashion Blvd.

A copy of the proposed plan is located at
<https://www.murray.utah.gov/1868/fpwsap>

If you would like to comment on this agenda item at the meeting please register at: <https://tinyurl.com/y2nsppnq> or you may submit comments via email at planningcommission@murray.utah.gov. If you would like to view the meeting only you may watch via livestream at www.murraycitylive.com or www.facebook.com/MurrayCityUtah/.



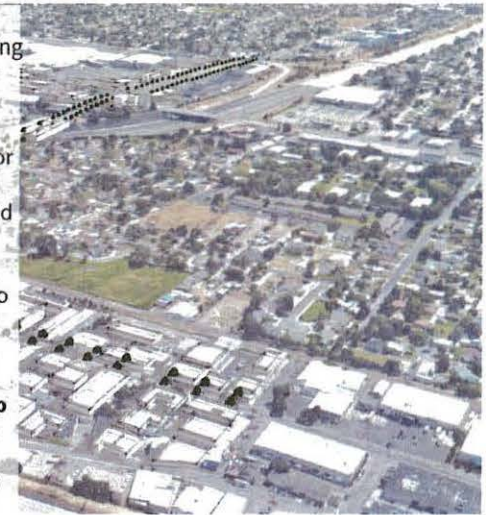
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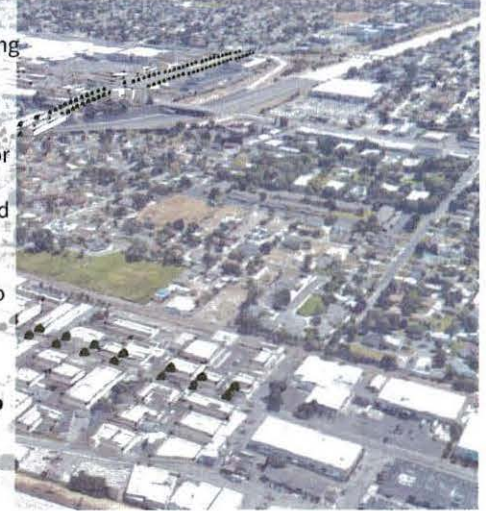
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MURRAY CITY

FASHION PLACE WEST SMALL AREA PLAN

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**MURRAY CITY
CORPORATION
NOTICE OF
PUBLIC HEARING**

NOTICE IS HEREBY GIVEN that on the 17th day of December, 2020, at the hour of 6:30 p.m. of said day the Murray City Planning Commission will hold and conduct a Public Hearing for the purpose of receiving public comment on and pertaining to a Fashion Place West Small Area Plan as an amendment to the Murray City General Plan for the properties generally bounded from 6100 South Street to Lester Avenue (6790 South) and from State Street to the Frontrunner line that is generally along 400 West; also properties abutting Winchester Street from State Street to Fashion Blvd. If you would like to comment on this agenda item at the meeting please register at: <https://tinyurl.com/y2nspnq> or you may submit comments via email at plannin@commission@murray.utah.gov. If you would like to view the meeting only you may watch via livestream at www.murraycitylive.com or www.facebook.com/MurrayCityUtah/. No physical meeting location will be available.

Jared Hall, Manager
Planning Division
1305055 UPAXLP

Product

Salt Lake Tribune

Placement

Legal Liner Notice

Position

Public Meeting/Hear

Scheduled Date(s):

12/06/2020

utahlegals.com

utahlegals.com

utahlegals.com

Scheduled Date(s):

12/06/2020

Deseret News

Legal Liner Notice

Public Meeting/Hear

Scheduled Date(s):

12/06/2020

12/4/2020 10:05:37AM

From: [Heydon Kaddas](#)
To: [Planning Commission Comments](#)
Subject: [EXTERNAL] Comments on Recent Fashion Place Small Area Plan
Date: Wednesday, December 9, 2020 8:31:33 PM

To whom it may concern,

I am a resident of a neighborhood included in the area identified by this plan. After receiving the flier in the mail recently and reviewing the plan online, I have a several concerns I would like to see addressed:

- The plan outlines that apartment and mixed-use high-density buildings between 4-6 stories are planned along State Street and Winchester. This is absolutely undesirable. My family specifically selected this neighborhood as it combined the convince of central living, outlined in the plan, with the feel of a quiet, suburban neighborhood that has gorgeous mountain views. Being surrounded by 6 story buildings will destroy the atmosphere that brough my family to this area in the first place. Having buildings be limited to 4 stories or less would help preserve the aesthetic of our neighborhood.

- Section 3 page 51 is of particular concern as it outlines decreasing the open space from 20% to 10 % as well as altering yard setbacks (both decreasing the set back to 0 and setting a limit on the maximum setback). This seems like it is designed to cram as many buildings and people into as small of a space as possible. The infrastructure in this area is already overwhelmed and this rezoning is listed as a short-term priority whereas working with UDOT to increase infrastructure in the area is listed as a long-term goal. This will further overwhelm the area without providing the assistance the area needs.

- I also have significant worries about the plan's advocacy for more accessory dwellings. The plan includes no regulation for these accessory dwellings. This looks like it is an attempt to not actually rezone the neighborhood to medium or high density but in effect achieve the same goal.

Thank you,
Heydon Kaddas

From: [Matthew Schneider](#)
To: [Planning Commission Comments](#)
Subject: [EXTERNAL] Fashion Place Small Area Plan
Date: Monday, December 7, 2020 2:41:58 PM

Hello, as a resident of the area in question I have a couple concerns I'd like to see addressed:

1 - Allowing high density buildings of 4-6 stories along State and Winchester is less than ideal. Perhaps 2-4 stories or less, more preserves the look and feel of the neighborhood. Six stories would drastically change the feel of the neighborhood and the mountain views of the residents.

2 - On page 72 of the report it notes that the area encompassing Grant Park be re-zoned to mixed use urban. Why would the park be removed or why allowed for mixed use zoning to encroach that far into the neighborhood?

3 - My final concern stems from advocating for more accessory dwellings. By promoting them without regulation it seems you could just add many people to a low density neighborhood and just end up making it a medium density one.

thank you,

Matt Schneider

From: [Nicolle Stookey](#)
To: [Planning Commission Comments](#)
Subject: [EXTERNAL] Questions for Fashion Place West Small Area Plan
Date: Monday, December 7, 2020 2:39:16 PM

Questions I have for the city meeting:

1. I have lived in Murray now for 16 years over on Creek Dr. In 2004, we bought our house for \$130,000, which for its size is a reasonable price. Now, homes in my neighborhood sell for \$250,000 and more. I am concerned with all the housing developments proposed, that housing will be inflated even more. To the point, that as a resident, when I am ready to sell my house now, I can't afford to live in Murray, nor am I enticed to. Wages are not meeting our housing industry. How is Murray focusing put on creating sustainable housing pricing?
2. Traffic on State Street to the mall is especially obnoxious during the holiday season. Those exiting the mall turnout in front of traffic to get left with no consideration of other drivers. Lights as far down past Sams Club are backed up and often blocked because of the traffic. Drivers entering the mall, especially on the west side there between H&M and Crate and Barrel stop the right lane of traffic. Really that entrance should be removed or adjusted that those entering can transition into the lot easier. With all these enhancements, what improvements will we see to the traffic situation? And if people are not riding public transportation now, they won't automatically start, so that is not a solution.
3. Homelessness and crime are growing in our area. At almost every light at the I215 exit, State St and Winchester, Winchester and Fashion Place Blvd all have someone there panhandling all day, causing the trash to build up. Crime in my neighborhood has been on the rise, with multiple car break-ins and thefts and nothing seems to be being done. How is this being addressed in these changes?

Thank you,
Nicolle Stookey

Agenda item #8
FPWSAP

From: [KRISTI M](#)
To: [Planning Commission Comments](#)
Subject: [EXTERNAL] Fashion Place West Housing
Date: Tuesday, December 8, 2020 11:49:06 AM

Hello,

I live in a home built in 1948 on Clear Street in the Fashion Place West area. Is there any discussion or intention to buy out the older homes in order to update the look of the housing within the area of discussion? If so, what would be the timing and process? How would that impact the current home owners financially?

Thank you,
Kristi

Agenda item #8
FPWSAP

From: [Mat Scilling](#)
To: [Planning Commission Comments](#)
Subject: [EXTERNAL]
Date: Thursday, December 17, 2020 7:54:42 PM

As other commentors I am very concerned about higher density housing.

It is difficult enough to get on to Winchester when a train stops.

From: [Brandon Tiedt](#)
To: [Planning Commission Comments](#)
Subject: [EXTERNAL] Fashion Place Comment for Meeting
Date: Thursday, December 17, 2020 5:22:10 PM

I am a property owner off of Malstrom lane. Me and my family are strongly opposed to high density apartment buildings being built, along with all the other issues this project would bring.

From: [Matthew Schneider](#)
To: [Planning Commission Comments](#)
Subject: [EXTERNAL] Fashion Place Small Area plan follow up
Date: Tuesday, December 22, 2020 11:23:08 PM

Hello,

I wanted to write with some concerns following the meeting last week:

- The commission for both the Fashion Place plan and the property re-zone on 53rd south repeatedly used the refrain that this wasn't for any specific project and all future projects will be reviewed. This is incredibly disingenuous - you'll approve the re-zone / small area plan and then when a developer suggests a type of project that residents don't want to begin with it will be approved because it follows the plans you just approved!
- The chair discussed the need to regain public trust. The commission then followed that up by unanimously approving something that had overwhelmingly not been wanted in the small area plan. What's the point of public input then?
- There was yet another shooting near the Murray North trax station - the last area the city decided to try and cram more people into. Do not turn Fashion Place into another Fireclay or abomination that has become the area around SouthTowne mall near that Trax station. Why does the commission seem intent on doing things that benefit mythological future residents of Murray rather than the current ones. We gain nothing from cramming another 500 dwellings in this small area other than more crowded streets and neighborhoods and more crime.

I would like to hear some follow up to this and some answers to the final short questions below:

- What are the next steps for this plan? Is the city council another entity that can put a stop to it?
- How does one get on the planning commission?

Thank you,
Matt Schneider

FASHION PLACE WEST

SMALL AREA PLAN

P/C 9/3/2020

Project #20-001 (928 total)

Project Area + 500' radius + affect ent

Dennis C Thornton; Shelley L Thornton
196 W 6100 S
Murray , UT, 84107-7062

Karman, Inc
6000 S Stratler St
Murray , UT, 84107-3304

Scott R Pace
180 W 6100 S
Murray , UT, 84107-7062

SOST
2265 E Cottonwood Cove Ln
Cottonwood Hts , UT, 84121-5018

M Aaron Ravonsheed
1736 E Lahar Dr
Millcreek , UT, 84106-3339
** returned in mail**

Lyle Blair Wilkinson
6049 S 300 W
Murray , UT, 84107-6922

Verne A Cotton; Mary P Cotton (Jt)
6094 S Liberty Oaks Cv #4
Murray , UT, 84107-3325

Colleen Mcguire
6094 S Liberty Oaks Cv #3
Murray , UT, 84107-3325

Ray L Daniels (Jt)
6094 S Liberty Oaks Cv #2
Murray , UT, 84107-3325

Mary Alice Black
6094 S Liberty Oaks Cv #1
Murray , UT, 84107-3325

CVA Protection Tr
6076 S Liberty Oaks Cv #1
Murray , UT, 84107-3324

Julie R Tolman
6076 S Liberty Oaks Cv #2
Murray , UT, 84107-3324

Michael M Day; Jill Day (Jt)
6062 S Liberty Oaks Cv #4
Murray , UT, 84107-3331

Javaid M Lal; Sunita S Lal (Jt)
6062 S Liberty Oaks Cv #3
Murray , UT, 84107-3331

Trust Not Identified
6062 S Liberty Oaks Cv #2
Murray , UT, 84107-3331

Helaman Berrios; Heather Berrios (Jt)
6062 S Liberty Oaks Cv #1
Murray , UT, 84107-3316

Rosemary K Dorrance;
Martin Dorrance (Jt)
6044 S Liberty Oaks Cv #1
Murray , UT, 84107-3316

Patricia R Capps
6044 S Liberty Oaks Cv #2
Murray , UT, 84107-3316

F L Sullivan
6044 S Liberty Oaks Cv #3
Murray , UT, 84107-3330

Phillip W Johnson; Jace P Johnson (Jt)
6044 S Liberty Oaks Cv #4
Murray , UT, 84107-3316

Chong Lee
2636 W Tamra Dr
Taylorsville , UT, 84129-7325

6042 Liberty Oaks, LLC
9161 S Baronay Cir
Sandy , UT, 84093-3858

Flg Liv Trust
6042 S Liberty Oaks Cv #3
Murray , UT, 84107-3323

Andre Mercer
6042 S Liberty Oaks Cv #4
Murray , UT, 84107-3316

Brandon Quigley; Rae Quigley (Jt)
6042 S Liberty Oaks Cv #5
Murray , UT, 84107-3316

Janalee Malmstrom
6042 S Liberty Oaks Cv #6
Murray , UT, 84107-3323

Brandy Lynne Valle
6028 S Liberty Oaks Cv #6
Murray , UT, 84107-3316

Theresa Schuyler
6028 S Liberty Oaks Cv #5
Murray , UT, 84107-3316

Earl Bradford Pitts Family Trust
6028 S Liberty Oaks Cv #4
Murray , UT, 84107-3316

BJA Rev Tr
6028 S Liberty Oaks Cv #3
Murray , UT, 84107-3322

DB Fam Tr
6028 S Liberty Oaks Cv #2
Murray , UT, 84107-3322

Akiko Kamimura
6028 S Liberty Oaks Cv #1
Murray , UT, 84107-3316

Kim W Lundeberg
6039 S Liberty Oaks Cv #4
Murray , UT, 84107-3334

Lance M Park; Kristi Park (Jt)
6039 S Liberty Oaks Cv #3
Murray , UT, 84107-3317

Penny Coleman
6039 S Liberty Oaks Cv #2
Murray , UT, 84107-3334

Josefina Abed
6039 S Liberty Oaks Cv #1
Murray , UT, 84107-3317

Secretary Of Housing And Urban
Development
2401 N W 23Rd St
Oklahoma City , OK, 73107-
** returned in mail**

Down Home, LLC
5969 S 450 E
Murray , UT, 84107-

Charlean Coulter
6047 S Liberty Oaks Cv #2
Murray , UT, 84107-3333

Trust Not Identified
505 E Edindrew Cir
Murray , UT, 84107-6525

Kent W Baker & Barbara H Baker
Revocable Living Trust 08/06/2019
6063 S Liberty Oaks Cv #1
Murray , UT, 84107-3317

George L Iii Sears; George L Sears
1908 E Summer Meadow Cir
Sandy , UT, 84093-7010

Binh T Huynh
6063 S Liberty Oaks Cv #3
Murray , UT, 84107-3317

Desiree K Preston
6063 S Liberty Oaks Cv #4
Murray , UT, 84107-3317

Murray Oaks Condo Common Area
301 W 5400 S # 120
Murray , UT, 84107-8224

Murray City School District
5102 S Commerce Dr
Murray , UT, 84107-4710

Simon Bradstreet
93 W 6020 S
Murray , UT, 84107-6901

Kelly Rae Moulton
6023 S 115 W
Murray , UT, 84107-

Sean W Tingey; Ashley A Tingey (Jt)
101 W 6020 S
Murray , UT, 84107-6943

Series E 112 W 6025; An Individual
Series Series E 112 W 6025
5754 S Ridge Creek Rd
Murray , UT, 84107-6617

Doug Hannay; Stormy Hannay (Jt)
85 W 6020 S
Murray , UT, 84107-6901

Lc Ez; Gale Day
286 W 550 N
St George , UT, 84770-

Nicholas R Benson; Joni D Morgan (Jt)
82 W 6025 S
Murray , UT, 84107-6955

Kenneth W Scribner; Barbara A
Scribner (Jt)
102 W 6025 S
Murray , UT, 84107-6948

Howard E Bird
1251 E Walden Ln
Draper , UT, 84020-9563

Osman Mackovic Living Trust
09/14/2019
56 W 6025 S
Murray , UT, 84107-6946

Linda K Incardine; Joseph J Incardine
8945 S Rockwell Dr
Sandy , UT, 84093-1968

Bonnie P Kilgrow
6012 S Nova Dr
Murray , UT, 84107-6913

Ashlee Nichole Smith
32 W 6025 S
Murray , UT, 84107-6946

Rebekuh Middlesworth; Jason
Middlesworth (Jt)
6013 S Nova Dr
Murray , UT, 84107-6914

Trust Not Identified
6019 S Nova Dr
Murray , UT, 84107-6914

Ginnie Van Leeuwen;
Rachel Reimann (Jt)
111 W 6025 S
Murray , UT, 84107-6947

James L Johansen;
Alison Johansen (Jt)
16 W 6025 S
Murray , UT, 84107-6946

Shane Callahan; Regina Martinez (Jt)
6014 S Main St
Murray , UT, 84107-6929

Bridget L Cox
81 W 6025 S
Murray , UT, 84107-6945

LHPFT
101 W 6025 S
Murray , UT, 84107-6947

Luis M Chacon;
Esperanza F Chacon (Jt)
74 W 5785 S
Murray , UT, 84107-5931

Trust Not Identified
51 W 6025 S
Murray , UT, 84107-6945

David M Johnson; Nancy L Johnson
71 W 6025 S
Murray , UT, 84107-6945

Travis S Gardner
57 W 6025 S
Murray , UT, 84107-6945

Linda M Richard
33 W 6025 S
Murray , UT, 84107-6945

Tina M Chapman; Philip R Culley (Jt)
47 W 6025 S
Murray , UT, 84107-6945

Sarah P Hardman
41 W 6025 S
Murray , UT, 84107-6945

David L Johnson; Beth J Johnson (Jt)
6052 S Main St
Murray , UT, 84107-6957

Dixie L Inlay; Deanna L Peterson (Jt)
19 W 6025 S
Murray , UT, 84107-6945

William & Helen Hoffman Fam Liv Tr
6032 S Main St
Murray , UT, 84107-6957

Amara Greene;
Timothy Rochelle (Jt)
88 W 6100 S
Murray , UT, 84107-7056

Marjorie L Brothers;
Amy L Brothers (Jt)
6082 S Main St
Murray , UT, 84107-6957

Bruce & Patricia Knight Family Trust
96 W 6100 S
Murray , UT, 84107-7056

Sara Nicole Staschke
58 W 6100 S
Murray , UT, 84107-7056

Karl B Poulson; Suzanne M Poulson (Jt)
80 W 6100 S
Murray , UT, 84107-7056

Derek Peterson
72 W 6100 S
Murray , UT, 84107-7056

Holandra Maricela Arroya; Nick L
Coombes (Jt)
28 W 6100 S
Murray , UT, 84107-7056

Gabriela Cuello Messina
64 W 6100 S
Murray , UT, 84107-7056

Vanice B Elsea
30 W 6100 S
Murray , UT, 84107-7056

Harry Imamura; Jay Imamura; Mary
Imamura
110 W 6100 S
Murray , UT, 84107-7058

Zz Property Management LLC
789 E Forest Side Ln
Murray , UT, 84107-

Murray City Corporation
5025 S State St
Murray , UT, 84107-4824

Christine Collard
50 W 6100 S
Murray , UT, 84107-7056

Edward Brothers; Marjorie Brothers
6082 S Main St
Murray , UT, 84107-6957

Leslie A Lefevre (Jt)
26 W 6100 S
Murray , UT, 84107-7056

Popperton Enterprises LLC
1776 Park Ave #4-210
Park City , UT, 84060-5148

Larry Craig Collard; Vicki Collard (Jt)
52 W 6100 S
Murray , UT, 84107-7056

Prh Management LLC
165 San Miguel Dr
Camarillo , CA, 93010-

Jba Maintenance, LLC
6211 S 380 W
Murray , UT, 84107-

D & T Investments LLC
6152 S Stratler St
Murray , UT, 84107-6984

Papa-Auni, LLC
5525 S Kenwood Dr
Murray , UT, 84107-6229

Vieweast Properties, LLC
6122 S Stratler St
Murray , UT, 84107-6984

L C Platt Holding
253 E Cottage Wood Ln
Murray , UT, 84107-3870

Shadow Mountain Properties, LLC
6182 S Stratler St
Murray , UT, 84107-6984

Crc Nationwide, LLC
Po Box 900033
Sandy , UT, 84090-

Crc Nationwide, LLC
Po Box 900033
Sandy , UT, 84090-

Bevan Investments, LLC
11567 S Heatherberry Cir
Draper , UT, 84020-9419

Popperton Enterprises LLC
1776 Park Ave #4-210
Park City , UT, 84060-5148

Silver Fedora Properties, LLC
2439 E Michigan Ave
Salt Lake City , UT, 84108-1926

Around The Bend Properties LLC
6122 S Stratler St
Murray , UT, 84107-6984

Papa-Auni, LLC
5525 S Kenwood Dr
Murray , UT, 84107-6229

Jeremiah Hamilton
6113 S 380 W
Murray , UT, 84107-

Papa-Auni, LLC
6195 S 380 W
Murray , UT, 84107-

Pm Blue Moon Management, LLC
6072 S 2180 E
Holladay , UT, 84121-1435

Papa-Auni LLC
6195 S 380 W
Murray , UT, 84107-

Papa-Auni LLC
6195 S 380 W
Murray , UT, 84107-

Larry Dean Construction Inc
649 E Draper Heights Wy
Draper , UT, 84020-7672
** returned in mail**

Launi Hamilton; Jeremiah Hamilton
6054 S Oslo Bay
Holladay , UT, 84121-1363

Dakota Development, LLC
6440 S Wasatch Blvd # 105
Holladay , UT, 84121-3559

Mak Leasing, LLC
6220 S 300 W
Murray , UT, 84107-7030

Iron Horse Winchester, LLC
Po Box 71687
Salt Lake City , UT, 84171-0687

Robert A & Jeneil Wahlen Family Trust
8655 S Danish Rd
Cottonwood Hts , UT, 84093-2108

Trust Not Identified
P O Box 704
Dove Creek , CO, 81324-

Boss Hogg Properties LLC
390 W 6500 S
Murray , UT, 84107-7003

High West Leasing, LLC
6194 S 300 W
Murray , UT, 84107-6925

W Liv Tr
357 W 6160 S # 1
Murray , UT, 84107-3314

RWK LLC
314 W 6160 S
Murray , UT, 84107-3310

Christensen Enterprises & Investments
6110 S 350 W # B
Murray , UT, 84107-

Ralph W Kramer Construction Co.
314 W 6160 S
Murray , UT, 84107-3310

Mak Leasing, LLC
6220 S 300 W
Murray , UT, 84107-7030

Wyocal, LLC
Po Box 1769
Wilson , WY, 83014-

Rebecca Reeves
6103 S Rainy Ln
Murray , UT, 84107-7047

EEJFL Trust
Po Box 704
Dove Creek , CO, 81324-0704

Dennis R. Sharp; Kathleen G. Sharp
6123 S Rainy Ln
Murray , UT, 84107-7047

Val M Stirling
2477 Shorewood Dr
Saratoga Springs , UT, 84045-

RLJ Fam Liv Tr
6228 S 300 W
Murray , UT, 84107-7030

Wes G Mccauley
6153 S Rainy Ln
Murray , UT, 84107-7047

Amber D Olsen; Thomas W Olsen (Jt)
6163 S Rainy Ln
Murray , UT, 84107-7047

Jackson D Pope;
Tess S Kooring (Jt)
6113 S Rainy Ln
Murray , UT, 84107-7047

Sophia Parsons Family Trust
4880 S Center St
Murray , UT, 84107-4846

Mark E Burton
6195 S Rainy Ln
Murray , UT, 84107-7047

Carson L Bowthorpe;
Skylar Carrington (Jt)
6143 S Rainy Ln
Murray , UT, 84107-7047

Sean P Borg; Casey A Smith (Jt)
6112 S Clear St
Murray , UT, 84107-7039

Zachary Bullock
6122 S Clear St
Murray , UT, 84107-7039

Michael Venable
6173 S Rainy Ln
Murray , UT, 84107-7047

Kristi A Miller
6142 S Clear St
Murray , UT, 84107-7039

SFT
12087 S Shannel Cir
Riverton , UT, 84065-3173

Brian J O'Connor
7735 S Sandra Wy
Midvale , UT, 84047-2603

Brandy L O'Bagy
6172 S Clear St
Murray , UT, 84107-7039

George D Mcadams; Lucinda Ann
Mcadams (Jt)
6182 S Clear St
Murray , UT, 84107-7039
** returned in mail**

Chandee Khantipab;
Charles T Pfaff (Jt)
6132 S Clear St
Murray , UT, 84107-7039

Vincent Lantz;
Colleen Rawlinson (Jt)
6198 S Clear St
Murray , UT, 84107-7039

Steven M Simmons;
Marie E Simmons (Jt)
455 Orange Blossom Ct
El Dorado Hills , CA, 95762-

Chandler Howe; Austin Ramaley (Jt)
6162 S Clear St
Murray , UT, 84107-7039

David Bonney; Emily Bonney (Jt)
207 W Clay Park Dr
Murray , UT, 84107-7021

Vagner Polund; Kendra A Polund (Jt)
187 W Clay Park Dr
Murray , UT, 84107-7090

David S Geary
6190 S Clear St
Murray , UT, 84107-7039

Corey Enloe; Brenda Enloe (Jt)
266 W Noah Cir
Murray , UT, 84107-7066

Samuel J Allen; Chelsea Dm Allen (Jt)
264 W Noah Cir
Murray , UT, 84107-7066

Marci May Meyers
227 W Clay Park Dr
Murray , UT, 84107-7021

Scott Nelson;
Roxanne Nelson (Jt)
268 W Noah Cir
Murray , UT, 84107-7066

Shawn R Hunter; Jamie R Hunter (Jt)
273 W Noah Cir
Murray , UT, 84107-7066

Blair B Little; Kortney A Little (Jt)
6226 S Clear St
Murray , UT, 84107-7002

Robert D Cook; Arlene R Cook (Jt)
6262 S Clear St
Murray , UT, 84107-7002

William Brian Simons
6216 S Clear St
Salt Lake City , UT, 84107-

Bryce E Park; Erica A Park (Jt)
267 W Noah Cir
Murray , UT, 84107-7066

Kathlene A White Living Trust
04/18/2006
6113 S Clear St
Murray , UT, 84107-7043

Bryan Tortora
6123 S Clear St
Murray , UT, 84107-7043

Don Sjoblom
6103 S Clear St
Murray , UT, 84107-7043

Matthew J Bacca; Mary L Bacca (Jt)
6143 S Clear St
Murray , UT, 84107-7043

Aaron Smyth; Anne Marie Smyth (Jt)
6153 S Clear St
Murray , UT, 84107-7043

Eric B Reynolds
6133 S Clear St
Murray , UT, 84107-7043

Larry R Swensen
5224 S Spring Clover Dr
Murray , UT, 84123-8415

Paul C Knott; Sherie J Knott (Jt)
6112 S Clay St
Murray , UT, 84107-7038

Kathleen Fredrickson
6163 S Clear St
Murray , UT, 84107-7043

Wesley E Swensen
6132 S Clay St
Murray , UT, 84107-7038

Sydney K Lafeen; Bryce A Tuttle (Jt)
6142 S Clay St
Murray , UT, 84107-7038

Andrew A Elser
7553 S Casa Blanca Dr
Midvale , UT, 84047-2851

Thomas Wilson; Mikah Koss (Tc)
6162 S Clay St
Murray , UT, 84107-7038

Spencer Nielsen
6175 S Clear St
Murray , UT, 84107-7043

Series L 6152 Clay St; An Individual
Series Series L 6152 Clay St
195 E Vine St
Murray , UT, 84107-4838

Travis J Carrell
228 W Creek Dr
Murray , UT, 84107-7046

Steven C Fivas
243 W Creek Dr
Murray , UT, 84107-7045
** returned in mail**

Kathryn D Child
236 W Creek Dr
Murray , UT, 84107-7046

Terry E Thompson (Tc)
225 W Creek Dr
Murray , UT, 84107-7045

Joseph Trujillo; Victoria Trujillo (Tc)
215 W Creek Dr
Murray , UT, 84107-7045

Dwight G Jarvis
233 W Creek Dr
Murray , UT, 84107-7045

Ronald T Pugmire
195 W Creek Dr
Murray , UT, 84107-7042

Jerry Shorter; Martha Shorter (Jt)
8875 S 1240 E
Sandy , UT, 84094-1905

Trust Not Identified
205 W Creek Dr
Murray , UT, 84107-7045

Keith W Hales
6264 S Cedar St
Murray , UT, 84107-7036

Tala Kamil Sweidan; Clara Kamil
Sweidan (Jt)
6217 S Clear St
Murray , UT, 84107-7001

Elizabeth A Slusser
6210 S Cedar St
Murray , UT, 84107-7036

Camille Acord; Michael Acord (Jt)
224 W Clay Park Dr
Murray , UT, 84107-7017

Silvino Gutierrez Munoz
206 W Clay Park Dr
Murray , UT, 84107-7017

Christopher S Gulden;
Mary Ann Gulden (Jt)
6237 S Clear St
Murray , UT, 84107-7001

Cindy L Reynolds
178 W Clay Park Dr
Murray , UT, 84107-7088

Jeremy Saxton; Jessica Saxton (Jt)
164 W Clay Park Dr
Murray , UT, 84107-7088

Grant Goeckeritz;
Nora Goeckeritz (Jt)
192 W Clay Park Dr
Murray , UT, 84107-7088

JC Storage, LLC
26 S 1185 E
Pleasant Grove , UT, 84062-

David H Jones
Po Box 26062
Salt Lake City , UT, 84126-0062

Teresa Oldham
3319 W Copper Point Ct
South Jordan , UT, 84095-5680

David H Jones
Po Box 26062
Salt Lake City , UT, 84126-0062

D & T Investments LLC
6152 S Stratler St
Murray , UT, 84107-6984

Trust Not Identified
495 E Calinas Creek Cir
Murray , UT, 84107-6600

D & T Investments LLC
6152 S Stratler St
Murray , UT, 84107-6984

Cory E Davies
6113 S Clay St
Murray , UT, 84107-7037

Joseph T Sorenson
6440 S Wasatch Blvd # 105
Holladay , UT, 84121-3559

Joseph T Sorenson
6440 S Wasatch Blvd # 105
Holladay , UT, 84121-3559

Brett Lafeen; Stevie Lafeen (Jt)
6143 S Clay St
Murray , UT, 84107-7037

Midrail Properties 2 Condo Owners
Association
5836 S Meadow Crest Dr
Murray , UT, 84107-6511

Luana L Slaugh
6103 S Clay St
Murray , UT, 84107-7037

Blythe E Mason;
McLean Aaron Mason (Jt)
6173 S Clay St
Murray , UT, 84107-7037

Jesse Winn
6123 S Clay St
Murray , UT, 84107-7037

Gloria C Chappell Revocable Living
6133 S Clay St
Murray , UT, 84107-7037

Gary S Axbom;
Brenda M Axbom (Tc)
6122 S Cedar St
Murray , UT, 84107-7034

Andres Perez Ortiz
6153 S Clay St
Murray , UT, 84107-7037

Greg A Lafeen; Deanna K Lafeen (Jt)
6163 S Clay St
Murray , UT, 84107-7037

Danny L. Carr Living Trust
6152 S Cedar St
Murray , UT, 84107-7034

Carolyn E Davis
6102 S Cedar St
Murray , UT, 84107-7034

Elisa D Eisert
6112 S Cedar St
Murray , UT, 84107-7034

Matthew D Schneider;
Heydon K Kaddas (Jt)
6185 S Clay St
Murray , UT, 84107-7037

Jenny M Hutchison
6132 S Cedar St
Murray , UT, 84107-7034

Zz Property Management LLC
789 E Forest Side Ln
Murray , UT, 84107-

Tyler L Johnson;
Calli M Johnson (Jt)
6196 S Cedar St
Murray , UT, 84107-7034

Anthony W Johnson;
Jeannie M Lowe (Jt)
6162 S Cedar St
Murray , UT, 84107-7034

Robert J Flink; Jacqueline A Flink (Jt)
6172 S Cedar St
Murray , UT, 84107-7034

C Fam Tr
1137 W Johnson Ridge Ln
West Jordan , UT, 84084-3578

Alex J Huggard;
Amber M Huggard (Jt)
6184 S Cedar St
Murray , UT, 84107-7034

Brandy K Hilden; Madison P Miller (Jt)
192 W Creek Dr
Murray , UT, 84107-7044

Kirk L Waterfall;
Tawnya A Waterfall (Jt)
6151 S Cedar St
Murray , UT, 84107-7033

Dennis A Gilhespie; Sheila Nanette
Gilhespie (Jt)
6101 S Cedar St
Murray , UT, 84107-7033

Jon A Dansie; Michelle P Dansie (Jt)
6111 S Cedar St
Murray , UT, 84107-7033

Brandi Sajec; Layton Mckee (Jt)
6181 S Cedar St
Murray , UT, 84107-7033

Erin C Yerra
6131 S Cedar St
Murray , UT, 84107-7033
** returned in mail**

Kirl L Waterfall; Tawnya A Waterfall (Jt)
6141 S Cedar St
Murray , UT, 84107-7033

LHB LIVING TRUST
6106 S Valley Dr
Murray , UT, 84107-7052
returned in mail

Utah Communications, Inc
1202 S 300 W
Salt Lake City , UT, 84101-3047

D C W 2018 Living Trust
5022 S 1034 E
Salt Lake City , UT, 84117-5734

Patricia R Ward
6198 S Valley Dr
Murray , UT, 84107-7052

Hulda M Kniss (Jt)
6191 S Cedar St
Murray , UT, 84107-7033

Patricia R Ward
6198 S Valley Dr
Murray , UT, 84107-7052

Cathy Lynn Alderman; Terry Lee
≪Life Estate≫ Alderman
27 W 6100 S
Murray , UT, 84107-7055

Map Fmly Tr; Janice H Peterson
6192 S Valley Dr
Murray , UT, 84107-7052

Trust Not Identified
6196 S Valley Dr
Murray , UT, 84107-7052

Randy Lewis
15 W 6100 S
Murray , UT, 84107-7055

Cathy L Alderman; Terry L & Life
Estate & Alderman
21 W 6100 S
Murray , UT, 84107-7055

JFT
37 W 6100 S
Murray , UT, 84107-7055

Ph Fam Trust
36 W Creek Dr
Murray , UT, 84107-7076

Nina L Borzoni; Michael P Borzoni (Jt)
48 W Creek Dr
Murray , UT, 84107-7076

Scott Beer; Carliane Beer (Jt)
444 E Bridlewalk Ln
Murray , UT, 84107-6623

Weston Adam; Melanie Weston (Jt)
18 W Creek Dr
Murray , UT, 84107-7076

Barbara J Holmes
30 W Creek Dr
Murray , UT, 84107-7076

George Fenstermacher; Vicki
Fenstermacher (Jt)
42 W Creek Dr
Murray , UT, 84107-7076

Emir Delilovic; Senada Delilovic (Jt)
49 W Creek Dr
Murray , UT, 84107-7040

Alexandra N Benson
14 W Creek Dr
Murray , UT, 84107-7041

Sharron R Sharp; Craig S Sharp (Jt)
24 W Creek Dr
Murray , UT, 84107-7076

Emst
25 W Creek Dr
Murray , UT, 84107-7040

R S Liv Tr
35 W Creek Dr
Murray , UT, 84107-7040

Bradley Jordan; Michele Jordan (Jt)
6 W Creek Dr
Murray , UT, 84107-7041

Ryatt S Summers; Kelli S Summers (Jt)
11 W Creek Dr
Murray , UT, 84107-7040

Christina Davis
19 W Creek Dr
Murray , UT, 84107-7040

Dotty Riley
31 W Creek Dr
Murray , UT, 84107-7040

Elicia Lawson; Brian Pye
38 W Valley Dr
Murray , UT, 84107-7050

Dalton Real Properties, LLC
4669 S Rainbow Dr
Murray , UT, 84107-3809

Tim D Erickson
15 W Creek Dr
Murray , UT, 84107-7040

Cindy Call
20 W Valley Dr
Murray , UT, 84107-7050

Kelsey M Le; Manila H Le (Jt)
32 W Valley Dr
Murray , UT, 84107-7050

Cody F Pace; Lindsey N Pace (Jt)
46 W Valley Dr
Murray , UT, 84107-7050

Candy L Young
8 W Valley Dr
Murray , UT, 84107-7050

Heather Lyn Ackley
16 W Valley Dr
Murray , UT, 84107-7050

Jennifer L Mangum
26 W Valley Dr
Murray , UT, 84107-7050

Quinten R. Bardsley; Larae Bardsley
6211 S Cedar St
Murray , UT, 84107-7035

DDMR Trust
4 W Valley Dr
Murray , UT, 84107-7050

Peter Borowczyk;
Alexandria Borowczyk (Jt)
10 W Valley Dr
Murray , UT, 84107-7050

Katherine Davis; Tyler Davis (Jt)
47 W Valley Dr
Murray , UT, 84107-7049

Rhonda Da Luz; Joao Da Luz (Jt)
6200 S Valley Dr
Murray , UT, 84107-7054

Nicholas T Mangome; Karen E
Mangome (Jt)
6203 S Cedar St
Murray , UT, 84107-7035

Michuel Austin Paify; Magali
Manriquez Gomez (Jt)
550 N Sir Anthony Cir
Salt Lake City , UT, 84116-2421

Clinton Feragen
43 W Valley Dr
Murray , UT, 84107-7049

Kyle R Timm; Linda M Timm (Jt)
8901 S Renegade Rd
Sandy , UT, 84093-1717

Gerry Stuart Swanson;
Jacqueline Rose Swanson (Jt)
7215 S Aerie Hill Dr
West Jordan , UT, 84081-3993

Russell P Koch
31 W Valley Dr
Murray , UT, 84107-7049

Spyder Adreon
39 W Valley Dr
Murray , UT, 84107-7049

Cheryl Sensing
2240 E Georgia Ave
Phoenix , AZ, 85016-3513

Matthew Monsen;
Natalie Blomquist (TC)
110 S Wellington Dr
Kaysville , UT, 84037-6733

Brandon Lafleure;
Sara B Graminske (Jt)
27 W Valley Dr
Murray , UT, 84107-7049

Larry D Martin; Shauna B Martin (Jt)
Po Box 573675
Salt Lake City , UT, 84157-3675

Cameron Andrews
3 W Valley Dr
Murray , UT, 84107-7049

Brian D Martin; Melanie Feeney (Jt)
15 W Valley Dr
Murray , UT, 84107-7049

Griggs Family Trust 05/11/2005
98 W Clay Park Dr
Murray , UT, 84107-7087

Barbara L Lehnhof
118 W Clay Park Dr
Murray , UT, 84107-7087

Cynthia Ann May
6261 S Cedar St
Murray , UT, 84107-7035

Richard A Evans
68 W Clay Park Dr
Murray , UT, 84107-7087

K Contreras; Erik M Contreras (Jt)
88 W Clay Park Dr
Murray , UT, 84107-7087

Yabing Luo
1050 Crestview Dr
Mountain View , CA, 94040-
** returned in mail**

Lynda Peterson
38 W Clay Park Dr
Murray , UT, 84107-7087

Chirstopher Mark Shenefelt;
Megan W Shenefelt (Jt)
58 W Clay Park Dr
Murray , UT, 84107-7087

Steven D Townsend;
Cindi J Townsend (Jt)
78 W Clay Park Dr
Murray , UT, 84107-7087

Raymond M Kelly Revocable Living Tr
6343 S Clay Park Dr
Murray , UT, 84107-7086

Guy M Adams; Marci Adams (Jt)
28 W Clay Park Dr
Murray , UT, 84107-7087

James & Ashley Dunkelberger
Revocable Trust 05/10/2018
48 W Clay Park Dr
Murray , UT, 84107-7087

Paul R Johnson;
Sherylee A Johnson (Jt)
6361 S Clay Park Dr
Murray , UT, 84107-7086

Verl Kenneth Williams;
Allison Williams (Jt)
6351 S Clay Park Dr
Murray , UT, 84107-7086

JLFT; MDCFT
22 W Clay Park Dr
Murray , UT, 84107-7087

Purple Lizard LLC
700 E 5600 S
Murray , UT, 84107-6432

Tikal Enterprises, LLC
372 W Winchester St
Murray , UT, 84107-7026

Charles W Jones; Rebecca M Jones (Jt)
6357 S Clay Park Dr
Murray , UT, 84107-7086

Richard E Castleberry;
Julia Castleberry (Jt)
7080 S 2400 W
West Jordan , UT, 84084-3020

Extra Space Properties 105 LLC
Po Box 800729
Dallas , TX, 75380-0729

Bryan Demann; Pamela Demann (Jt)
194 W Winchester St
Murray , UT, 84107-7238

Matthew K Jackson;
Ashley C Jackson (Jt)
198 W Winchester St
Murray , UT, 84107-7238

Bryce Demann;
Mary B Welch-Demann (Jt)
190 W Winchester St
Murray , UT, 84107-7238

William E Hansen
389 W Winchester St
Murray , UT, 84107-7061

Jennifer R Sorensen
180 W Winchester St
Murray , UT, 84107-7238

Purple Lizzard LLC
170 W Winchester St
Murray , UT, 84107-7238

Ryan C Hume; Sara J Hume (Jt)
9570 Hawkstone Way
Parker , CO, 80134-

Robert A & Jeneil Wahlen Family Trust
8655 S Danish Rd
Cottonwood Hts , UT, 84093-2108

Pine Mountain Properties, LLC
1908 E Rio Cir
Sandy , UT, 84093-6924

Michael Matthews;
Darlene Matthews (Jt)
6444 S Jefferson St
Murray , UT, 84107-7012

Rachel Leann Carlson
6430 S Jefferson St
Murray , UT, 84107-7012

Pine Mountain Properties, LLC
Po Box 350
Copperton , UT, 84006-0350

Trust Not Identified
6450 S Jefferson St
Murray , UT, 84107-7012

Christopher J Koerth; Kimberley A
Hutton (Jt)
6446 S Jefferson St
Murray , UT, 84107-7012

Elroy Barlow; Margie Barlow (Jt)
201 W Winchester St
Murray , UT, 84107-7097

Emily C Boley; Bruce W Pape (Jt)
6453 S Travis James Ln
Murray , UT, 84107-7094

Jeffrey Stephen White; Stephen C
White
6433 S Travis James Ln
Murray , UT, 84107-7094

Jackson B Riley; Rebecca Riley (Jt)
6440 S Jefferson St
Murray , UT, 84107-7012

CJB Living Trust Dated
9860 N Oquirrh View Dr
Eagle Mountain , UT, 84005-

Rex D Mills; Cindy J Mills (Jt)
6457 S Travis James Ln
Murray , UT, 84107-7094

Mark Dunn; Jana Dunn (Jt)
6448 S Jefferson St
Murray , UT, 84107-7012

William N Green
223 W Winchester St
Murray , UT, 84107-7023

Braden Cooper Living Trust 02/21/2020
6427 S Travis James Ln
Murray , UT, 84107-7094

Rodeina H Soweidan; Hassan Saad
6441 S Travis James Ln
Murray , UT, 84107-7094

James Fleurimond;
Dayra De Gaitan-Crespo (Jt)
6448 S Travis James Ln
Murray , UT, 84107-7006

Konstantin Gurlov
6428 S Travis James Ln
Murray , UT, 84107-7006

Blue Lake LLC
6471 S Travis James Ln
Murray , UT, 84107-7094

Utah Transit Authortiy
669 W 200 S
Salt Lake City , UT, 84101-1004

Marty Springer & Kristine Springer
Living Trust 01/25/2018
6456 S Travis James Ln
Murray , UT, 84107-7006

Matt Morris
233 W Winchester St
Murray , UT, 84107-7023

MJS Dental, Inc
6065 S Fashion Blvd #200
Murray , UT, 84107-7381

Hansen Crew Ltd
9463 S Wheatleigh Ct
South Jordan , UT, 84095-3353

Sheryl Weston
6436 S Travis James Ln
Murray , UT, 84107-7006

MW4 Investors, LLC
9045 S 1300 E
Sandy , UT, 84094-3134

Brad Olsen; Velia Olsen (Jt)
1744 E 11400 S
Sandy , UT, 84092-5430

Brett Mulvey; Melisa Mulvey (Jt)
6468 S Travis James Ln
Murray , UT, 84107-7006

L L C Erickson Investments
4294 S 615 E
Murray , UT, 84107-

Gary L Erickson; Tracy L Erickson (Jt)
2017 W 12310 S
Riverton , UT, 84065-

Trust Not Identified
Po Box 57861
Murray , UT, 84157-0861

Gustavo Meza
6426 S Blaine Dr
Murray , UT, 84107-7213

Ronald B Campbell;
Shamie J Campbell (Jt)
6436 S Blaine Dr
Murray , UT, 84107-7213

Hansen Crew Ltd
9463 S Wheatleigh Ct
South Jordan , UT, 84095-3353

Kennett D Galbraith;
Kimberly D Galbraith (Jt)
6460 S Blaine Dr
Murray , UT, 84107-7213

Erik M Bigler; Lisa M Bigler (Jt)
156 W 6480 S
Murray , UT, 84107-7242

Bah Liv Tr
6441 S Jefferson St
Murray , UT, 84107-7011

David L Fisher
140 W 6480 S
Murray , UT, 84107-7242

Marcus Autrand; Katie Pymm (Jt)
132 W 6480 S
Murray , UT, 84107-7242

Kenya K Arnett (Jt)
111 W Winchester St
Murray , UT, 84107-7237

Joseph P Allen; Wendy S Allen (Jt)
116 W 6480 S
Murray , UT, 84107-7242

Nicolas E Scott; Clarice Scott (Jt)
104 W 6480 S
Murray , UT, 84107-7242

Trust Not Identified
6448 S Blaine Dr
Murray , UT, 84107-7213

Colby Ellis
508 W Daniel Wy
Murray , UT, 84123-6511

John E Tibolla & Lucile Tibolla Inter
Vivos Trust 11/23/1994
135 W Winchester St
Murray , UT, 84107-7237

Trust Not Identified
146 W 6480 S
Murray , UT, 84107-7242

Craig Mcaffee
6451 S Jefferson St
Murray , UT, 84107-7801

Alan L. Mulvey; Penny K. Mulvey
141 W 6480 S
Murray , UT, 84107-7241

Lynda Garside
128 W 6480 S
Murray , UT, 84107-7242

Diane C Martin
129 W 6480 S
Murray , UT, 84107-7241

Eric Whitelock; Amy Wilkey (Jt)
117 W 6480 S
Murray , UT, 84107-7241

John E Tibolla; Lucile M Tibolla
135 W Winchester St
Murray , UT, 84107-7237

Jerry W James
Po Box 17172
Salt Lake City , UT, 84117-0172

Nita Patel; Kusum Patel (Jt)
6415 S Blaine Dr
Murray , UT, 84107-7212

Rex L Winn
151 W 6480 S
Murray , UT, 84107-7241

Alice Jensen
6437 S Blaine Dr
Murray , UT, 84107-7212

J & Lm Fam Tr
4995 S 1130 W
Taylorsville , UT, 84123-4415

Ronald T. Willes; Lu Jane K. Willes
133 W 6480 S
Murray , UT, 84107-7241

Bart Burton
87 W Winchester St
Murray , UT, 84107-7235

Colleen Bradshaw
83 W Winchester St
Murray , UT, 84107-7235

Justin W Keetch; Tiffany A Keetch (Jt)
105 W 6480 S
Murray , UT, 84107-7241

Mindee E Matagi; Isaac L Matagi (Jt)
96 W 6480 S
Murray , UT, 84107-7240

Bryan D Hale; Michelle W Hale (Jt)
88 W 6480 S
Murray , UT, 84107-7240

Clara Evans
6427 S Blaine Dr
Murray , UT, 84107-7212

Kristian Sammann
70 W 6480 S
Murray , UT, 84107-7240

Brian K Bracken; Jackie L Bracken (Jt)
6479 S Betty Gene Dr
Murray , UT, 84107-7107

Andrew G Metcalf
97 W Winchester St
Murray , UT, 84107-7235

J & Ecft
6418 S Malstrom Ln
Murray , UT, 84107-7215

Donna L Kani
6424 S Malstrom Ln
Murray , UT, 84107-7215

J & Lm Fam Tr
4995 S 1130 W
Taylorsville , UT, 84123-4415

Garrison Niel Powers Evans Trust
Stacy Marie Evans Trust
67 W Caleb Pl
Murray , UT, 84107-7803

Jf & Jml Trust
89 W Caleb Pl
Murray , UT, 84107-7803

Dalton Real Properties LLC
4669 S Rainbow Dr
Murray , UT, 84107-3809

James A Sherman
2371 Hillsboroughheights
Sandy , UT, 84092-3319

Elizabeth J Brimley
6461 S Blaine Dr
Murray , UT, 84107-7212

Zz Property Management,
LLC 59 Winchester
789 E Forest Side Ln
Murray , UT, 84107-

Cayman D Williams;
Alyssa Kummer (Jt)
6417 S Malstrom Ln
Murray , UT, 84107-7214

Holly Ann Herrera;
Bryce Scott Herrera (Jt)
6375 S Malstrom Ln
Murray , UT, 84107-7204

Brandon L Teidt;
Natasha M Tomovich (Jt)
6428 S Malstrom Ln
Murray , UT, 84107-7215

Robert A Gray; Rebecca B Gray (Jt)
147 W Clay Park Dr
Murray , UT, 84107-7090

Jennifer Stone Browne
6419 S Malstrom Ln
Murray , UT, 84107-7214

Garrison Evans; Stacy Evans (Jt)
67 W Caleb Pl
Murray , UT, 84107-7803

Javier Contreras Zamora;
Maria Cecilia Moreno (Jt)
117 W Clay Park Dr
Murray , UT, 84107-7090

Steven R Page; Shauntel Page (Jt)
137 W Clay Park Dr
Murray , UT, 84107-7090

Phenoy D Mahnken;
Irene L Mahnken (Jt)
2371 Hillsboroughheights
Sandy , UT, 84092-3319

Diane M Gonzalez
91 W Clay Park Dr
Murray , UT, 84107-7089

Nadeem Nasir; Dayna Orton (Jt)
109 W Clay Park Dr
Murray , UT, 84107-7090

Dakoda A Antelope
6421 S Malstrom Ln
Murray , UT, 84107-7214

Craig J Dietrich; Kimberly A Silvester (Jt)
6354 S Clay Park Dr
Murray , UT, 84107-7085

Corp Of Pb Of Ch Jc Of Lds
50 E Northtemple St #2225
Salt Lake City , UT, 84150-0022

Robert O Allen; Laurie S Allen (Jt)
157 W Clay Park Dr
Murray , UT, 84107-7090

Duy Tran
32 W Winchester St # 200
Murray , UT, 84107-5608

Cgl Lv Tr
6362 S Clay Park Dr
Murray , UT, 84107-7085

Keith I Harrop (Jt)
127 W Clay Park Dr
Murray , UT, 84107-7090

Cassity Family LLC
114 W Winchester St
Murray , UT, 84107-7238

Yu J Wang; Yi Wang (Jt)
167 W Clay Park Dr
Murray , UT, 84107-7090

Kay M Hunsaker (Surv)
103 W Clay Park Dr
Murray , UT, 84107-7090

Winpark Holdings, LLC
63 E 11400 S # 107
Sandy , UT, 84070-
returned in mail

DPPH LLC
114 W Winchester St
Murray , UT, 84107-7238

Duy Tran
32 W Winchester St # 200
Murray , UT, 84107-5608

Veritas United, LLC
1042 E ft union Blvd #1002
Murray , UT, 84107-7238
returned in mail

Cassity Family LLC
106 W Winchester St
Murray , UT, 84107-7238

Cassity Family LLC
114 W Winchester St
Murray , UT, 84107-7238

Cassity Family LLC
106 W Winchester St
Murray , UT, 84107-7238

Blue Fern LLC
102 W Winchester St #101
Murray , UT, 84107-7238

Dpph LLC
114 W Winchester St
Murray , UT, 84107-7238

Orion, Inc
8332 S Via Riviera Wy
Cottonwood Hts , UT, 84093-6532

Twin Peakes Holdings, Inc
6790 S 400 W
Midvale , UT, 84047-1009

Veritas United, LLC
122 W Winchester St
Murray , UT, 84107-7238
** returned in mail**

Gary K Dupaix Family Limited
Partnership
12032 S Hidden Valley Rd
Sandy , UT, 84092-5925

L L C J H P
4599 S Stockbridge Ln
Salt Lake City , UT, 84117-8057

Winchester Office Park Condominiums
325 Front St # 413
Evanston , WY, 82930-3633

L L C Alder Investments
6676 S 400 W
Murray , UT, 84107-

L L C Alder Investments
6676 S 400 W
Murray , UT, 84107-

Twin Peaks Holdings, Inc
6790 S 400 W
Midvale , UT, 84047-1009

6530 Commerce, LLC
3374 W Homstead Rd
Park City , UT, 84098-4839

Bird Leasing, LLC
6570 S 400 W
Murray , UT, 84107-

Rusted Spur LLC
1717 S Redwood Rd
Salt Lake City , UT, 84104-5110

Fsi Properties LLC
6763 S 400 W
Midvale , UT, 84047-1008

Clayburn, LLC
639 E Ocean Ave
Boynton Beach , FL, 33435-5016

L C Cole Machine Enterprises
6730 S 400 W
Midvale , UT, 84047-1009

Bc Landholdings, LLC
6645 S 400 W
Murray , UT, 84107-

Salt Lake County
Po Box 144575
Salt Lake City , UT, 84114-4575

Alder Investments LLC
6663 S 400 W
Murray , UT, 84107-

L L C E C J Leasing
6922 S Hollow Mill Dr
Cottonwood Hts , UT, 84121-3322
returned in mail

Patricia Lloyd; Sherry Lloyd (Jt)
6624 S Cottonwood St
Murray , UT, 84107-7010

B C Landholdings, LLC
6645 S 400 W
Murray , UT, 84107-

Gf Mac Investments, LLC
7889 S Prospector Dr
Cottonwood Hts , UT, 84121-5937
** returned in mail**

Salt River Investments, Inc
6767 S 400 W
Midvale , UT, 84047-1008

Pace Rental Properties LLC
6590 S Cottonwood St
Murray , UT, 84107-7008

6530 Cottonwood, LLC
7 E Pepperwood Dr
Sandy , UT, 84092-4932

379 Warehouse, LLC
1960 E Meadow Dr
Cottonwood Hts , UT, 84121-2961

Marathon Investments Group, LLC
10938 S Wood Stone Cir
South Jordan , UT, 84095-8272

Cottonwood Business Center, LLC
44 Red Pine Dr
Alpine , UT, 84004-

Egan Brothers Partnership
6680 S Cottonwood St
Murray , UT, 84107-7010

Salt River Investments, Inc
13366 S Pioneer St
Herriman , UT, 84096-4650

Jenn Investments, LLC
3759 E Catamount Ridge Wy
Sandy , UT, 84092-6044

Jenn Investments, LLC
3759 E Catamount Ridge Wy
Sandy , UT, 84092-6044

Zip84, LLC
4241 Rose Springs Rd
Erda , UT, 84074-

Findlay Dental Design Inc
8565 S Terrace Dr
Sandy , UT, 84093-1075

Gines Properties, LLC
6667 S Cottonwood St # 2
Murray , UT, 84107-7059

Coda Octopus Colmek, Inc
6526 S Cottonwood St
Murray , UT, 84107-7008

B & T Associates
1071 Crest View Dr
Mesquite , NV, 89027-8886

Ronald K Clifford
6649 S Cottonwood St
Murray , UT, 84107-7009

Cottonwood Business Center, LLC
44 Red Pine Dr
Alpine , UT, 84004-

Gord Cottonwood Properties, LLC
2432 S State St
South Salt Lake , UT, 84115-3137

Jwm Tr
7644 S State St
Midvale , UT, 84047-2006

J Jesus Cabrera Nunez
6670 S Cottonwood St
Murray , UT, 84107-7010

Derek D Dewey
6498 S Jefferson St
Murray , UT, 84107-7012

Stephen Tyler Kirkham;
Amelia Kirkham (Jt)
6494 S Jefferson St
Murray , UT, 84107-7012

Tel Equipment, LLC
Po Box 95728
South Jordan , UT, 84095-0728
** returned in mail**

Lori Jean Spiers; Garth Spiers (Jt)
6560 S Jefferson St
Murray , UT, 84107-7014

J Deloy Shaw (Jt)
6502 S Jefferson St
Murray , UT, 84107-7800

Omega Investments LLC
6795 S Cottonwood St
Midvale , UT, 84047-1054

R & Jm Fam Tr
6479 S Travis James Ln
Murray , UT, 84107-7094

Scott D Wayman; Judy Wayman (Jt)
208 W Travis James Ln
Murray , UT, 84107-7802

B & T Associates Lc
6436 S 1680 E
Murray , UT, 84121-2570

Justin Sparks
6518 S Jefferson St
Murray , UT, 84107-7800

Mathew C Schilling;
Shelli A Schilling (Jt)
6510 S Jefferson St
Murray , UT, 84107-7800

David Hagen
Po Box 877
Draper , UT, 84020-0877

Marcos Losada-Perez;
Benigno Losada-Perez (Jt)
6499 S Travis James Ln
Murray , UT, 84107-7094

Jonathan W Stone; Tina B Stone (Jt)
214 W Lisa Rae Cir
Murray , UT, 84107-7000

Jared Carmichael;
Marie Carmichael (Jt)
6496 S Jefferson St
Murray , UT, 84107-7012

Steven Jensen; Diana L Jensen (Jt)
218 W Lisa Rae Cir
Murray , UT, 84107-7000

Dennis L Peacock (Jt)
219 W Lisa Rae Cir
Murray , UT, 84107-7000

Trust Not Identified
200 W Lester Ave
Murray , UT, 84107-7018

Ian George; Cristy George (Jt)
223 W Lisa Rae Cir
Murray , UT, 84107-7000

Richard B Fowlks; Deleen P Fowlks (Jt)
208 W Lester Ave
Murray , UT, 84107-7018

Terry D Long; Wendee D Long (Jt)
6476 S Travis James Ln
Murray , UT, 84107-7006

Becky Dawson
6582 S Jefferson St
Murray , UT, 84107-7014

Jeffrey D Jorgensen;
Tara C Jorgensen (Jt)
6588 S Jefferson St
Murray , UT, 84107-7014

Bradosty Family LLC
299 S Main St
Salt Lake City , UT, 84111-1941
** returned in mail**

Brent John Holmquist; Debra
Holmquist (Jt)
6566 S Jefferson St
Murray , UT, 84107-7014

Daniel Christensen;
Joanne Christensen (Jt)
6554 S Jefferson St
Murray , UT, 84107-7014

Eli Maxfield; Kayli Mckarra Maxfield (Jt)
6538 S Jefferson St
Murray , UT, 84107-7014

Brent John Holmquist; Debra
Holmquist (Jt)
6562 S Jefferson St
Murray , UT, 84107-7014

Chloe Place Homeowners Assoc, Inc.
218 W Lisa Rae Cir
Murray , UT, 84107-7000

Stephen Bergquist;
Jennifer Bergquist (Jt)
224 W Lisa Rae Cir
Murray , UT, 84107-7000

Jamshid Dehghani
6576 S Jefferson St
Murray , UT, 84107-7014

KCW Land LLC
8859 S 1275 E
Sandy , UT, 84094-1950

Blakely Hankins; Spencer Hankins (Jt)
206 W Lisa Rae Cir
Murray , UT, 84107-7000

Cory Tueller; Stephanie Tueller (Jt)
889 W Walden Meadows Dr
Murray , UT, 84123-5477

A & I Property Management, LLC
3088 W Royal Meadows Wy
South Jordan , UT, 84095-3050

Michael R Slater; Stephanie D Slater (Jt)
217 W Lisa Rae Cir
Murray , UT, 84107-7000

KBA Property Management LLC
3088 W 10275 S
South Jordan , UT, 84095-

Jade M Mcdermaid
95 W 6480 S
Murray , UT, 84107-7239

Christopher K Rodesch
6556 S Jefferson St
Murray , UT, 84107-7014

Cottonwood Landing Owners
Association Inc
Po Box 71590
Salt Lake City , UT, 84171-0590

Robert Later; Connie L Later (Tc)
6455 S Jefferson St
Murray , UT, 84107-7801

JLLFFT
200 W Lester Ave
Murray , UT, 84107-7018

Benjamin Workman
79 W 6480 S
Murray , UT, 84107-7239

CPH Tr
128 W Fayelle Ave
Murray , UT, 84107-7113

A Better Quality Home, LLC
6576 S Jefferson St
Murray , UT, 84107-7014

Jeremy Lunt
4664 W Atwater Ln
South Jordan , UT, 84009-7760

Kennedy Byrd
100 W Fayelle Ave
Murray , UT, 84107-7113

Nathan Q Longhurst;
Anita G Longhurst (Tc)
202 W Lisa Rae Cir
Murray , UT, 84107-7000

Grace Wieringa
112 W Fayelle Ave
Murray , UT, 84107-7113

S Tyler Kirkham; Amelia Kirkham (Jt)
123 W Fayelle Ave
Murray , UT, 84107-7112

Bristlecone Industries, LLC
6533 S Cottonwood St
Murray , UT, 84107-7007

Trust Not Identified
6518 S Betty Gene Dr
Murray , UT, 84107-7159

J&Ja Fam Tr
99 W Fayelle Ave
Murray , UT, 84107-7110

G Investment Group
6530 S Hinson St
Las Vegas , NV, 89118-

Marlin D. Anderson; Barbara E.
Anderson
111 W Fayelle Ave
Murray , UT, 84107-7112

David Andrew Huffman; April Patricia
Huffman (Jt)
6507 S Jefferson St
Murray , UT, 84107-7013

Clinton Rawlins; Lindsay Ross (Jt)
87 W 6480 S
Murray , UT, 84107-7239

Timothy J Vincent
6550 S Betty Gene Dr
Murray , UT, 84107-7159

Kimball C Ward
6548 S 130 W
Murray , UT, 84107-7106

RCS Ltr
136 W Fayelle Ave
Murray , UT, 84107-7113

Samantha L Wilkinson
135 W Fayelle Ave
Murray , UT, 84107-7178

RPT
7540 Foothill Dr
Lake Point , UT, 84074-9249

Shawna B Packer
122 W Fayelle Ave
Murray , UT, 84107-7113

Robert R Despain
6551 S Jefferson St
Murray , UT, 84107-7072

L & Rpft
6555 S Jefferson St
Murray , UT, 84107-7072

SLW Tr
90 W Fayelle Ave
Murray , UT, 84107-7111

Shawn J Barr Trust
6571 S Jefferson St
Murray , UT, 84107-7072

Carla M Clark
6581 S Jefferson St
Murray , UT, 84107-7072

Shawn R Clayton; Linda K Clayton (Jt)
6553 S 130 W
Murray , UT, 84107-7105

Kevin Yates
102 W Lester Ave
Murray , UT, 84107-7117

Austin K Arce-Hallows; Sarah T Arce-
Hallows (Jt)
100 W Lester Ave
Murray , UT, 84107-7117

Trust Not Identified
87 W Fayelle Ave
Murray , UT, 84107-7110

Braewood Ltd
3989 S 900 E # 100
Salt Lake City , UT, 84124-1000

Courtney Hammer;
Blake Hammer (Jt)
120 W Lester Ave
Murray , UT, 84107-7117

Ocean Family Trust
6509 S Jefferson St
Murray , UT, 84107-7013

Randy Roberts; Amy Roberts (Jt)
6564 S John David Ln
Murray , UT, 84107-5710

Rob & Jill Hakes Family Trust
6567 S John David Ln
Murray , UT, 84107-5710

Christine Marie Jones;
David Allen Jones (Tc)
6513 S Jefferson St
Murray , UT, 84107-7013

LC Draper Oaks
67 E 6850 S
Midvale , UT, 84047-1215

Lc Draper Oaks
67 E 6850 S
Midvale , UT, 84047-1215

Robert C Johnson; Jenny Johnson (Jt)
6545 S Jefferson St
Murray , UT, 84107-7072

Cody S Curtis; Ashley D Curtis (Jt)
112 W Lester Ave
Murray , UT, 84107-7117

Mariam Jackson
108 W Lester Ave
Murray , UT, 84107-7117

Shawn J Barr
6575 S Jefferson St
Murray , UT, 84107-7072

Robert W. Boettcher; Barbara A.
Boettcher
6493 S Betty Gene Dr
Murray , UT, 84107-7107

Raymond B Stensrud & Nancy L
Stensrud Family Trust
6507 S Betty Gene Dr
Murray , UT, 84107-7109

Michael Allred; Mackenzie Sharette;
Miles Sharette (Jt)
106 W Lester Ave
Murray , UT, 84107-7117

Trust Not Identified
6533 S Betty Gene Dr
Murray , UT, 84107-7109

Levi Juston Kesler; Jessie Kesler (Jt)
6430 S Malstrom Ln
Murray , UT, 84107-7215

Braewood Ltd
3989 S 900 E # 100
Salt Lake City , UT, 84124-1000

Shu Xing Zhao
Po Box 27943
Salt Lake City , UT, 84127-0943

John V Ozberkmen
6545 S Betty Gene Dr
Murray , UT, 84107-7109

Jordan Valley Water Conservancy
District
8215 S 1300 W
West Jordan , UT, 84088-9422

Gary Evans; Cathryn Evans (Jt)
6470 S Malstrom Ln
Murray , UT, 84107-7215

Mark D Ogden; Desirae F Ogden (Jt)
6478 S Malstrom Ln
Murray , UT, 84107-7215

Johns Place Pud Homeowners
Association
110 W Lester Ave
Murray , UT, 84107-7117
** returned in mail**

Vayvang Keothammakhoun
6451 S Malstrom Ln
Murray , UT, 84107-7214

Robert T Rasmussen
38 W Malstrom Ct
Murray , UT, 84107-7356

Mark K Martin; Christine J Martin (Jt)
6517 S Betty Gene Dr
Murray , UT, 84107-7109

Trevor M Carr; Lindsey M Carr (Jt)
44 W Malstrom Ct
Murray , UT, 84107-7356

Amelia Chipman
668 Mya Ln
Idaho Falls , ID, 83402-5060

Christopher Mejia; Samantha Hamby
(Jt)
6434 S Malstrom Ln
Murray , UT, 84107-7215

Jody L Luthi; Frank R Luthi
5684 W 8030 S
West Jordan , UT, 84081-5927
** returned in mail**

Dana Dunbar; Brett L Leavitt (Jt)
37 W Malstrom Ct
Murray , UT, 84107-7300

Douglas R Paul
6557 S Betty Gene Dr
Murray , UT, 84107-7109

South 67 Condmn Common Area
Master Card
262 E 3900 S # 200
Murray , UT, 84107-1558

Bernadette Cordova
14 W Lester Ave
Murray , UT, 84107-7115

Taylor J Combs; Jessica Wixom Combs;
James Combs; Anne Elise Combs (Jt)
6431 S Malstrom Ln
Murray , UT, 84107-7214

Elizabeth R Wilson
1419 E Stanley Dr
Sandy , UT, 84093-2340

Randy H Brotherson
14 W Lester Ave # 14A
Murray , UT, 84107-

William Warner; Melissa Warner (Jt)
6459 S Malstrom Ln
Murray , UT, 84107-7214

Anthony J Stockdale;
Carly K Stockdale (Tc)
14 W Lester Ave # 16A
Murray , UT, 84107-

Ashlee Kunz
14 W Lester Ave
Murray , UT, 84107-7115
** returned in mail**

Louise C Jakeman; Kolby L Jakeman (Jt)
24 W Malstrom Ct
Murray , UT, 84107-7356

Elliott Wood
14 W Lester Ave # 21A
Murray , UT, 84107-7127

Blaine Bowden; Angela Bowden (Jt)
Po Box 460564
Leeds , UT, 84746-
** returned in mail**

Rayford V Leota; Dalexis Mei Leun
Leota (Jt)
23 W Malstrom Ct
Murray , UT, 84107-7300

David Clayton
1075 E Bates Canyon Rd
Erda , UT, 84074-

Deanne Colclough
14 W Lester Ave # 25A
Murray , UT, 84107-7161

Renza Irrevocable Trust
Po Box 9808
Salt Lake City , UT, 84109-9808

Lan-Fong Luk
4246 S 3425 W
West Valley , UT, 84119-5003

Brooke Jensen
14 W Lester Ave # 28A
Murray , UT, 84107-

Jam Liv Tr
14 W Lester Ave # 15A
Murray , UT, 84107-7148
** returned in mail**

Zackary J Schwartz
20 W Lester Ave
Murray , UT, 84107-7115

Jonathan M Davies
20 W Lester Ave # 13B
Murray , UT, 84107-
** returned in mail**

14 West Lester, LLC
617 E Par Three Ln
Murray , UT, 84107-7691

Ginger Bair
20 W Lester Ave
Murray , UT, 84107-7115

Jolynne D Edwards
20 W Lester Ave # 16B
Murray , UT, 84107-

Heidi E Anderson
14 W Lester Ave
Murray , UT, 84107-7115

20 West Lester Avenue LLC
380 N 200 W # 112
Bountiful , UT, 84010-7075

Denise Rodriguez
20 W Lester Ave
Murray , UT, 84107-7115

Candice R Mcphee; Ian C Mcphee (Jt)
22210 Bridgestone Pine Ct
Spring , TX, 77388-3535
** returned in mail**

Anna Sedillo
20 W Lester Ave
Murray , UT, 84107-7115

Jacie-Cole Webster
20 W Lester Ave # 24B
Murray , UT, 84107-

Grace L Redmond
20 W Lester Ave # 11B
Murray , UT, 84107-7128

Lauren Salvatore
20 W Lester Ave
Murray , UT, 84107-7115

Kevin You; Julie You (Jt)
20 W Lester Ave
Murray , UT, 84107-7115
** returned in mail**

Joanne Saltas
20 W Lester Ave # 14B
Murray , UT, 84107-

K & Dh Fam Tr
8365 W Bajada Rd
Peoria , AZ, 85383-3883

Jerry J Capito
24 W Lester Ave # 12C
Murray , UT, 84107-

William T Schmitz
20 W Lester Ave
Murray , UT, 84107-7115

Venda Seal Bytendorp
24 W Lester Ave
Murray , UT, 84107-7115

Steven Politis
24 W Lester Ave # 15C
Murray , UT, 84107-7164

Wayne Dykes; Marsha Rosati-Dykes (Jt)
20 W Lester Ave # B22
Murray , UT, 84107-

Barbara J Dawes
24 W Lester Ave
Murray , UT, 84107-7115

Dean Collett; Jill Fasy (Jt)
24 W Lester Ave # 18C
Murray , UT, 84107-

Mit Properties LLC
13273 S Corner Wood Dr
Draper , UT, 84020-3101

Valeria Quinteros
24 W Lester Ave
Murray , UT, 84107-7115

Melinda S Martin
24 W Lester Ave # 23C
Murray , UT, 84107-

Jordan Ragsdale
20 W Lester Ave
Murray , UT, 84107-7115

Loretta Digioacchino
24 W Lester Ave
Murray , UT, 84107-7115

Gerardo Martinez Santiago
24 W Lester Ave
Murray , UT, 84107-7115

Donna M Odell
24 W Lester Ave # 13-C
Murray , UT, 84107-

Aubree Keyser
24 W Lester Ave
Murray , UT, 84107-7115

Siesta Holdings LLC
7974 S Siesta Dr
Cottonwood Hts , UT, 84093-6276

Charles Burchett; Jean Burchett (Jt)
11943 S Cottage View Ln
Draper , UT, 84020-8223

Tori M Macie
30 W Lester Ave # 13D
Murray , UT, 84107-

Judy P Rapich
30 W Lester Ave # 14D
Murray , UT, 84107-

Vasily Arteeve; Irina Arteeve (Jt)
24 W Lester Ave
Murray , UT, 84107-7115

Alec Sauan Ocon
30 W Lester Ave
Murray , UT, 84107-7189

Natalie Dixon
30 W Lester Ave
Murray , UT, 84107-7189

Jamie Swenson
24 W Lester Ave # 24C
Murray , UT, 84107-

Kelsey Moss
30 W Lester Ave
Murray , UT, 84107-7189

Rainer Schmidt
30 W Lester Ave # D22
Murray , UT, 84107-

Vincent N King
24 W Lester Ave # 27C
Murray , UT, 84107-

Kory Burrows
30 W Lester Ave
Murray , UT, 84107-7189

John V Henrichsen
30 W Lester Ave # 25D
Murray , UT, 84107-7168

Zelda M Ewing
30 W Lester Ave # D12
Murray , UT, 84107-

Jasmine Rose
30 W Lester Ave
Murray , UT, 84107-7189

Linda A Roberson
30 W Lester Ave # D-28
Murray , UT, 84107-

Patricia C Baker
30 W Lester Ave # 15D
Murray , UT, 84107-7167

Karie A Ishino
34 W Lester Ave # E-12
Murray , UT, 84107-

Amanda C Deherrera
34 W Lester Ave # 13E
Murray , UT, 84107-
returned in mail

Amy Wilson
10462 S Weeping Willow Dr
Sandy , UT, 84070-4244

Brooke Zeman
34 W Lester Ave # 15E
Murray , UT, 84107-7170

Shawn S Dunn
9447 S Wheatleigh Ct
South Jordan , UT, 84095-3353

Jeremy Cornwall
30 W Lester Ave
Murray , UT, 84107-7189

Karlee Carter
34 W Lester Ave
Murray , UT, 84107-7189

Sheryl L Rees; Thomas D Rees (Jt)
800 N Icy Springs Rd
Coalville , UT, 84017-

30 W Lester Ave, LLC
881 W Baxter Dr
South Jordan , UT, 84095-8506

Jessica Simmons
452 N B St
Salt Lake City , UT, 84103-2544

Leslie Helmich
34 W Lester Ave
Murray , UT, 84107-7189

Tracee N Greene
4321 S 500 E
MillCreek , UT, 84107-2881
returned in mail

Danny R Gallegos
34 W Lester Ave # 26E
Murray , UT, 84107-

Bernadine Y Hiett
5102 S Stardust Dr
Taylorsville , UT, 84129-1267

Danielle Corson
34 W Lester Ave # 14E
Murray , UT, 84107-

Wendy L Karr
3334 S 825 E # 1
Salt Lake City , UT, 84106-1558
** returned in mail**

Vicki Millett
42 W Lester Ave
Murray , UT, 84107-7189

Jeanne P Stanford; Matthew L
Stanford; Rollin W Stanford (Jt)
34 W Lester Ave
Murray , UT, 84107-7189

Eneida J Irizarry
42 W Lester Ave # F14
Murray , UT, 84107-

Chandra Solt
42 W Lester Ave # 15F
Murray , UT, 84107-7175

Burton-Greninger LLC
3127 E Fort Union Blvd
Cottonwood Hts , UT, 84121-3438

Terry Alford
42 W Lester Ave # 17F
Murray , UT, 84107-

Lauralee Holley
42 W Lester Ave
Murray , UT, 84107-7189

Annette Kaye Kavoukas
34 W Lester Ave
Murray , UT, 84107-7189

Julie Shafizadeh
1620 E Langdale Cir
Cottonwood Hts , UT, 84093-6264

Kelly Kade Richardson
42 W Lester Ave
Murray , UT, 84107-7189

Karen Pomfret
34 W Lester Ave
Murray , UT, 84107-7189

Rachel Stott
42 W Lester Ave # F25
Murray , UT, 84107-7177

Kasandra A Brearton
42 W Lester Ave # 26F
Murray , UT, 84107-

Jeffrey N Anderson
42 W Lester Ave # 13F
Murray , UT, 84107-

Doug Jensen; Mary Bennett (Jt)
5041 S Rocky Rd
Taylorsville , UT, 84129-1229

Mgm Alliance Properties LLC
70 E 1430 N
Orem , UT, 84057-2700

Jimmie L Prettyman
42 W Lester Ave # F16
Murray , UT, 84107-

Lisa M Reynolds
46 W Lester Ave # 13G
Murray , UT, 84107-

Milo & Racquel Bishop Revocable Trust
7868 S Boston Cir
Cottonwood Hts , UT, 84121-5601

Blaine Bowden; Angela Bowden
42 W Lester Ave # 21F
Murray , UT, 84107-7137

Katie Larsen
46 W Lester Ave # G16
Murray , UT, 84107-

Zachary Loyd; Jenny Loyd (Jt)
46 W Lester Ave # 17G
Murray , UT, 84107-

Gloria Pena
42 W Lester Ave
Murray , UT, 84107-7189

Chad L Mills; Clara E Mills (Jt)
46 W Lester Ave # 21G
Murray , UT, 84107-7139
****returned in mail****

Walter Lee
46 W Lester Ave # G22
Murray , UT, 84107-

Mark Packer
42 W Lester Ave # 27F
Murray , UT, 84107-

David A Lacy Family Trust
46 W Lester Ave
Murray , UT, 84107-7189

Andrew S Hunter
46 W Lester Ave
Murray , UT, 84107-7189

Nicole Rasmussen
46 W Lester Ave
Murray , UT, 84107-7189

Patricia Westlake
46 W Lester Ave # 27G
Murray , UT, 84107-

Miles A Romney
46 W Lester Ave # 28G
Murray , UT, 84107-

Melissa B Black; James R Black (Jt)
46 W Lester Ave # 15G
Murray , UT, 84107-7188

Trust Not Identified
3835 W 8350 S
West Jordan , UT, 84088-5005

Alysha M Hernandez
10344 Lafoy Dr
Huntersville , NC, 28078-4661

Nathaniel P Jasper
46 W Lester Ave # 18G
Murray , UT, 84107-

Emily Jane Magill Trust
50 W Lester Ave
Murray , UT, 84107-7189
**** returned in mail****

Carleah Bernice Riches
50 W Lester Ave # H16
Murray , UT, 84107-

Linda K Burrows; Kristopher Brad
Burrows (Jt)
46 W Lester Ave # 23G
Murray , UT, 84107-

James & Robyn Hobbs Trust
50 W Lester Ave
Murray , UT, 84107-7189

Ian Davie
50 W Lester Ave
Murray , UT, 84107-7189

Angelique Pollock
46 W Lester Ave # 26G
Murray , UT, 84107-

Susan C Brenner
4840 Exeter Estates Lane
Wellington , FL, 33449-

Jesus Navarro;
Natasha Velasquez (Jt)
50 W Lester Ave
Murray , UT, 84107-7189

Connie L Beaty
50 W Lester Ave # 11H
Murray , UT, 84107-7140

Hyangmi Hogan
50 W Lester Ave
Murray , UT, 84107-7189

Nancy Marie Bovee
726 N Oakley St
Salt Lake City , UT, 84116-3836

Joell E Wilkins
50 W Lester Ave # 14H
Murray , UT, 84107-

54 W Lester Ave, LLC
Po Box 27772
Salt Lake City , UT, 84127-0772

M & LI Fam Tr
9662 S Rames Ct
South Jordan , UT, 84095-2457

Byron May
50 W Lester Ave
Murray , UT, 84107-7189
** returned in mail**

Gerie Brigham; Craig R Gill (Jt)
54 W Lester Ave # 14I
Murray , UT, 84107-

Inga S Tlatova
54 W Lester Ave # 15-I
Murray , UT, 84107-

Scott M Dawson; Ramona M Koegler-
Dawson (Jt)
50 W Lester Ave
Murray , UT, 84107-7189

Donald C Cook
54 W Lester Ave # 17-I
Murray , UT, 84107-

Arben Kurti
54 W Lester Ave
Murray , UT, 84107-7189

Amy E Reich
50 W Lester Ave # 25H
Murray , UT, 84107-7437

Stacey Chase
54 W Lester Ave # 22-I
Murray , UT, 84107-

Keith Jorgensen
5493 S Avalon Dr
Murray , UT, 84107-6221

David A Eliason
Po Box 2413
Sandy , UT, 84091-2413

Cody M Rudd; Sierra J Rudd (Jt)
54 W Lester Ave
Murray , UT, 84107-7189

54 W Lester Ave LLC
Po Box 27772
Salt Lake City , UT, 84127-0772

Brant Harris
54 W Lester Ave
Murray , UT, 84107-7189

Grovesland Lc
11615 S Temple Dr
South Jordan , UT, 84095-7842

Stephen A Boyer
3045 Porter Ave
Ogden , UT, 84403-

Anne Woolbright
54 W Lester Ave # I-16
Murray , UT, 84107-

Karrie Ann Ogilvie
62 W Lester Ave # 13J
Murray , UT, 84107-

Rachelle Bytendorp
62 W Lester Ave
Murray , UT, 84107-7189

54 W Lester Ave, LLC
Po Box 27772
Salt Lake City , UT, 84127-0772

Ami Williams
62 W Lester Ave # 16J
Murray , UT, 84107-

Janice Flavin; Michael Patrick Flavin (Jt)
62 W Lester Ave
Murray , UT, 84107-7189

Trust Not Identified
54 W Lester Ave # I-24
Murray , UT, 84107-

Oscar Ozuna Daniel
62 W Lester Ave # 21J
Murray , UT, 84107-7447
** returned in mail**

Carol Baye
62 W Lester Ave # 22J
Murray , UT, 84107-

Lydia Graham; Richard E Graham (Tc)
54 W Lester Ave # I-27
Murray , UT, 84107-

Cynthia M Attridge
62 W Lester Ave # J24
Murray , UT, 84107-

Leanne Gail Gallagher
62 W Lester Ave # 25J
Murray , UT, 84107-7145

Cjb Lvg Tr
62 W Lester Ave # 12J
Murray , UT, 84107-

Gloria H Knighton; Kevin L Knighton
(Jt)
62 W Lester Ave # J27
Murray , UT, 84107-

Linda Attaway
62 W Lester Ave
Murray , UT, 84107-7189

Chad L Mills; Clara E Mills (Jt)
8463 S 1520 W
West Jordan , UT, 84088-8258

Laurent Backman
66 W Lester Ave
Murray , UT, 84107-7189

Mackenzie Leiker; Robert Leiker
66 W Lester Ave
Murray , UT, 84107-7189

62 W Lester Ave, LLC
62 W Lester Ave
Murray , UT, 84107-7189
** returned in mail**

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MURRAY CITY
FASHION PLACE WEST
SMALL AREA PLAN

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INTRODUCTION

The Fashion Place West Small Area Plan provides a detailed plan for the area around the Fashion Place West TRAX station, a location that was identified as a priority in the 2017 Murray City General Plan. This Small Area Plan considers potential future development patterns in the area between the Fashion Place West TRAX station and Fashion Place Mall, and how the use of urban design and placemaking strategies can promote the establishment of a vibrant and well connected transit supported neighborhood—a key initiative in the General Plan.

With a population of 50,433 people in roughly 12 square miles, Murray is centrally located within the Salt Lake Valley. The Fashion Place West neighborhood and study area is located along Murray's southwestern border.

The study area is approximately 245 acres, which includes aging light industrial uses, Fashion Place Mall, two multi-family developments, and a stable residential neighborhood bisected by the I-215 interchange.

Given the potential for the eventual transition of the industrial areas, there are many opportunities to incorporate a mix of uses and attract new economic opportunity to the area. The expected population growth along the Wasatch Front anticipates an ongoing need for more variety in housing choice.

MURRAY CITY GENERAL PLAN

The current Murray City General Plan was adopted in 2017. The goal of the 2017 General Plan is to, "Guide growth to promote prosperity and sustain a high quality of life for those who live, work, shop, and recreate in Murray."

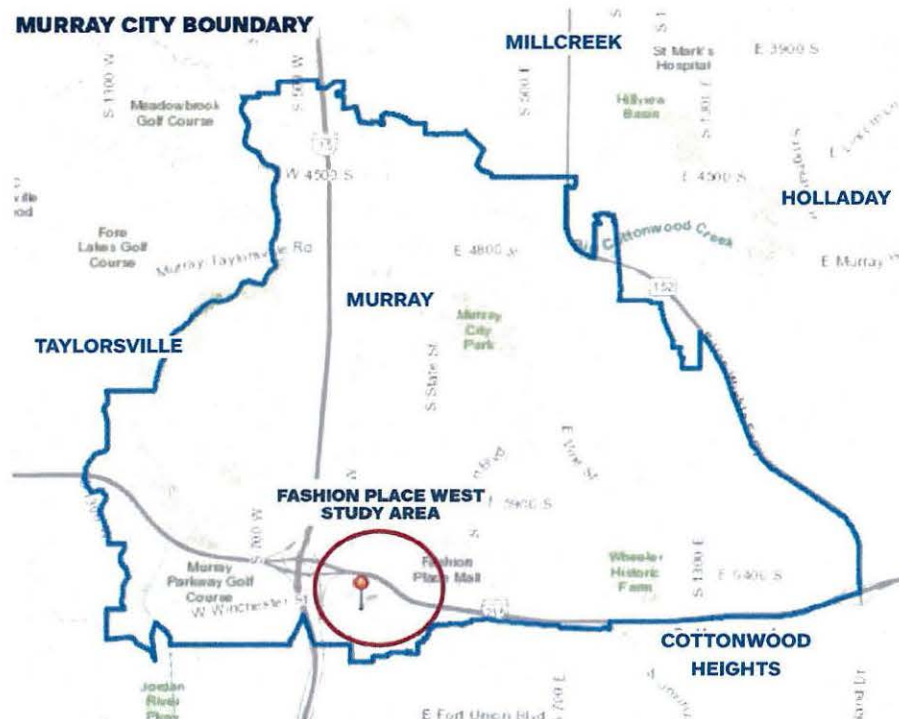


Figure 1.0 Map of Murray City and surrounding municipalities.

The initiatives that were identified in the General Plan plan were:

1. Build upon the existing City Center District
2. Create office and employment centers
3. Foster livable and vibrant neighborhoods
4. Link activity centers to surrounding areas
5. Create a City geared toward multi-modality

The Fashion Place West Small Area Plan and its vision will aggressively address Initiatives 3, 4, and 5. The 2017 General Plan also specifically identified the Fashion Place West station area as a priority for future small area planning and transit-oriented development.

1 EXECUTIVE SUMMARY

The Executive Summary provides a brief overview of the Small Area Plan goals, existing conditions, housing recommendations, connectivity suggestions, as well as possible implementation measures.

1.1 SMALL AREA PLAN GOALS

The following goals for the study area were established through the small area planning process:

- Strengthen relationship between TRAX station and Fashion Place Mall.
- Improve connectivity for the neighborhood.
- Improve overall neighborhood quality.
- Promote transit use and active transportation.

1.2 EXISTING CONDITIONS

The first step in the process is to understand the existing conditions as well as challenges that should be addressed within the Fashion Place West neighborhood.

1.2.1 ASSETS

The Fashion Place West study area is centrally located in Murray, in close proximity to many valuable community assets, such as the Fashion Place West TRAX station and Fashion Place Mall.

1.2.2 CHALLENGES AND OPPORTUNITIES

Challenges in the study area could limit achieving the goals of the plan if they are not acknowledged and addressed as part of the planning process. Challenges include bridges and major interstates bisecting the neighborhood and poor connectivity for vehicles, pedestrians, and cyclists.

Opportunities in the study area include:

- Future land use amendments to current irregular development patterns.
- Developing Jefferson Detention Basin as an activated park space.
- Using potential future expansion projects at Fashion Place Mall as an

opportunity for improved urban design and innovative solutions to provide increased connectivity.

1.2.3 BARRIERS TO DEVELOPMENT

Barriers to development within the study area include:

- Lack of City owned land that could spur private development.
- Current zoning regulations prohibiting density and growth including front yard setbacks, height limits, open space requirements, and parking requirements.
- The cost of construction and lack of labor force needed to expand development.

1.2.4 ECONOMIC CONDITIONS

Economic conditions in the Fashion Place West area are relatively similar to those of Murray City and Salt Lake County as a whole. The median age in the study area is 32.5 years, which is similar to the County and a bit younger than the City.

Median household income is lower in the study area (\$54,974) than the City (\$65,132) and the County (\$73,627). However, the access to jobs within the study area (7.4) is far higher than the County (6.4), but still below the City (8.2).

Taxable sales per capita in 2018 in Murray City, totaled \$2.28 Billion, approximately \$46,508 per resident. This is notably high in comparison to nearby cities, as shown by the data for South Jordan (\$21,907), West Valley (\$19,880), and West Jordan (\$15,990). Additionally, per capita statistics for Salt Lake County are \$25,092.

The metrics show that the study area could be a prime location to live and visit, given the strong economy. Additionally, these metrics illustrate the need for more affordable and diverse housing types as well as improved alternative transportation methods, especially between public transit and Fashion Place Mall.



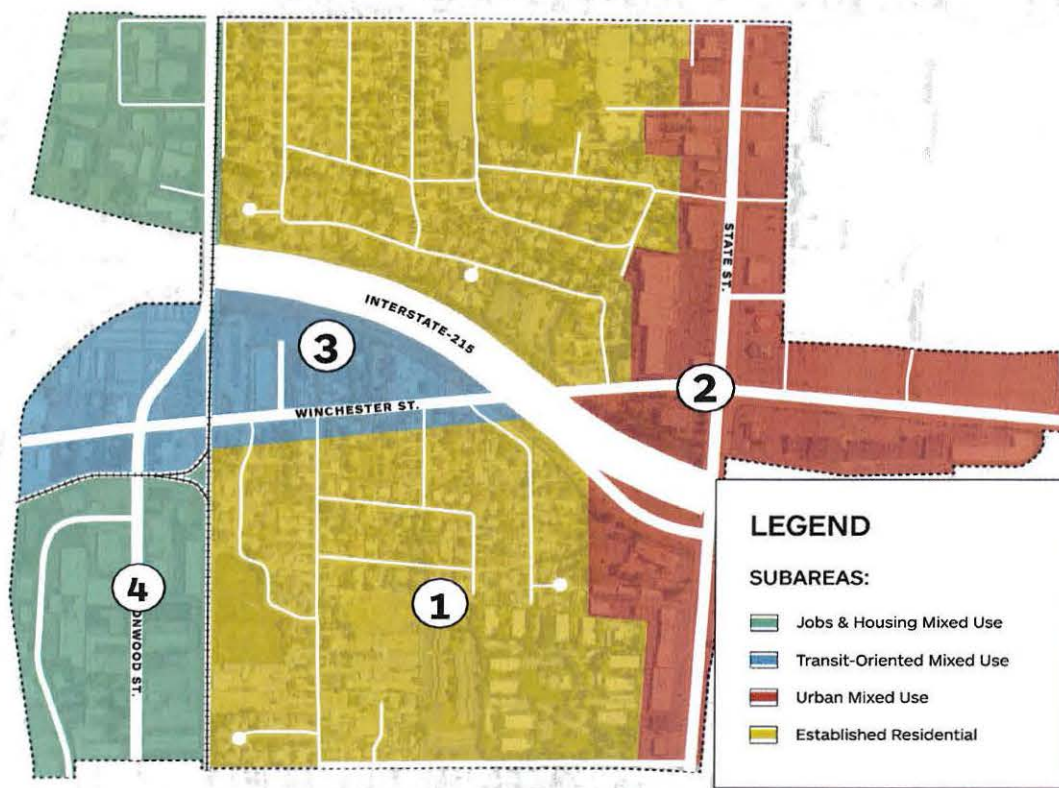


Figure 1.1 Map of subarea areas within the Fashion Place West study area. Residential use recommendations vary by subarea.

1.2.5 HOUSING TRENDS

Median Home Values in the study area are lower (\$239,474) than the City (\$318,596) and the County (\$327,451). The housing and transportation costs per household in the study area are 28 percent of household expenses compared to that of the County at 27 percent.

These statistics are an indication that the housing within the study area is more moderately priced, fulfilling a need in the region that is difficult to find, while also indicating that more diverse options should be encouraged and considered in the neighborhood.

1.2.6 CONNECTIVITY CONDITIONS

Connectivity within the study area is poor due to its geographic location and lack of streetscape amenities. Future improvements should address these issues and improve access between residential neighborhoods, as well as to and from the TRAX station and the Mall for all transportation types.

Current barriers include:

- Lack of bicycle infrastructure (with the exception of Winchester Street).
- Lack of pedestrian-friendly infrastructure at locations in, and adjacent to, Fashion Place Mall.
- Multiple residential neighborhoods lacking sidewalks.

1.3 HOUSING RECOMMENDATIONS

In order to maintain and protect the character of the established Fashion Place West neighborhood as well as promote growth around it, future development should be focused on providing more

diverse housing options. These options and housing recommendations should vary and be context sensitive depending on the location. Creating subareas will help to give specific recommendations on housing types that complement the surroundings.



1 EXECUTIVE SUMMARY

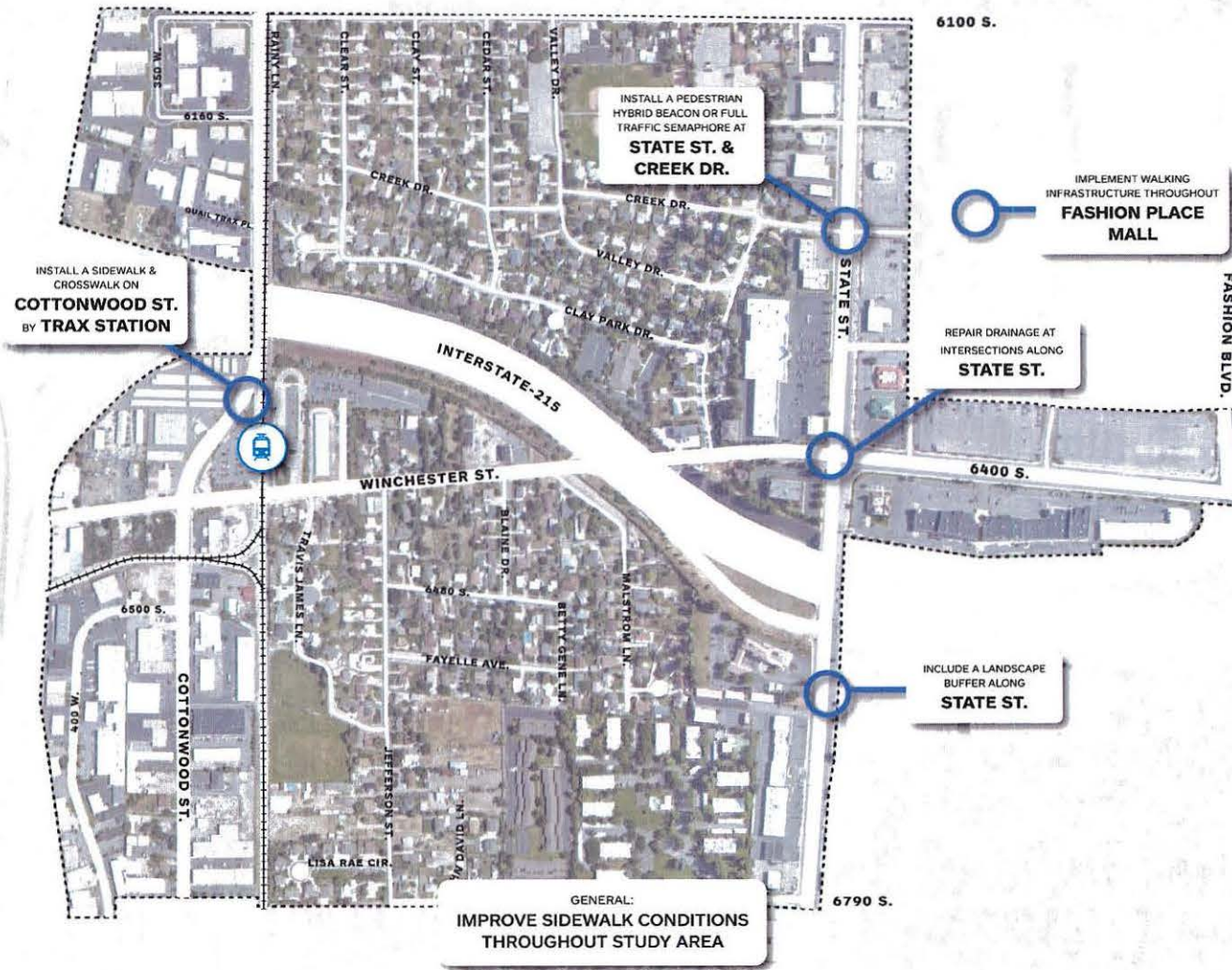


Figure 1.2 The Fashion Place West neighborhood lacks adequate infrastructure for pedestrians. The map above illustrates improvements that would improve the pedestrian experience in the study area.



Figure 1.3 Implementation recommendations include adopting a streetscape improvement plan that would include enhanced bicycle connectivity.

1.4 CONNECTIVITY RECOMMENDATIONS

Connectivity recommendations in the study area should be guided primarily by the way in which pedestrians and bicyclists access the Fashion Place West TRAX station and the mall. Additionally, vehicular travel between the north and south sides of the study area should be improved. Recommendations including streetscape improvements and bridge reconstruction are important to the flow in the study area with respect to vehicular traffic, public transit, as well as bicycle and pedestrian access.

Types of improvements should include:

- Updating overall active transportation connectivity between residential neighborhoods, the TRAX station, and Fashion Place Mall.
- Developing a parking strategy.
- Adopting a streetscape improvement plan to ensure future connectivity.

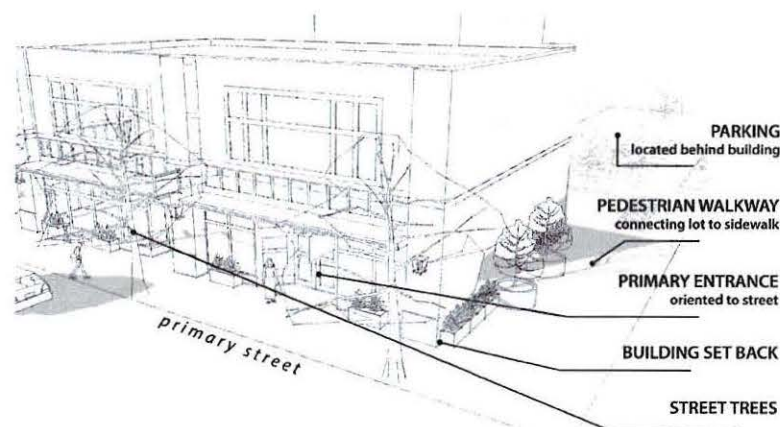


Figure 1.4 The diagram above illustrates the ideal placement of residential buildings to maximize the lot while addressing the street.

1.5 DESIGN GUIDELINES

Design Guidelines in the Fashion Place West study area should focus on creating an inviting environment for pedestrians, and a pleasant destination for residents and visitors. The guidelines should discuss elements such as:

- Building placement
- Building design
 - Ground floor details
 - Ground floor transparency
 - Prominent entrances
 - Treatment of blank walls
 - Articulation
- Signage design
- Street and streetscape design relating to active transportation and vehicular travel
- Parking lot design and location



1 EXECUTIVE SUMMARY

1.6 STRATEGIC IMPLEMENTATION MEASURES

1.6.1 INTRODUCTION

In order for the vision and objectives laid out in this plan to be realized, it will likely be the result of a long-term process, where residents, City staff, elected officials, as well as other public entities champion the vision to ensure the revitalization of the Fashion Place West study area that they want to see. The strategic implementation measures in this section present the vision and illustrative plan for the study area.

The implementation outlines phasing and policy recommendations for the Fashion Place West study area. They are intended to provide action items that the City, UTA, UDOT, and other stakeholders would need to complete in order for the area to succeed in becoming a vibrant transit-oriented neighborhood.

Strategic recommendations are broken down into the following five categories:

1. Housing
2. Connectivity
3. Policy Updates and Land Use Amendments
4. Phasing
5. Economic Development

MARKET FORCES

In discussions with local developers during the planning process, barriers were identified that may hinder future development and revitalization of the Fashion Place West study area. Some of the concerns included:

1. Existing parking requirements
2. Existing zoning
3. Lack of publicly controlled property

4. Lack of financial incentives (opportunity zone tax credits, TIF financing)
5. Lack of walkability
6. Vehicular connectivity issues

Some of these barriers could be addressed by amending necessary land use documents. Improving walkability and vehicular connectivity are issues that should be tackled first by drafting and adopting a plan that lays out phasing and responsibilities, so that all types of connectivity in the study area are improved.

1.6.2 HOUSING PRIORITIES

Housing priorities within the study area were determined by a combination of industry best practices, current market conditions, and desires of residents. These priorities include:

1. Offering services and amenities near housing
2. Providing housing for all stages of life
3. Creating a walkable neighborhood
4. Increasing allowable residential densities along, and adjacent to, the Fashion Place West TRAX station, I-15, and State Street



Figure 1.5 The implementation strategies recommend ordinance amendments that would allow a mix of uses at higher densities in the Fashion Place West neighborhood.





Figure 1.6 Improving the connectivity for vehicles, cyclists, and pedestrians is a key component of the implementation strategy in the Fashion Place West neighborhood.

5. Addressing established residential neighborhoods by creating appropriate transitions between existing residential and new, higher density developments
6. Incorporating a mix of uses into new residential developments as well as existing single-use zone districts

1.6.3 CONNECTIVITY PRIORITIES

Connectivity enhancements to the Fashion Place West study area should be centered around improved traffic flow and increased comfort for pedestrians and bicyclists. These include the following priorities:

1. Improving overall active transportation connectivity between residential neighborhoods, the TRAX station, and Fashion Place Mall
2. Developing parking strategy
3. Adopting a streetscape improvement plan to ensure future connectivity in key areas:
 - (a) Winchester Street
 - (b) Cottonwood Street
 - (c) Key intersections
 - (d) Fashion Place Mall access points

1.6.4 POLICY UPDATES AND LAND USE AMENDMENTS

1. Create new Fashion Place West overlay zone district (FPW). This new overlay zone should consider the following:
 - (a) Parking
 - (i) Include shared parking provision
 - (ii) Reduce residential parking requirements based on proximity to TRAX station and shared parking calculations
 - (iii) Implement parking maximums
 - (b) Consider reducing front yard setbacks from 15 feet and 25 feet, to 0 feet in order to encourage human scale development
 - (c) Implement maximum setback requirements
 - (d) Decrease open space requirements from 20 percent to 10 percent
 - (e) Implement Ground Floor activation recommendations
2. Support re-zoning areas within the study area boundaries per recommendations of the General Plan Future Land Use map:
 - (a) Commercial District (C-D) to Mixed-use (M-U)
 - (b) Manufacturing (MFG) to Fashion Place West Overlay (FPW)
 - (c) Residential Neighborhood Business (R-N-B) to Fashion Place West Overlay zone (FPW)



Figure 1.7 Housing priorities in the Fashion Place West Small Area Plan include zoning amendments to allow more housing types in close proximity to the TRAX station.

1 EXECUTIVE SUMMARY

1.6.5 PHASING

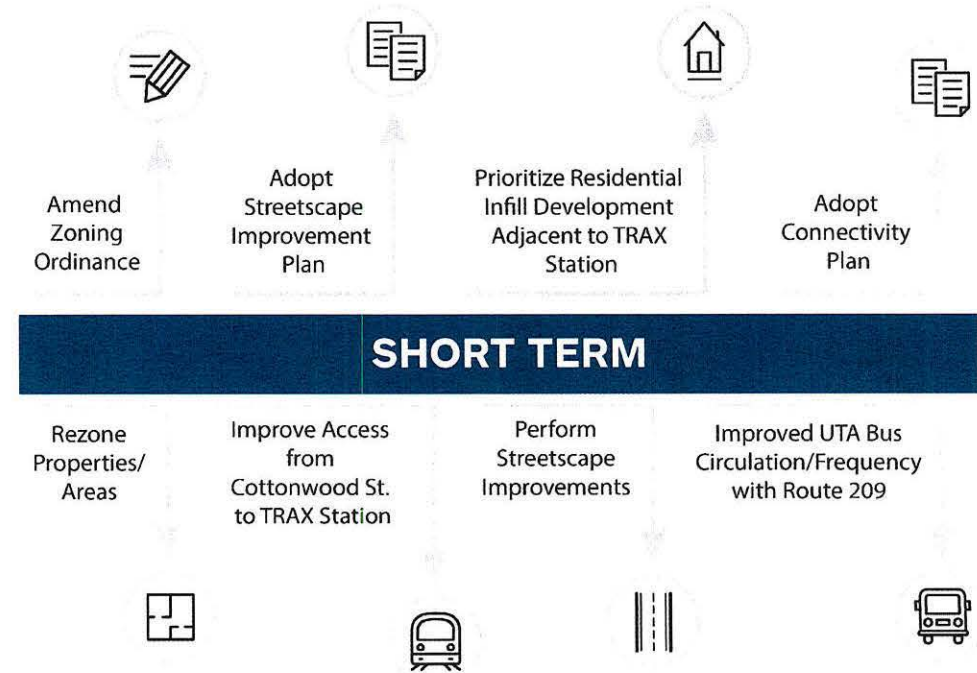
A phased approach to change to the Fashion Place West area aligns with the limitations of the City and development community. The three phases of redevelopment in the study area are detailed on the following pages, with discussion of responsible parties and needed collaboration amongst entities.

SHORT TERM

1. Adopt streetscape improvement and connectivity plans.
2. Prioritize residential infill development adjacent to TRAX station.
3. Perform streetscape improvements:
 - (a) Sidewalks
 - (b) Street trees
 - (c) Right-of-way changes:
 - (i) Bike lanes
 - (ii) Vehicular lane configurations
 - (d) Street lighting
4. Improved UTA bus circulation and frequency with Route 209.
5. Amend zoning ordinance and adopt Fashion Place West overlay zoning.

MEDIUM TERM

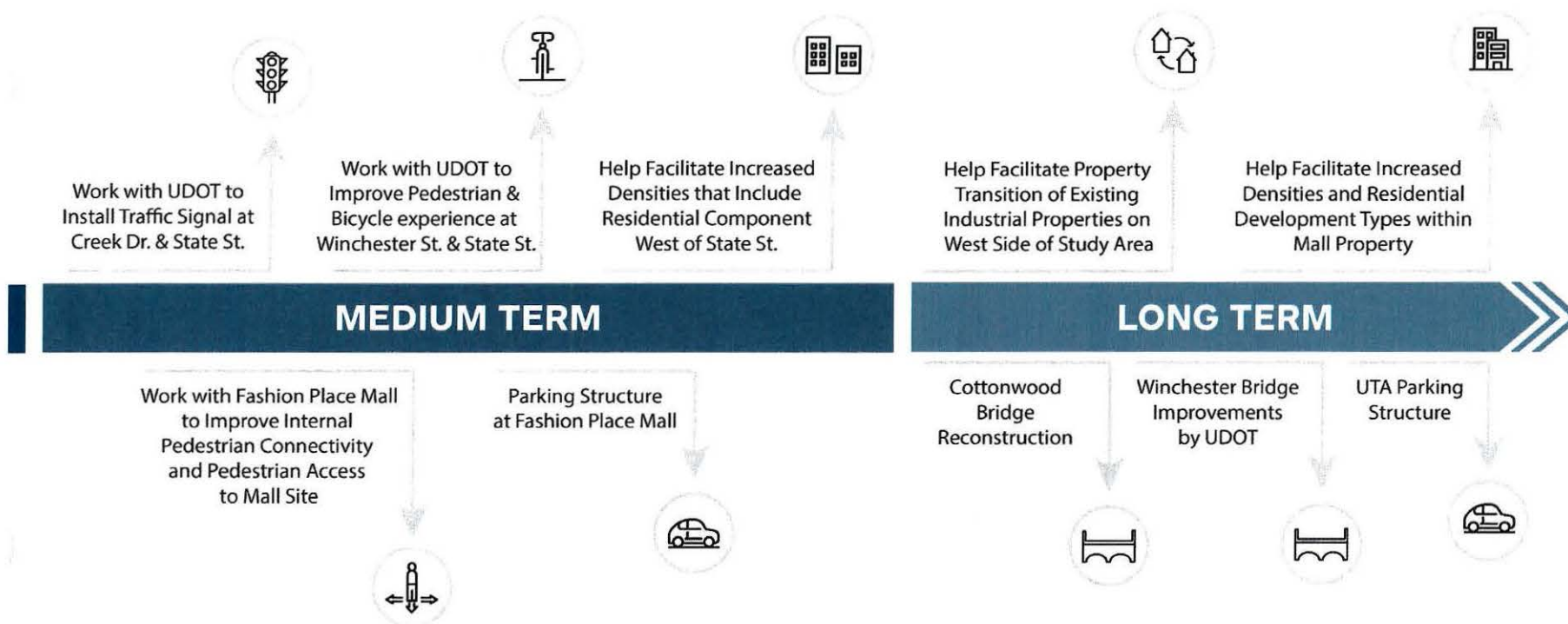
1. Work with UDOT to install a traffic signal at Creek Drive and State Street.
2. Work with Fashion Place Mall to improve internal pedestrian connectivity and pedestrian access to mall site.
3. Work with UDOT to improve pedestrian and bicycle experience at Winchester and State Street intersection.
4. Add a parking structure at the mall.
5. Help facilitate increased densities that includes residential component on West side of State Street.



LONG TERM

1. Reconstruction of Winchester and Cottonwood Street Bridges by UDOT.
2. Recommend construction of UTA Parking structure to facilitate development of a more mixed-use destination for the City.
3. Support the increase of densities and residential development types within mall property, especially adjacent to State Street and 6400 South
4. Facilitate property transition of existing industrial properties on west side of study area





FINANCIAL TOOLS AND INCENTIVES TO CONSIDER

1. Bonding
2. Future Budget Allocation
3. Public-Private Partnerships
4. CRA/RDA funding for housing developments
5. Grants
 - (a) UTA
 - (b) UDOT
 - (c) Other public transit related funding



2 EXISTING CONDITIONS



2.1 AREA HISTORY

The Fashion Place West station area hosts a centrally located UTA TRAX station, various types of light industrial and commercial businesses, an apartment complex, condo development, and approximately 200 single-family homes. The TRAX station is a jumping off point for shoppers, employees, and residents coming and going from around the Salt Lake valley. The area has been primarily occupied by light industrial and single-family residences since the neighborhood was originally developed.

For much of its history, the study area was dominated by agricultural production. Transportation corridors, both rail and auto, cut through this area early in the development of regional transportation networks. With State Street serving as a major north-south connection, a majority of development in the study area was focused on this corridor. Aerial photography from 1964 (Figure 2.0) illustrates the types of development found in the area prior to the introduction of the Interstate Highway system.

The study area is bisected by two interstate highways, Interstate 15 and the Interstate 215 beltway. The area is directly connected to I-215 via the State Street and 280 East exits. I-15 via I-215 can be accessed at the interchange located one mile west of the State Street exit, immediately adjacent to the western boundary of the study area.

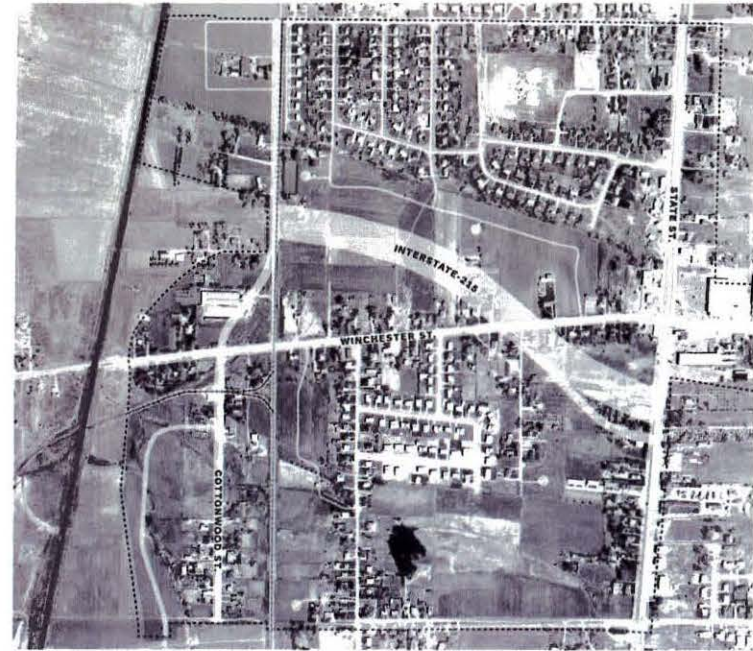


Figure 2.0 Study area, circa 1964, shown with modern-day roads as an overlay. Prior to Interstate Highway construction, the study area was primarily agricultural with suburban development along State Street.



Figure 2.1 Fashion Place Mall circa 1985.

2 EXISTING CONDITIONS

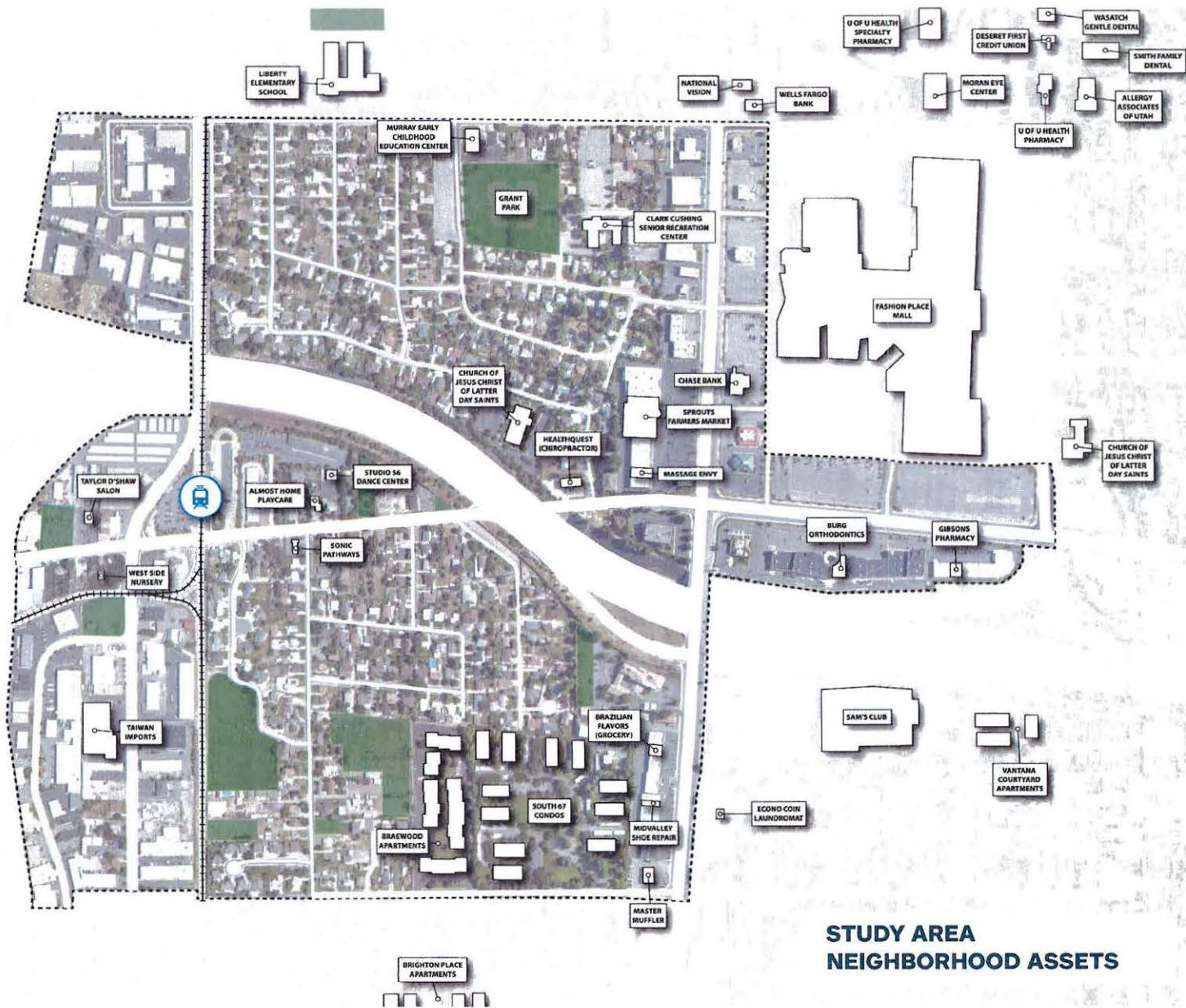


Figure 2.2 Study Area Asset Map



2.2 NEIGHBORHOOD ASSETS

The Fashion Place West study area is in close proximity to many valuable community assets, with many of those within the study area itself. Though isolated in some ways from the surrounding community, the neighborhood is in close proximity to major thoroughfares such as State Street, Cottonwood Street, Winchester Street, I-15, and I-215.

2.2.1 UTA TRAX STATION

The value and desirability of the Fashion Place West neighborhood is influenced by the presence of a UTA TRAX station. Having a TRAX station nearby with appropriately zoned properties can be very attractive to future property owners, residents, business owners, and developers. The Fashion Place West TRAX station is also one of the few stations in the south end of the system that serves both the Red and Blue lines.

2.2.2 CENTRAL LOCATION

The Fashion Place West study area is in a prime location from a regional perspective. The neighborhood's proximity to transportation networks that connect to the rest of the region gives the area great value. State Street offers motorists easy access to both I-15 and I-215 while TRAX offers a convenient mode of alternative transportation. By train, riders can reach downtown Salt Lake City in 23 minutes, the University of Utah in 24 minutes, and the Salt Lake International airport within 30 minutes.

TIME TO GET TO... via TRAX



12 minutes to Sandy
23 minutes to Downtown Salt Lake City
24 minutes to Daybreak
70 minutes to Provo
90 minutes to Ogden



Figure 2.3 Fashion Place Mall is a regional destination for the City of Murray and can be leveraged to attract more walkable, connected development to the study area.

2.2.3 FASHION PLACE MALL

Fashion Place Mall is a valuable and productive employment center and destination in the valley. This proximity gives the area a great opportunity to attract future higher density residential, office, and mixed-use walkable development. The Mall is also the largest generator of sales tax for Murray City.

2.2.4 COMMUNITY RESOURCES

The Fashion Place West neighborhood and vicinity have many community assets including Grant Park, Jefferson Detention Basin public space, two elementary schools, as well as Murray Senior Recreation Center. These and other community resources can be extremely attractive to prospective residents and are valued by current residents.

2 EXISTING CONDITIONS



Figure 2.4 Sidewalks and bike lanes on Winchester Street could benefit from improvement.

2.3 NEIGHBORHOOD CHALLENGES

While there are many community assets within the Fashion Place West study area, the neighborhood is also faced with its share of challenges. Many of the challenges within the study area are related to physical infrastructure as well as connectivity to and within the area.

The focus on motorists within the study area has resulted in an environment that disregards the needs of pedestrians and cyclists. This has created an unpleasant experience for those not inside a vehicle. Vehicle speed, road noise, as well as inconsistent and unattractive pedestrian facilities have created a community without much in the way of quality infrastructure. In addition to a lack of pedestrian infrastructure, the study area lacks standard cyclist and pedestrian amenities such as street trees, well marked bicycle lanes, seating, and well-marked frequent pedestrian crossings on major roadways.

The study area is located directly adjacent to the I-15 and I-215 interchange. These freeways act as major physical barriers to the area from the surrounding neighborhoods. These substantial barriers have

restricted the areas' development as a cohesive neighborhood. While bridges over these barriers offer a minimum level of pedestrian access, none of them offer a quality experience for pedestrians or cyclists.

The Cottonwood Street bridge is in close proximity to the TRAX station, and is a narrow two-lane bridge consisting of a single narrow sidewalk on the west side, and the TRAX rail on the east side, leaving virtually no room for expansion to consider pedestrians or cyclists. The Winchester Street bridge is along the most direct route to Fashion Place Mall from the TRAX station. This bridge is wider than the Cottonwood Street bridge and includes protected sidewalks on either side as well as striped bike lanes. The study area is primarily focused around the Fashion Place West TRAX station; however, the access to the station from the surrounding area is poor, isolating the station from destinations and services.

Fashion Place Mall lacks a relationship and connection to the neighborhood and also lacks connectivity within the mall site. Within the parking that surrounds the mall, clear pedestrian paths and sidewalks are absent. When pedestrians are approaching Fashion Place Mall on foot from State Street as well as the other surrounding roads, they are not welcomed with clear connections to the mall itself.

State Street is a Utah Department of Transportation (UDOT) controlled road. Currently, State Street's design focuses solely on motorist capacity, to the exclusion of all human-scale design through the study area. State Street has great potential in terms of redevelopment but this redevelopment can be challenging due to long time frames and strict regulations in place by UDOT. These constraints should be considered when proposing changes to the area, and additional time to collaborate with UDOT should be accounted for.





Figure 2.5 Multiple parcels in the study area prime for infill development.

2.4 BARRIERS TO DEVELOPMENT

The Fashion Place West study area is challenged with several barriers to future development that includes both physical and regulatory limitations. Physical barriers can include property ownership concerns or access and connectivity obstacles. Regulatory barriers to development can include elements such as capital improvement funding hurdles, zoning, or possible inter-agency road blocks. These barriers may not necessarily halt the planning and development process but should be considered hurdles to future development.

As a UDOT owned facility, State Street has a major impact on development patterns along this corridor. The process to working with UDOT to update their infrastructure is lengthy, and will need to be considered as development occurs along State Street.

Current zoning and land use regulations within the study area should be considered a regulatory barrier to development. In order for development or redevelopment to occur in the Fashion Place West neighborhood, zoning regulations, primarily along Winchester Street and other major thoroughfares, should be reevaluated to encourage

and allow a more diverse mix of uses, as well as higher density residential and mixed-use commercial developments. As such, design guidelines in the area will also need to be amended. Reducing front yard setbacks, changing height limitations, reducing open space requirements, and reevaluating parking requirements should also be considered to foster development.

Parking requirements and especially parking minimums can be a way for cities to regulate and ensure adequate parking for residential and commercial developments. However, strict parking requirements such as these can in also hinder development. Large, underutilized parking lots are often a result of strict parking minimum requirements. A more modern approach to parking management is to encourage and incentivize shared parking when possible.

At present, Murray City has not established a financial toolbox or programs to incentivize and encourage higher quality development within the Fashion Place West study area. Additionally, working with local entities to establish a redevelopment project area in this neighborhood would give the City and Redevelopment Agency the capacity to use property tax increment as a way to reimburse developers for burden costs associated with site conditions. Burden costs are defined as development costs that are unique to a particular site.

Another major barrier to development in 2020 is the cost of construction and lack of labor force needed to meet demand. With construction costs vastly out-pacing inflation, delaying major construction projects such as road rebuilds or streetscape improvements is only serving to increase their cost in significant ways. The City should prioritize which projects may have the largest impact and set a course of construction as soon as feasible.



2 EXISTING CONDITIONS

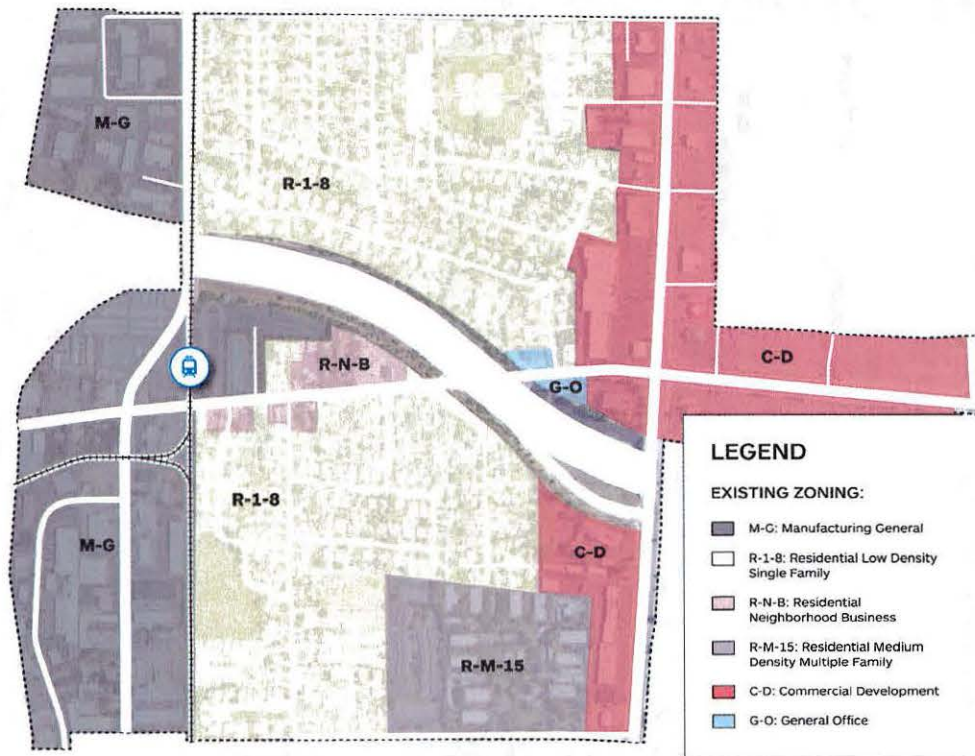


Figure 2.6 Map of existing zoning designations. Future zoning changes should be based on achieving the goals for the future of the Fashion Place West area.

2.5 LAND USE CONDITIONS

2.5.1 CURRENT LAND USES

Overall, the Fashion Place West neighborhood has a mix of uses that fall into three general land use categories: light industrial, commercial, and residential. These three use types are segregated from each other within the area between the eastern, central, and western areas.

The eastern most segment of the study area is primarily a commercial area. It includes properties along State Street and 6400 South, and

portions of the southern and western sides of the mall site.

The central portion of the study area is made up of two single-family residential neighborhoods, one north and one south of I-215, but also includes two multi-family developments and a few neighborhood businesses.

The western segment of the study area is predominately composed of light industrial uses. It is located along I-15, Cottonwood Street, and the TRAX corridor. This is the second largest section by land area and includes vacant and underutilized parcels.

2.5.2 CURRENT ZONING

The parcels within Fashion Place West study area boundary are designated as one of the following six zones:

- R-1-8 Low Density Single-Family
- R-M-15 Medium Density Multiple Family
- R-N-B Residential Neighborhood Business
- C-D Commercial Development
- M-G Manufacturing General
- G-O General Office

Zoning around the Fashion Place West TRAX station does not address the station itself in its zoning designations. Murray City should consider amending its zoning ordinance to adopt more guidelines that promote transit oriented development.



2.6 ECONOMIC CONDITIONS

2.6.1 POPULATION TRENDS

Utah's population is projected to increase from approximately 3 Million in 2015 to 5.8 Million in 2065. This represents an increase of 2.8 Million people with an annual average rate change of 1.3 percent. Although the rate of growth in population will decelerate over the next 50 years, it is still projected to exceed national growth rates.

Murray City as a whole has a population of 50,433. This places Murray toward the lower end of the spectrum compared to the population size of neighboring cities such as Taylorsville, Sandy, Millcreek, and Midvale. Murray City's population is projected to reach 67,668 residents by 2040. The Fashion Place West study area has a population of 1,714 residents in .55 square miles.

Salt Lake County has a population of approximately 1,150,000 residents, with an anticipated increase of more than 500,000 residents in the next 25 to 30 years. With the population throughout the state growing rapidly, there is ever-increasing pressure for the development of more residential units. This development pressure is and will continue to be felt across the state, in Murray, and in the Fashion Place West study area.

2.6.2 MEDIAN AGE

Murray has a median age of 36.6 which is higher than that of the Fashion Place West study area and the county-wide average of 32.5. The 32.3 year indicator in the study area is similar to those of neighboring cities but the City's 36.6 year indicator is much higher than



Figure 2.7 Study Area population as a percentage of Murray City's (U.S. Census data, Esri Forecast, 2019)

surrounding cities. Median age data is closely followed by developers and can impact housing choices and potential development types within the City.



Figure 2.8 The access to jobs in the study area is similar to that of the City, and better than the County average. This score is an index based on access to jobs and a variety of employment.

2.6.3 EMPLOYMENT AND JOBS

Total employment within Salt Lake County is estimated to reach 970,805 in 2020. By 2040, total employment is projected to reach 1,239,908, an increase of 269,103 employees, according to the Kem C. Gardner Policy Institute.

More central neighborhoods near employment centers have higher scores than others. When comparing Murray with the study area in particular, and the County, the study area is considered to have high access to jobs with a score of 7.4. Murray City has a score of 8.2 (out of 10), and Salt Lake County has a score of 6.4.

A total of 4,757 employees are in the Fashion Place West study area, which represents 2.77 employees per resident, a ratio far larger than the County and surrounding cities. This metric refers to the number



Figure 2.9 Median Age Comparison (Esri Demographic Profile, 2019)



2 EXISTING CONDITIONS

of employees in the community per resident. Cities with low ratios are reflective of limited jobs, bedroom communities, and typically high median household sizes (large families with children who are not employed). Areas which have higher ratios are typically reflective of employment centers or areas with lower median household sizes.

This large ratio may be due to the fact that Fashion Place Mall is within the study area. Cities further south show lower ratios as they generally are more representative of bedroom communities than employment centers.

2.6.4 MEDIAN HOUSEHOLD INCOME

Median household income in the Fashion Place West study area is \$51,974 per year, which is lower than Murray as a whole (\$65,132/year) and significantly less than the Salt Lake County average of \$73,627 per year.

A lower median income in this area can indicate lower educational attainment of residents of the study area, and can also correlate with added dependence on the transit system. Creating connections with opportunities for educational programs, as well as improved connectivity to transit service can have a large impact on neighborhood livability and opportunity access.

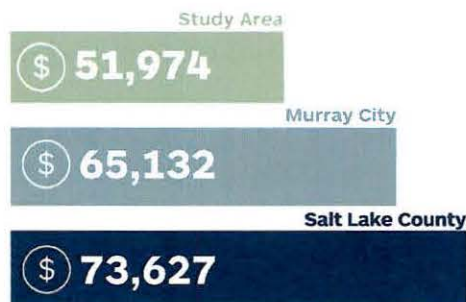


Figure 2.10 Median Household Income comparisons between the study area, Murray City and Salt Lake County. The study area has a significantly lower median household income than the rest of the City and County. (Esri Income Comparison Profile, 2019)

2.7 HOUSING TRENDS

2.7.1 HOUSING CONDITIONS

Of the approximately 245 acres and 777 parcels that make up the Fashion Place West Project study area, 577 or 74 percent of those are residential land uses. The remaining 200 parcels make up the other 26 percent of the parcels and are occupied by non-residential land uses.

2.7.2 HOUSING STOCK

The housing stock within the Fashion Place West neighborhood is aging. A majority of the existing homes were built in the 1960s and 1970s. Most of the single-family homes were built in the 1960s and are single story structures with various facade materials including brick, wood siding and stucco. The most recent single-family development was built in the mid 1990s. This development consists of predominately two-story homes with stucco facades.

There are two multi-family developments within the study area as well. The South 67 Condo development was built in the 1970s (with over 100 units) and is an individually owned town home type development. The Braewood Apartments is a five-building (51 unit) apartment complex, directly west of the condominium development.

Current zoning in the area permits accessory dwelling units (ADUs) as an allowable use with types that include basement apartments and apartments over garages. Accessory dwelling units are defined as a secondary unit within or on the same parcel as an owner occupied single-family home. Allowing and encouraging ADUs would create the opportunity to provide more diverse housing options to residents at affordable prices.

Areas with a diversity of housing choices are more stable and have more to offer to residents. A housing-diverse area would have a broad range



of housing types, rental and ownership options, at varying price levels that would include many options.

2.7.3 HOUSEHOLD SIZE

By the end of 2020, the median household size in Salt Lake County is estimated to increase to 2.78 however, it is projected to decrease to 2.53 by 2040, according to the Kem C. Gardner Policy Institute.

Median household size in Murray and the Fashion Place West study area specifically, are both reported to be an average of 2.57 which is slightly less than the county-wide average of 3. Neighboring cities like Taylorsville (3.0) and South Salt Lake (2.7) also have a slightly higher average household size.

The household sizes in Murray has remained largely unchanged in the last ten years, reflecting a trend similar to other cities in the central portion of Salt Lake County. Conversely, areas along the western and southern boundaries of the County have reflected high household sizes, primarily reflecting an influx of families into rapidly developing areas. Future trends will most likely show a continuing decline of median household size in developed, aging areas, while new growth areas will represent higher household sizes.

2.7.4 HOME VALUES

Housing prices in Murray have increased notably over the past several years, commensurate with trends experienced along the greater Wasatch Front. Values for single-family, multi-family and vacant land have all appreciated.

The median residential property value in Murray, as of 2019, was \$318,596. By 2024, the median home value is projected to be \$343,182. The Fashion Place West study area has a median home value lower than

the City average at \$239,474, with Salt Lake County shown at roughly \$327,451. Higher values are reflected in Midvale, West Jordan, Sandy, and Millcreek, while lower values (in relation to Murray) are exhibited by West Valley, Kearns, and Taylorsville.

Over the past decade, across the nation, homes in the most walkable neighborhoods were also the ones that appreciated the fastest. In two-thirds of large metro areas, walkable neighborhoods have higher home values than car-dependent ones.

The walkability premium in Salt Lake County (the difference in the average value of homes in walkable neighborhoods compared to the average value of homes in car-dependent neighborhoods) was 32 percent higher in 2019.

Current trends across the country also show that homes in walkable areas also gain value at a faster rate than those in car-dependent areas. For example, in Salt Lake County walkable homes increase in value 19 percent faster than those of car-dependent homes.

For the Fashion Place West study area, these statistics show that due to the TRAX station and proximity of this area to Fashion Place Mall, as the study area transitions to a more walkable and well-connected



Figure 2.11 Median Home Value Comparison (U.S. Census, 2010, Esri Forecast, 2019).

neighborhood, home values may be higher and may increase faster than other areas in the valley that are more car-dependent.



2 EXISTING CONDITIONS

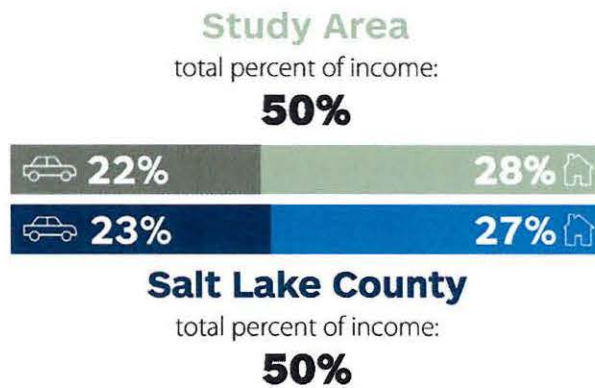


Figure 2.12. Housing and Transportation cost as a percentage of monthly income comparisons between the study area and Salt Lake County (CNT H+T Index, 2020).

2.7.5 INCOME SPENT ON HOUSING AND TRANSPORTATION

Murray households spend 24 percent of monthly income on housing, slightly below the county-wide level of 27 percent. Most nearby surrounding cities show percentages similar to Murray, while communities to the south reflect higher percentages, as housing costs are also notably higher.

Costs spent on transportation represent 21 percent of income for Murray residents, similar to the 23 percent shown for the County. Immediately surrounding cities reflect similar amounts, while south valley communities are spending a reduced portion of their income on transportation (near 15 to 16 percent). On average, Murray households spend roughly \$13,267 per year on transportation costs.

New development should consider the proximity of transportation options, and determine whether the ultimate cost of housing and transportation fits within the competitive range of total spending.

2.7.6 HOUSING AND TRANSPORTATION INDEX

By taking into account the cost of housing as well as the cost of transportation, the Center for Neighborhood Technology Housing and Transportation Affordability Index (CNT H+T Index) provides a more comprehensive understanding of the affordability of place.

While housing alone is traditionally deemed affordable when consuming no more than 30 percent of income, the H+T Index incorporates transportation costs (usually a household's second largest expense) to show that location efficient places can be more livable and affordable. Dividing these costs by the representative income illustrates the cost burden of housing and transportation expenses placed on a typical household.

According to the H+T Index, Murray is similar to Salt Lake County across key housing and transportation indicators such as annual transportation costs—both averaging approximately \$13,000 annually—illustrating that both jurisdictions having high access to a variety of jobs.

2.7.7 INCOME REMAINING AFTER HOUSING AND TRANSPORTATION

"Income remaining" indicates adjustments made to median household spending after transportation and housing. This metric indicates potential spending per household once essentials are covered. Remaining income after housing and transportation costs is comparable between Murray households and County households, with Murray at 55 percent remaining and the County at 50 percent remaining.

Also of note is the proximity to employment, which has become more of a consideration for new development. Some planners and developers have attempted to reduce the impact on roadways from new development by locating in areas with high job concentrations,



and by catering to those who want a reduced commute time. The Fashion Place West study area in particular is in an ideal location for new development given its proximity to transit and transportation networks.

2.7.8 AFFORDABILITY INDEX

The “affordability index” measures the relationship between median household incomes and median property values. The higher the ratio, the less “affordable” an average home becomes to the median household. Ratios decline as household incomes increase (assuming constant values), or increase as values accelerate at rates faster than incomes. Murray City shows an index reading of 4.27, fairly close to the county-wide figure of 4.23. South Salt Lake reflects an abnormally high number due to very low average household incomes, while cities such as West Valley, Kearns, and West Jordan show ratios below that of Murray. On a regional level, Salt Lake County is still considered more affordable by this measure than other major cities, including Portland, Denver, Las Vegas, Phoenix, and others.

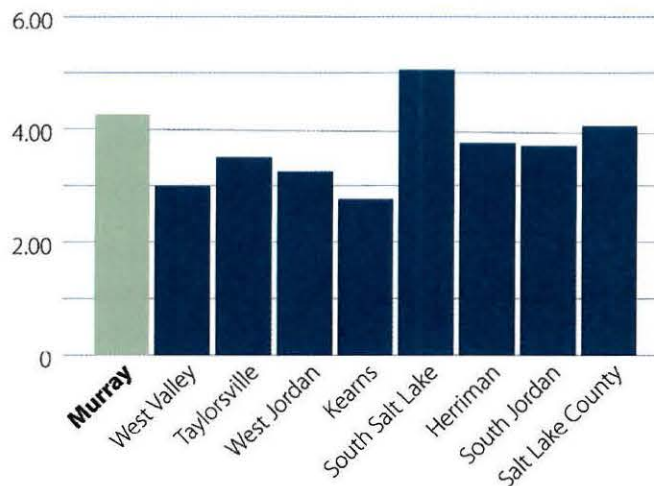


Figure 2.13 Affordability Index comparison between Murray and nearby cities (Esri Forecast, 2019).

2.8 RETAIL TRENDS

2.8.1 TAXABLE SALES PER CAPITA

Taxable sales per capita reflects an important statistic regarding the health of the local retail economy. For Murray City, total taxable sales in 2018 equaled roughly \$2.28 Billion, or approximately \$46,508 per resident. This is notably high in comparison to nearby cities such as South Jordan (\$21,907 per resident), West Valley (\$19,880 per resident), and West Jordan (\$15,990 per resident). Additionally, per capita statistics for Salt Lake County are shown at taxable retail sales of \$25,092. The data points show that Murray is not burdened with sales leakage of any kind, largely due to the success of the Fashion Place Mall and surrounding retail.

2.8.2 CURRENT RETAIL CONDITIONS

While consumer retail is an ever-changing industry, certain sectors are performing well, while others are not. High performing sectors include

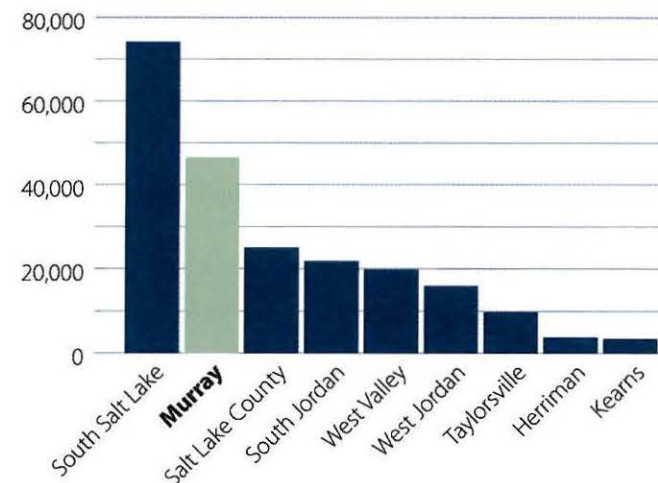


Figure 2.14 Taxable Retail Sales Per Capita comparison between Murray and nearby cities.



2 EXISTING CONDITIONS

grocery stores, automotive services, restaurants, experiential retail, and retail distribution. These sectors have remained relevant by adapting their business models. Changes such as shrinking physical space, expanding distribution, increasing convenience with pick-up or delivery service, as well as decreasing table space, are all tools retail outlets are using to succeed in Utah.

Poorly performing retail outlets include clothing stores, toy stores, jewelry stores, and department stores. Stores without an online shopping presence are also struggling.

In Utah, potential new retailers use various metrics when choosing a site to locate a business. These metrics include; strong traffic counts, multiple points of access, growing nearby populations, strong daytime populations, and destination locations. Retailers are also increasingly using more detailed demographic data that identifies zip codes with more of their target customers.

Consumers and cities increasingly want retail and services within walking distance of residential areas. This trend means that the Fashion Place Mall and the areas surrounding it may transition to meet this need. A wider variety of uses including housing and office are needed in the immediate proximity to encourage a more walkable district.

In the case of Fashion Place Mall, the parcels that surround the mall and face State Street are also occupied by surface parking. Increasing parking densities on site with structured parking make these locations ideal for the construction of liner buildings. To meet the needs and desires of residents, consumers, and developers, these liner buildings could house a number of uses including office, residential, and restaurants. These uses would complement each other, creating a stronger daytime and nighttime population, better supporting existing retail.

2.9 OFFICE TRENDS

2018 was a record setting year from the office sector in Utah, with nearly \$630 Million in permitted construction value. 2019 proved to be another strong year, as the office sector permitted nearly \$503 million, making it the third highest year on record (inflation adjusted).

2.9.1 REGIONAL OFFICE MARKET TRENDS

Salt Lake County led the state in office construction with nearly 70 percent of Utah's total permitted construction projects.

The growth that the State and Salt Lake County are seeing in the office sector can be attributed to Utah's expanding employment, especially in the tech, professional and business services sectors.

According to a 2019 midyear Utah Market Report compiled by Newmark Grubb Acres, a full-service commercial real estate and market research firm based in Salt Lake City, demand for Salt Lake County office space has remained very strong over the past 12 months, as available supply is at the lowest level the market has seen in several decades. High demand is clearly evidenced by 1.1 million square feet of positive net absorption over the past 12 months, compared with 956,207 square feet year over year—a healthy 12.3 percent increase. On the supply side, direct vacancy stands at 7.13 percent at midyear 2019, down from 7.76 percent at year-end 2018 and 8.04 percent 12 months ago.

The construction of office buildings is flourishing in Salt Lake County, with 2.8 million square feet currently underway. By comparison, 1.4 million square feet was under construction at midyear 2018, but at 103.3 percent, the year-over-year increase in office space under construction is also a clear indicator of demand. It is worth noting that much of the 2.8 million square feet currently under construction has been pre-leased.



Statistics indicate the positive metrics of the office market include the total square feet leased and the achieved average lease rates. Over the past four quarters, 4.1 million square feet were leased throughout the county, a 40.1 percent increase over the previous four quarters. Additionally, lease rates increased 3.6 percent to \$25.01 per square foot across all building classes over the same time frame. Interestingly, the number of overall lease transactions declined 6.9 percent compared with midyear 2018. However, transactions also grew larger in size year-over-year; the average transaction was 12,013 square feet at midyear 2019, compared with 8,011 square feet at midyear 2018. This data suggests that, the lease terms and overall lease rates have increased, further ensuring a stable market.

2.9.2 CENTRAL VALLEY OFFICE MARKET TRENDS

The Central Valley office market includes the western portion of Murray, including the Fashion Place West study area, the northern portion of Midvale, the eastern portion of Taylorsville, as well as a western segment of Millcreek.

At midyear 2019, the Central Valley market had a direct vacancy rate of 6.81 percent compared to 7.13 percent in Salt Lake County. The Central Valley market's vacancy rate was also lower than that of the Millcreek/Holladay market, with a direct vacancy rate of 9.68 percent.

The Central Valley office market's average lease rate by midyear 2019 was \$19.53 per square foot, which is similar to that of the Millcreek/Holladay market, which achieved lease rate of \$19.45 per square foot. However, the County's average lease rate was substantially higher at \$25.00 per square foot.

Net absorption is the difference between the commercial space vacated in a certain time period and the spaces leased by commercial entities in the same area. Positive net absorption means more space was leased

than was vacated in the market. Negative net absorption means that more commercial space was vacated in a particular market than was leased (or absorbed) by commercial tenants.

The Central Valley market has a positive net absorption of 27,655 square feet, which is much better than surrounding markets such as Millcreek/Holladay that has had a negative net absorption of 39,699 square feet over the same time period.

2.9.3 CURRENT ECONOMIC DEVELOPMENT TOOLS

Murray's Community and Economic Development offices facilitate redevelopment efforts and assist in economic development, community development and renewing urban areas. The Redevelopment Agency (RDA) of Murray City assists in redevelopment efforts by encouraging private and public investment in previously developed areas that are underutilized or blighted. Housing development is also a priority and the RDA works to increase the amount and variety of affordable housing within the community.

The City currently has six active project areas predominately throughout the west side of the City with varying expiration years and sizes. The Fashion Place West is not within a project area, but could be a prime candidate for future consideration due to its land values and proximity to the TRAX station and other important assets.



2 EXISTING CONDITIONS

2.10 TRANSPORTATION ANALYSIS

2.10.1 MAJOR STREETS

The study area is defined by one major arterial road, State Street, and three major collector streets, Winchester Street/6400 South, Cottonwood Street, Fashion Boulevard, and Interstate 15 and 215.

According to the current Murray General Plan, there is concern about traffic on neighborhood roadways originating from heavily congested major streets.

According to the UDOT Numetric collision database there were 493 recorded collisions in the study area from 2017–2019, with 34 of those resulting in injuries and none with fatalities. Of those collisions, 242 were considered intersection related. The largest clustering of collisions occurred at the intersection of Winchester Street and State Street. This intersection also saw the most injury crashes (7), bicycle crashes (3), and pedestrian crashes (2).

The intersection of State Street and Creek Drive had 14 collisions, 11 of which were turning left. Most of these collisions occurred during daylight hours in dry weather conditions.

2.10.1.1 WINCHESTER STREET/ 6400 SOUTH

Winchester Street/6400 South is a three-lane cross section arterial between the western edge of the study area and State Street—making it the main east-west corridor through the study area. Left-turn lanes are present at the intersections of Cottonwood Street, Fashion Place TRAX station, Travis James Lane, Jefferson Street, and Blaine Drive.

Besides these left-turn lanes, a central two-way left-turn lane services individual driveways along Winchester Street between Travis James Lane and 150 feet east of Clay Park Drive. The roadway widens to a four-lane

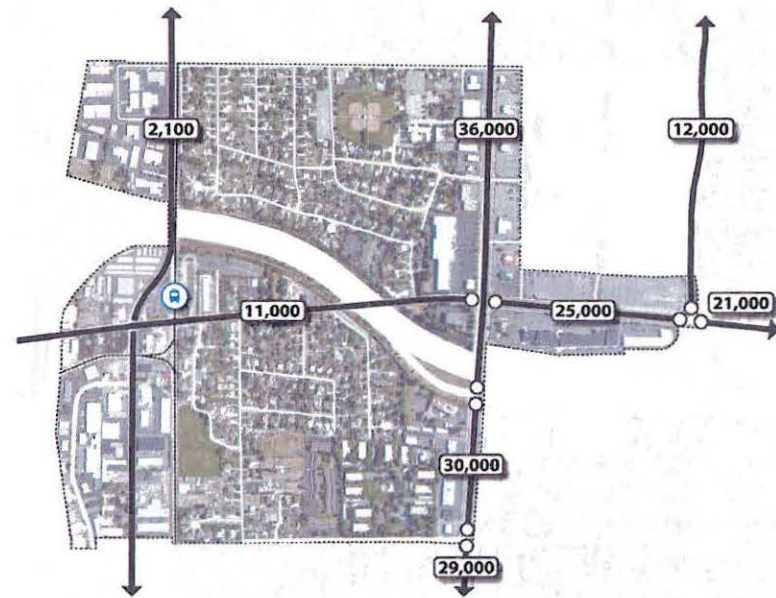


Figure 2.15 Map showing existing traffic counts on major roads in the study area.

cross section east of State Street up to the eastern edge of the study area.

There is on-street parking along Winchester Street from the western edge of the study area to 100 feet west of Cottonwood Street.

According to UDOT's 2016 statewide estimates, Winchester Street experiences an average annual daily traffic (AADT) volume of 11,000 vehicles per day between the western edge of the study area and State Street, and it experiences an AADT volume of 25,000 vehicles per day between State Street and the eastern edge of the study area.

There are bicycle sharrows on both sides of Winchester Street from the western edge of the study area to the intersection with Cottonwood Street. From Cottonwood Street east to Jefferson Street, five-foot bike lanes run adjacent to the curb on both sides of Winchester Street. From



100 feet east of Jefferson Street to Malstrom Lane, bicycle sharrows again appear in place of bike lanes. The five-foot bike lanes resume along Winchester Street from Malstrom Lane to 100 feet east of Clay Park Drive.

Sidewalks exist on both sides of Winchester Street throughout the study area. All sidewalks are four feet wide, except for a seven foot wide portion between State Street and South Fashion Boulevard.

2.10.1.2 COTTONWOOD STREET

Cottonwood Street is configured as a two-lane cross section throughout the study area, with additional right and left-turn bays present at the northbound and southbound approaches to Winchester Street. A left-turn lane also exists for the northbound approach to 6100 South. A sidewalk narrower than six feet spans the western edge of the roadway from the northern edge of the study area to the southern end of the I-215 overpass bridge. From this southern edge of the bridge, the sidewalk widens to ten feet wide until the intersection with Winchester Street, where the sidewalk narrows to eight feet wide until the intersection with 6500 South. From 6500 South to the southern edge of the study area, the sidewalk further narrows to seven feet wide.

According to UDOT's 2016 statewide estimates, Cottonwood Street experiences an AADT volume of 2,100 vehicles per day across the study area. Additionally, a signalized train crossing exists 230 feet south of the intersection with Winchester for the TRAX Red Line train.

2.10.1.3 STATE STREET

State Street (US-89) is a six-lane, 90-foot wide major north-south arterial across the study area that widens with turn bays at major intersections. There are left-turn bays on the northbound and southbound approaches at the intersections of 6100 South, Creek Drive,

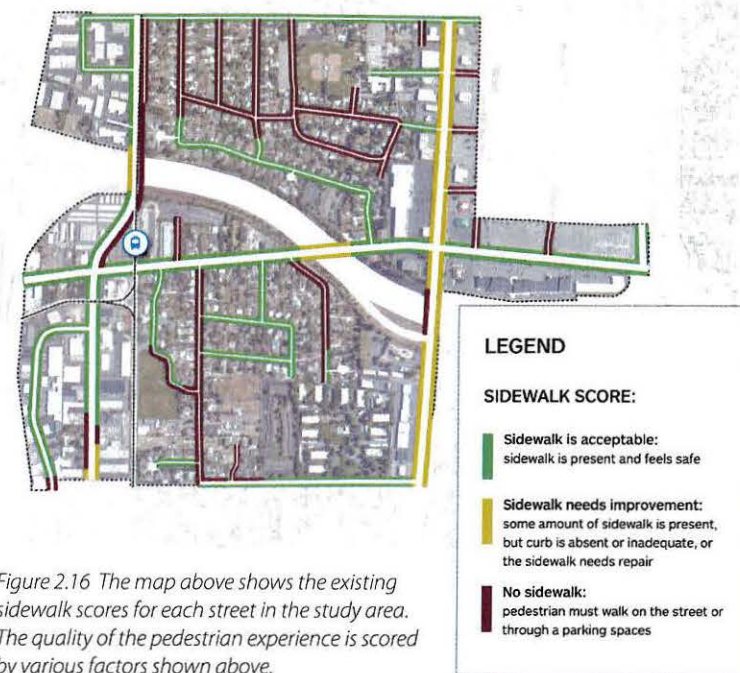


Figure 2.16 The map above shows the existing sidewalk scores for each street in the study area. The quality of the pedestrian experience is scored by various factors shown above.

and 6790 South. Two southbound left-turn lanes and one northbound left-turn lane exist at the intersection with 6400 South, along with a channelized right-turn lane on the southern approach. Two southbound left-turn lanes also exist at the intersection with the I-215 eastbound ramps, along with a right-turn lane on the southern leg. South of the I-215 ramp, a left-turn lane serves the Supersonic Express Car Wash on the western side of State Street. The intersection with the Sam's Club driveway has two left-turn lanes on the northern approach of State Street, as well as one left-turn lane on the southern approach.

Sidewalks exist on both sides of State Street throughout the study area. Along the roadway between 6100 South and 6400 South, the sidewalks are seven feet wide on the eastern side of State Street and five feet wide on the western side. Between the intersection with 6400 South, the sidewalks on the eastern side of State Street remain at seven feet wide



2 EXISTING CONDITIONS

while the sidewalks on the western side widen to eight feet wide. Across the bridge over I-215, the sidewalks on both sides of State Street narrow to five feet in width. From the southern edge of the I-215 bridge to 6790 South, the sidewalks widen to six feet wide on both sides of State Street.

Route 201, one of UTA's most utilized bus routes, runs along State Street across the study area with stops at the intersections with 6100 South, Creek Drive, the Sam's Club driveway, and 6790 South. UTA's future Bus Rapid Transit (BRT) will also run along State Street across the study area.

According to UDOT's 2016 statewide estimates, State Street experiences an AADT volume of 36,000 vehicles per day between the northern edge of the study area and Winchester Street, and it experiences an average annual daily traffic volume of 30,000 vehicles per day between Winchester Street and the southern edge of the study area.

2.10.1.4 SOUTH FASHION BOULEVARD

South Fashion Boulevard has a 60-foot five-lane cross section (two through lanes and one center left-turn lane) through the study area. At the intersection with Winchester Street, additional left-turn and right-turn lanes are also present for the southbound approach. Sidewalks exist on both sides of South Fashion Boulevard with widths of four to six feet. However, most of these sidewalks have little or no buffer zone or park strip between the pedestrian zone and adjacent travel lanes.

According to UDOT's 2016 statewide estimates, South Fashion Boulevard experiences an AADT traffic volume of 12,000 vehicles per day across the study area.

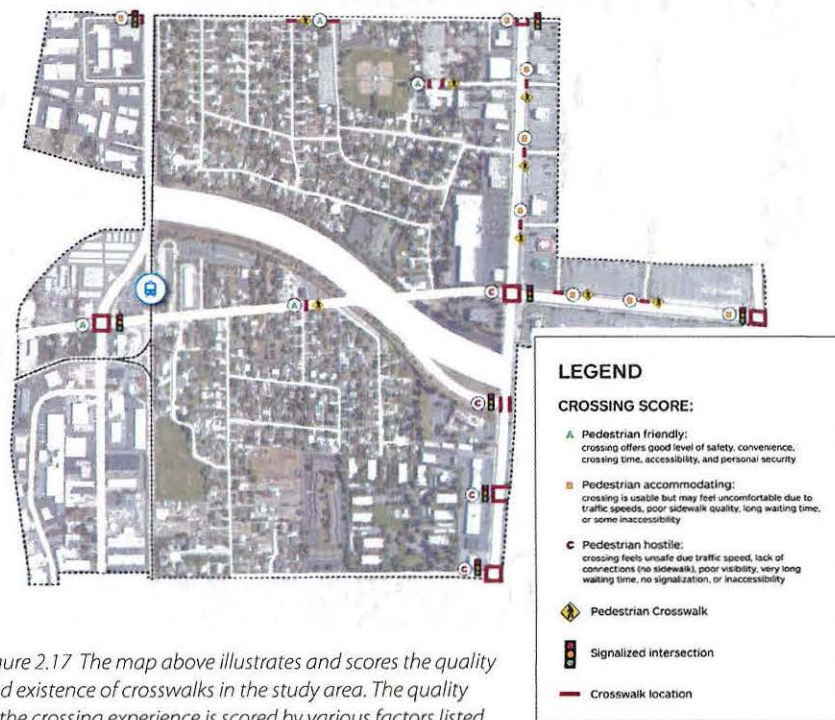


Figure 2.17 The map above illustrates and scores the quality and existence of crosswalks in the study area. The quality of the crossing experience is scored by various factors listed above.

2.10.2 MINOR STREETS

2.10.2.1 JEFFERSON STREET

Jefferson Street is a north-south neighborhood roadway extending from Winchester Street on the north to the southern edge of the study area. It is an unmarked 30-foot roadway with no sidewalks.

2.10.2.2 CREEK DRIVE

Creek Drive connects the northern neighborhood in the study area to State Street. It is an unmarked 40-foot roadway with no sidewalks.



2.10.2.3 6100 SOUTH

6100 South is a 30-foot wide two-lane collector road extending from its westernmost origin with 350 West to the eastern edge of the study area. The roadway extends to 50 feet wide at the signalized intersection with State Street to accommodate a left-turn bay and a right-turn bay. Five-foot sidewalks exist on both sides of 6100 South for the entirety of the study area. High-visibility crosswalks provide school crossings on the western and southern legs of the intersection with Cedar Street.

2.10.2.4 SOUTH MALSTROM LANE

South Malstrom Lane is a 25-foot wide unmarked neighborhood roadway with its northernmost point at Winchester Street that narrows to 15 feet wide at the intersection with Caleb Place. The only sidewalk is on the eastern side of the segment from the southern edge of the roadway to 380 feet south of Caleb Place.

2.10.2.5 400 WEST

400 West turns off 6500 South and extends to the southern edge of the study area. It is a 30-foot wide unmarked roadway that traverses an industrial zone. Sidewalks exist on both sides of 400 West throughout the study area.

2.10.2.6 790 SOUTH

6790 South is a 30-foot wide neighborhood collector roadway with four-foot sidewalks on both sides. 6790 South connects neighborhood access roads as far west as Jefferson Street to the State Street arterial. Sidewalks extend from 70 West to State Street on both sides of the roadway. There are no sidewalks along 6790 South from 70 West to the western edge of the study area.

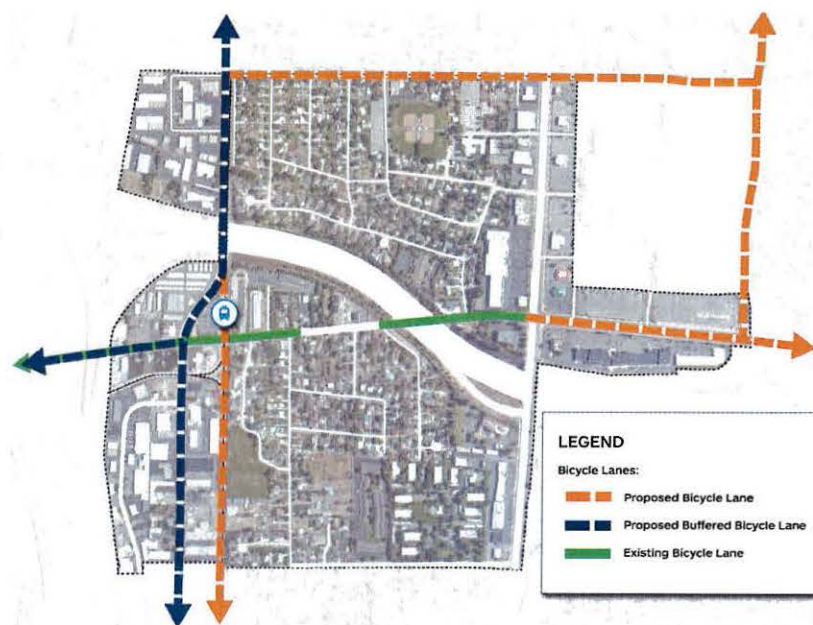


Figure 2.18 Map of existing and proposed bicycle connectivity in the study area.

2.10.4 BICYCLE CONNECTIVITY

The only bicycle infrastructure in the study area is on Winchester Street. From the western edge of the study area to Cottonwood Street, bicycle sharrows exist on both sides of the roadways, giving way to dedicated bike lanes up until 100 feet east of Jefferson Street. From here, a parking lane runs along the curb in place of the bike lane. There are no signs or markings indicating this curbside transition between bike lane and parking lane. Sharrows resume along Winchester Street until bike lanes resume at Malstrom Lane. These dedicated bike lanes continue from Malstrom Lane to 100 feet east of Clay Park Drive, where sharrows resume to the intersection with State Street.

State Street has no bicycle infrastructure despite it being an important vehicular connection for the area. It is possible that cyclists do not feel safe to travel on State Street due to high vehicular traffic counts.



2 EXISTING CONDITIONS

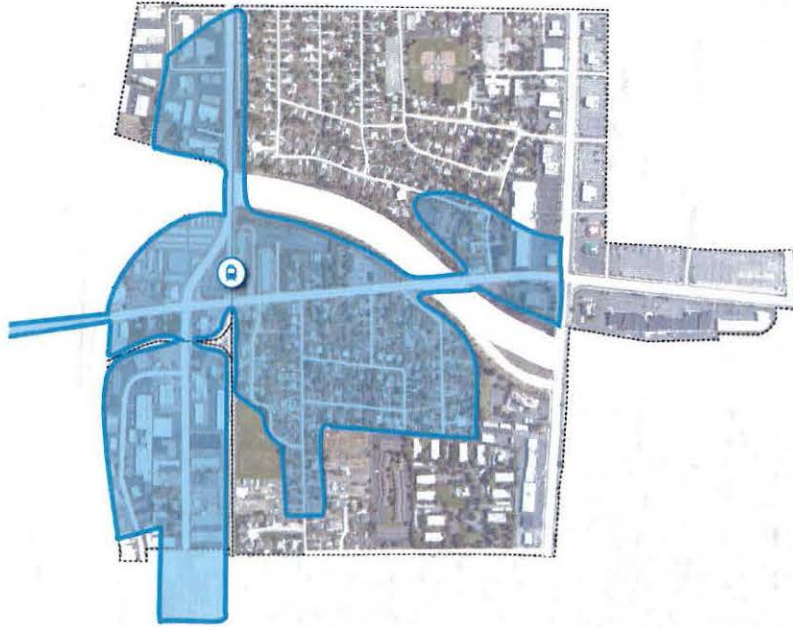


Figure 2.19 This 10 minute “walkshed” map illustrates the average distance covered by walking for 10 minutes from the center of the study area.

According to the UDOT numeric collision database there were 10 recorded bicycle-related collisions within the study area from 2017-2019. Seven of these collisions resulted in injuries, and three of these collisions occurred at the intersection of Winchester and State Street with drivers often noting that they were unaware of the presence of bicycles. The intersection of Winchester Street and State Street sees the most bicycle-related crashes of any intersection in the study area. The bicycle infrastructure from Winchester Street is not carried into the intersection with State Street.

According to Strava bicycle data, Winchester Street and Cottonwood Street see the most bicycle activity of the study area, as shown in the corresponding map.

The Wasatch Front Regional Council's (WFRC) Regional Transportation Plan and the Murray City General Plan outline several bicycle infrastructure improvements for the study area. The Murray General Plan also details current, future, and desired bicycle infrastructure. According to the Murray General Plan, citizens would like to bike more but do not feel safe to do so.

2.10.5 WALKABILITY

Sidewalks are present throughout much of the study area, all at least four feet wide. State Street, Winchester Street, 6100 South, and 400 West all have sidewalks on both sides of the roadway. A sidewalk exists only on the west side of Cottonwood Street between the northern edge of the study area to Winchester Street, then expands to both sides south of Winchester Street to the southern edge of the study area.

Roughly half of the neighborhood roadways in the study area have sidewalks on both sides of the street, the other half of the roadways have no sidewalks at all. There are few sidewalks throughout most of the Western Park neighborhood, just north of I-215 between Cottonwood Street and State Street. The Atwood neighborhood on the south side of I-215 has more sidewalks than Western Park, but some streets such as Jefferson Street and Malstrom Lane have only portions of or no sidewalk at all. A narrow sidewalk on Cottonwood Street across the busy interstate leaves pedestrians feeling unsafe as they travel between the TRAX station and final destination. Jefferson Street and 6790 South are important streets for the Atwood neighborhood, yet sidewalks are incomplete on both sides of the roadways.

According to the Murray General Plan, people would like to walk more but do not feel safe to do so or feel that desired destinations are not walk-friendly (i.e., large parking lots in front of a store entrance, limited park strip and street trees on sidewalks).



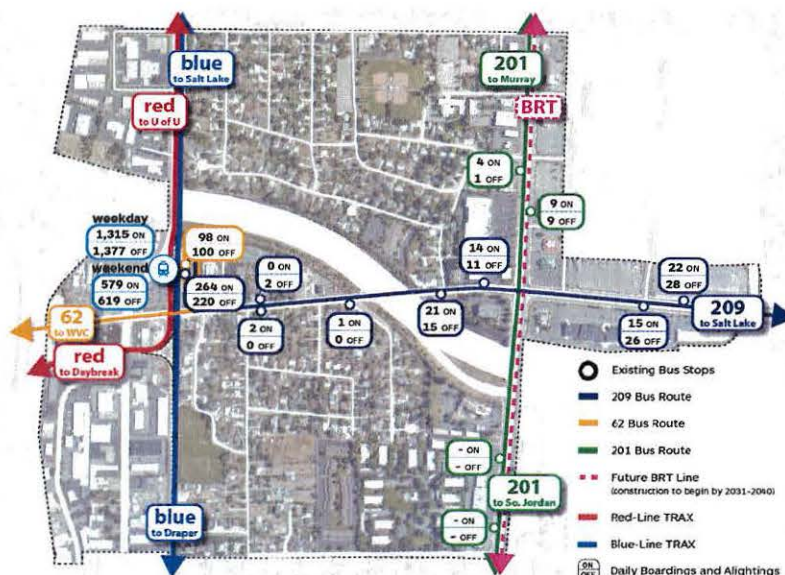


Figure 2.20 Map showing existing TRAX and bus service routes in the study area.

2.10.6 TRANSIT SERVICE

2.10.6.1 TRAX STATION CONNECTIVITY

The Fashion Place West TRAX station is the southernmost TRAX station where the Blue line and the Red line run concurrently. The Red Line connects to Daybreak Parkway in South Jordan and operates every 15 minutes from 5:15am-11:45pm on weekdays and every 20 minutes from 6:00am-11:30pm on weekends. The Blue Line connects to Draper Town Center and operates every 15 minutes from 5:00am-12:00am on weekdays and every 20 minutes from 5:45am-11:30pm on weekends.

The Fashion Place TRAX station carries thousands of passengers into and out of the study area every week, yet Fashion Place Mall is not easily accessible from this station if traveling by a means other than personal vehicle.

2.10.6.2 PUBLIC TRANSIT

UTA Route 201 connects Murray Central Station to the South Jordan station, operating north-south on State Street in the study area and stopping adjacent to 6100 South, Creek Drive, Sam's Club driveway (southbound only), and 6790 South. The 201 bus runs on half-hour headways from 6am-8pm on weekdays and from 7am-8pm with hour headways on Saturdays. The 201 bus does not operate on Sundays.

The UTA Route 62 bus connects the Oquirrh Shadows stop in South Jordan to the Fashion Place West TRAX station. Within the study area, the 62 bus runs east-west along Winchester Street from the western edge of the study area to the Fashion Place TRAX station, stopping only at the Fashion Place TRAX station within the study area. The route with hour headways from 6:30am-6:30pm on weekdays and 90-minute headways from 6:30am-9:30pm on Saturdays. The 62 bus does not operate on Sundays.

The UTA 209 bus connects the Fashion Place West TRAX station to the North Temple TRAX station in downtown Salt Lake City. Within the study area, the 209 bus runs east-west along Winchester Street from the Fashion Place West TRAX station to the eastern edge of the study area, stopping adjacent to Jefferson Street, Malstrom Lane, Clay Park Drive, and Fashion Place Mall. The 209 bus operates on 15-minute headways from 6:00am-10:30pm on weekdays, half-hour headways from 7:00am-9:30pm on Saturdays, and on hour headways from 7:30pm-8:00pm on Sundays.

Most bus stops within the study area consist of signage only, with few shelters, benches, waste receptacles, or other improvements present.

2 EXISTING CONDITIONS

Route	Stop Name	Total Ons	Total Offs	Average Ons	Average Offs
62	Fashion Place West	2,160	-	98	-
62	Fashion Place West	-	2,208	-	100
201	State Street 5590 South	27	224	1	10
201	State Street 6300 South	192	200	9	9
201	State Street 6200 South	86	21	4	1
209	Winchester Street 219 East	491	613	22	28
209	Winchester Street 50 East	298	244	14	11
209	Winchester Street 170 West	5	52	0	2
209	Fashion Place West	-	4,845	-	220
209	Fashion Place West	5,800	-	264	-
209	Winchester Drive 171 West	48	5	2	0
209	Winchester Drive 97 West	12	3	1	0
209	Winchester Street 31 East	471	337	21	15
209	Winchester Street 194 East	333	576	15	26
TRAX	Weekday	28,925	30,288	1,315	1,377
TRAX	Saturday	2,896	3,036	724	759
TRAX	Sunday	2,171	2,397	434	479

Figure 2.21 The table above outlines the public transit boardings and alightings (exiting the bus) for all the stops and stations in the study area during May, 2019.

The Fashion Place West TRAX station is the most utilized station in the study area with over 1,300 average weekday boardings. Similarly, the bus routes in the study area experience their highest utilizations at the TRAX station connection. Route 62 has about 100 average daily boardings, and Route 209 experiences over 200 daily boardings at the Fashion Place West TRAX station connection.

2.10.7 LOCAL AND REGIONAL PLANNING CONTEXT

The Murray City General Plan emphasizes the City's desire to improve accessibility by walking, biking, and public transit in the corridor between I-15 and State Street to provide adequate infrastructure for existing and planned commercial development. The General Plan recommends the following improvements to mobility and circulation in the study area:

- Construction on Cottonwood Street to relieve north-south congestion on State Street and 700 West (Murray Boulevard). This project is in progress. The reconstruction of the bridge over I-215 will include sidewalks and bike lanes.
- Encourage employers to offer incentives and alternatives to relieve peak period vehicular congestion.
- Adopt a complete streets policy applicable to new and reconstructed roadways where feasible.
- Identify transit use impediments and prioritize solutions.
- Develop and implement an Active Transportation Plan.
- Implement a dedicated funding source for the improvement of pedestrian and bicycle facilities.
- Implement traffic calming measures on roadways where traffic operates beyond the target speed.

2.10.8 WASATCH FRONT REGIONAL COUNCIL REGIONAL TRANSPORTATION PLAN

The WFRC Regional Transportation Plan (RTP) outlines several roadway infrastructure improvements, summarized in map showing future projects:

- State Street is planned for future operational road improvements.
- Winchester Street will be widened from two travel lanes (68-foot right-of-way) to four travel lanes (86-foot right-of-way).
- A new Bus Rapid Transit (BRT) line will operate along State Street throughout the study area, along with the existing Route 201.
- A priority buffered bike lane is planned for Cottonwood Street between the northern edge of the study area and Winchester Street, as shown in the Active Transportation Implementation Plan map.
- A shared-use trail is planned to run along the TRAX Blue line from Winchester Street to the southern edge of the study area.



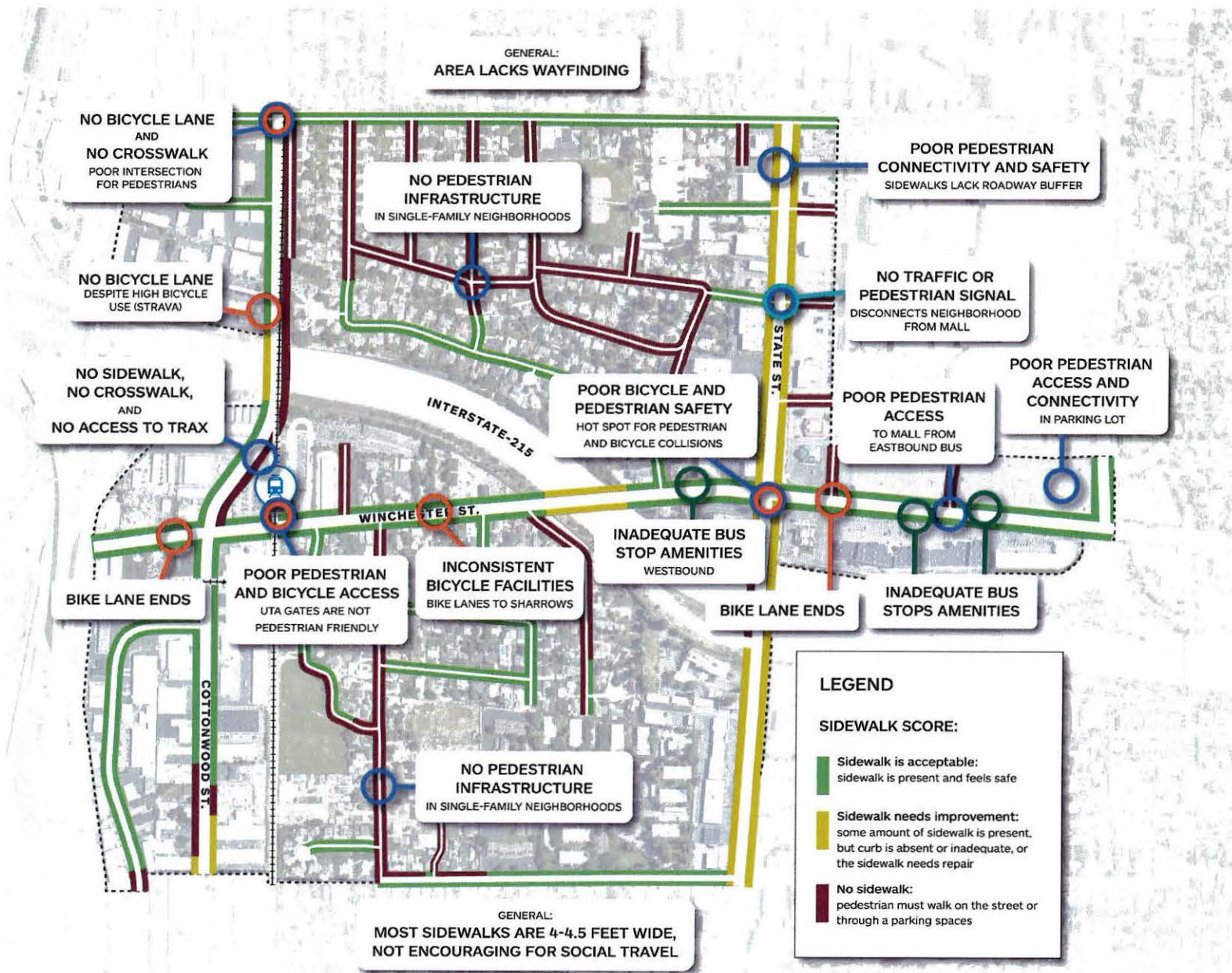


Figure 2.22 Walkability infrastructure scoring for the study area.



3 HOUSING



3.1 INTRODUCTION

Housing investment is a vital component to continued growth and vitality for any community. In recent years, interest in more urban and concentrated housing options have grown across the country, including Murray and the Salt Lake metro area. This interest is driven largely by a demand for housing options that fit changes in demographics, lifestyle, resource use, and budgets.

In order to promote growth and sustained development energy in the Fashion Place West neighborhood, focusing on more diversity of housing options is essential. Because of its location in the valley and proximity to transit, the neighborhood will soon face similar development pressures that are being experienced by other parts of Murray and other cities throughout the Wasatch Front. The small area



Figure 3.1 Housing choices near transit service and other transportation networks are a vital part of expanding economic development in the City and providing affordable household options.



- **15 MINUTES TO FASHION PLACE MALL**
- **15 MINUTES TO LIBERTY ELEMENTARY SCHOOL**



- **12 MINUTES TO SANDY**
- **23 MINUTES TO DOWNTOWN SALT LAKE CITY**
- **24 MINUTES TO DAYBREAK**
- **70 MINUTES TO PROVO**
- **90 MINUTES TO OGDEN**



- **40 MINUTES TO PARK CITY**
- **10 MINUTES TO BIG COTTONWOOD CANYON**
- **23 MINUTES TO SLC AIRPORT**

Figure 3.0 Ease of access to transportation networks and jobs centers from this area make it a prime location for expanding housing choices.

planning process is a proactive way for the City to define the way in which the study area expects to plan for future growth.

More housing brings more people to the neighborhood for more hours of the day than retail or office uses. This change and growth will support the nearby TRAX station by increasing the density around it, and with that, increase ridership, as well as support a greater variety of businesses, services, and other uses in the Fashion Place West area.



3 HOUSING

3.2 HOUSING DEMAND

3.2.1 POPULATION GROWTH

Over the last few years Utah housing inventory has not kept up with the rate of population growth both in single and multi-family dwellings. Overall (for sale and rental) vacancy rates in Salt Lake County are the lowest they have been in over a decade, at approximately 5.5 percent. Rental unit vacancy rates are a bit lower at 4.6 percent. Even though Utah has previously led the nation in homebuilding, constructing homes and apartments at a rate of nearly three times the national average, the state still faces a housing shortage. This lack of supply has led to increasing home prices and rental rates. The Salt Lake Chamber polled their members regarding their thoughts on affordable housing in the region, and almost 95 percent of survey respondents agreed that affordable housing is a major problem for Utah's continued economic growth.

With Utah's population expected to double by 2065, the demand for affordable housing will only increase. In order to accommodate the



Figure 3.2 Strong population growth in Salt Lake County will ensure increased demand for housing in more walkable and bikeable neighborhoods near employment centers.

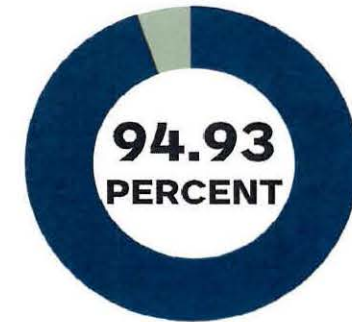
housing needs of both current and future residents, tools must be implemented that increase inventory, diversify options, and expand affordability. Planning for continued population growth is a primary challenge that the region faces in the short and long-term.

3.2.2 AFFORDABILITY

3.2.2.1 MODERATE INCOME HOUSING

Utah State Code Section 10-9a-403 states that each municipality is required to include a plan for moderate-income housing as part of their General Plan. This plan must facilitate a reasonable opportunity for individuals of moderate-income levels the option to live in the City. Moderate-income housing is defined by the Department of Housing and Urban Development (HUD) as, "housing occupied or reserved for occupancy by households with a gross household income equal to or less than 80 percent of the median gross income for households of the same size in the county in which the City is located."

This section uses the Salt Lake County Area Median Income (AMI) and average household size to determine moderate income thresholds for Murray City. This data will help the City and more specifically, the Fashion Place West study area to determine housing needs, and thus encourage and incentivize developers to build housing of different types and for differing income levels.



of survey respondents agree that affordable housing is a major problem for Utah's continued economic growth
(Source: The Salt Lake Chamber)

Figure 3.3 Survey responses regarding housing affordability.



3.2.2.2 COST-BURDENED HOUSEHOLDS

A household spending 30 percent or more of its gross income on total housing expenses—rent or mortgage, basic utilities, and property taxes—is considered cost burdened. A household spending 50 percent or more of its gross income on housing is considered to be severely cost burdened.

In the state of Utah, one in three households (~66,000) face a housing cost burden demanding at least 30 percent of monthly income, and one in eight households (~125,000) face a severe cost burden. In Salt Lake County, 24 percent of low income households (30-50 percent of AML), and 75 percent of extremely low income households (less than

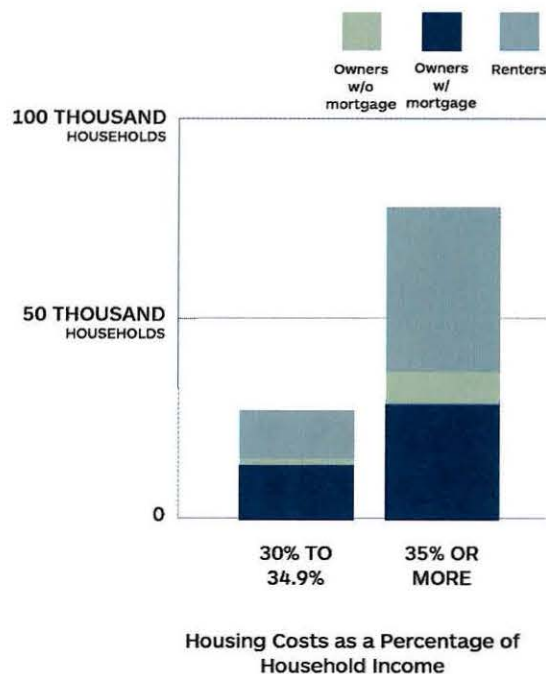


Figure 3.4 Renters in Salt Lake County make up the majority of cost-burdened households.

30 percent AML), are severely cost burdened. These households are far more susceptible to changes in the economy or personal emergencies, either of which could result in dire financial consequences or even homelessness.

Providing support for the cost burdened households in Murray is needed to reduce the number of short-term residents and create more stable neighborhoods.

3.2.2.3 NEW MARKET-RATE HOUSING

There is a common misconception around the construction of new market-rate housing in lower income areas and how this development affects housing costs of more affordable housing options nearby. Market-rate housing is defined as any type of residential dwelling—whether the unit is to be owner or renter occupied—that is available at the prevailing market value for the area, and similar to comparable real estate transactions. Contrary to common concerns, new market-rate construction slows local rent increases rather than initiate or accelerate them. A recent study* performed by the Upjohn Institute shows that new market-rate buildings have the capacity to decrease nearby rents by 5-7 percent relative to locations slightly farther away or developed later, and can also increase in-migration from low income areas. The

*Asquith, Brian J., Evan Mast, and Davin Reed. 2019. "Supply Shock Versus Demand Shock: The Local Effects of New Housing in Low-Income Areas." Upjohn Institute Working Paper 19-316. Kalamazoo, MI: W.E. Upjohn Institute for Employment Research.



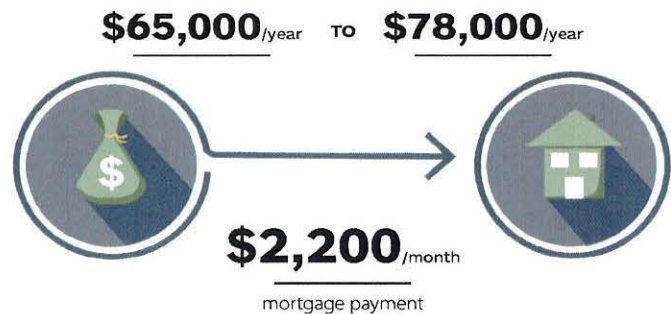
New construction lowers nearby rents by
5-7%

Figure 3.5 Market-rate housing can increase supply and help keep rental prices in check.*



3 HOUSING

INCOME NEEDED TO BUY A \$400,000 HOME IN SALT LAKE COUNTY 2020



Source: UtahRealEstate.com | Ridge Home Loans

Figure 3.6 Graphic showing necessary household income to purchase a home in Salt Lake County.

study also shows that new construction decreases the average income of people moving to the area by approximately 2 percent, as well as the number of people moving to the area who are from very low income neighborhoods by almost 3 percent. This is due to the fact that new buildings reduce costs in lower segments of the housing market.

Another misconception about the construction of new market-rate housing in a lower income neighborhood is that this development contributes to or initiates gentrification. The Upjohn Institute study found that new construction actually tends to occur after a neighborhood has already begun to change, or gentrify. The end result is the eventual accommodation of pre-existing demand, diverting high-income households from nearby units and reducing rents, instead of signaling that a neighborhood is now desirable.

Murray City should adopt strategies that encourage housing



Figure 3.7 With the projected increase in population over the next 20 years, market-rate and more income-dependent housing options will be important to maintaining affordability.

development. Regulatory restrictions on housing development can lead to higher rents, and faster home price growth. This leads to fewer people moving into economically successful areas. Strategies that promote residential construction foster more economically integrated neighborhoods, which also promotes economic mobility and housing options for low income residents. Market-rate housing construction not only improves regional affordability, but also neighborhood affordability.

3.2.2.4 ENERGY PRICES

In a world of higher energy costs, it will be essential to consider the combined costs of housing, transportation, and utilities—to ensure that families have adequate residual incomes to afford other necessities. This in turn suggests the importance of policies and practices that help to reduce these combined costs, for example, by ensuring the availability of affordable homes near public transit and job and retail centers—so



that families have options to reduce car usage. Such options may include walking, biking, public transit use, or shorter and fewer car trips.

3.3 HOUSING SUPPLY

3.3.1 LIFE CYCLE HOUSING

Murray City and the Fashion Place West neighborhood should be a place where residents can live in the City and in their neighborhood through any stage of life. The General Plan discusses life cycle housing throughout the document, with the goal to encourage diverse housing types that respond to housing needs, allowing individuals to stay in their communities as their housing needs evolve.

Life cycle housing involves reintroducing the model of providing a mix of housing types in a neighborhood. Typical suburban development tends to segregate people based on their income. By addressing all stages of life, ranging from young couples, the fixed-income student, to the aging grandparent, a wide variety of individuals and families live in proximity to each other, creating a more dynamic social environment,

Young couples may have children and become young families.

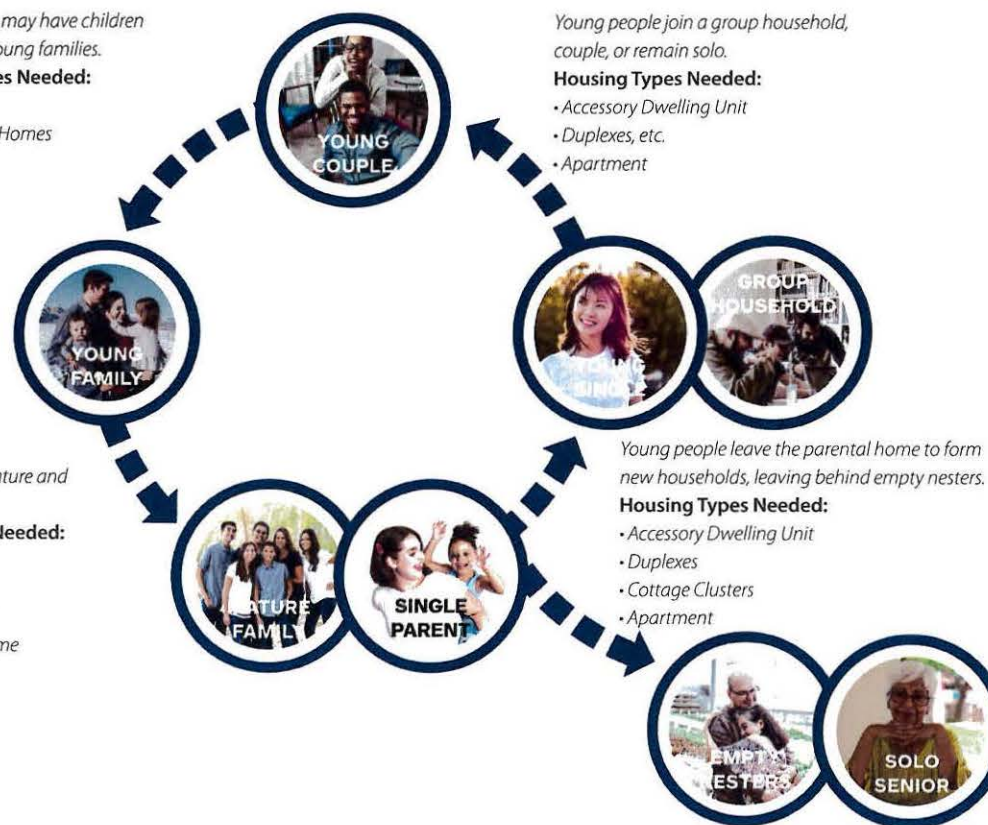
Housing Types Needed:

- Townhomes
- Single Family Homes

Young people join a group household, couple, or remain solo.

Housing Types Needed:

- Accessory Dwelling Unit
- Duplexes, etc.
- Apartment



Young families mature and increase in size.

Housing Types Needed:

- Townhomes
- Duplexes
- Cottage Clusters
- Single Family Home

Young people leave the parental home to form new households, leaving behind empty nesters.

Housing Types Needed:

- Accessory Dwelling Unit
- Duplexes
- Cottage Clusters
- Apartment

Figure 3.8 Life cycle housing is a strategy to ensure that all households have access to housing choice in their neighborhood throughout their lifetime.

and more choices for any household. A neighborhood that has housing options for all of these groups is less dependent on any one particular demographic group, and will see more social stability as individual households are able to stay within established social networks, despite changes in household needs.

Life cycle housing is a housing strategy that the City should continue to support and identify how the housing stock in the Fashion Place West area can be diversified beyond its current housing stock.



3 HOUSING

3.3.2 HOUSING OPTIONS

Neighborhoods centered around public transit and transit-oriented development (TOD) are intended to provide a wider range of choices in transportation, retail, and housing. Housing for people of all income levels is especially appropriate in these types of neighborhoods. Housing choices in transit-oriented developments allow a greater number of people from a wider range of backgrounds and affordability levels to access jobs without driving. Additionally, residents of lower income levels are more often transit-dependent than residents within middle-income brackets. Expanding housing styles, types, and providing housing near frequent and effective transit increases quality of life and access to employment opportunities and services. Increasing housing choices and development will help meet the changing residential demand and build a larger residential economic base.

3.3.2.1 PHYSICAL HOUSING TYPES

In order to respond to Murray's changing demographics and the



Figure 3.9 Housing supply of all kinds at all price points is lacking throughout the region.

housing needs of its diverse community, it is critical to begin to look within the City for real and responsive change that will encourage the market to develop the housing and infrastructure needed to accommodate our growing community. This goal focuses on the need to increase the diversity of housing types and opportunities in the City by seeking policy reforms that can enhance the flexibility of the land use code and create an efficient and predictable development process for community growth. Strategic policy decisions that integrate the transportation system, development related infrastructure, financial institutions, and data, as well as innovative design and construction methods, can break down social and economic segregation, thus building a City for everyone.

While the Fashion Place West study area is predominately built-out, there is ample opportunity for redevelopment and infill development of existing parcels that complement current development patterns. Context sensitive development can ensure the character of neighborhoods is protected and enhanced by new development. While the type and location of housing is largely driven by the market, land use regulations and City policies can help guide the development. The Fashion Place West study area has the capacity for infill development of appropriate types and locations, and can benefit from partnerships with local housing developers who are already active in creating urban, mixed-use, multifamily projects. The City and development community can work together to address changes in housing preferences and needs, and provide more housing choices for buyers and renters at all price levels to meet housing objectives.

3.2.2.2 FOR RENT AND FOR SALE HOUSING

A healthy housing stock requires a diverse inventory of for-sale and for-rent products. These products can and should take many different forms. Units designed and constructed to be rented and owned can



include single-family homes, condominiums, townhomes, apartments, as well as accessory dwelling units (ADUs). Residents require different styles of housing at different points in their lives. Within the Fashion Place West study area, for-sale single-family homes dominate the landscape. As mentioned previously, the area does include both an apartment and condominium development but other housing types do not exist. Diversifying the nature of the for-sale and rental market in the study area will further contribute to creating an affordable neighborhood and City.

3.4 TRANSPORTATION AND HOUSING

3.4.1 AFFORDABILITY AND TRANSIT

Increased public transit options and proximity to housing and job centers can have a great impact on the increase of affordable housing options. The Metropolitan Planning Council and Center for Housing Policy performed a study in 2010 that identified public



4-24%

HIGHER

median sales price of neighborhoods near public transit

Figure 3.10 Public transit can greatly increase home values.



1 in 4 homes, within proximity to public transit, does not own a personal vehicle

Figure 3.11

transportation as a key variable to the availability of affordable housing. In order to make housing cheaper, public transportation needs to be more accessible and less expensive, and a municipality's definition of affordable housing should include transportation costs.

Affordable housing that is more compact and closer to transit lowers housing costs. When compact, residential development is located near public transit hubs or work centers, it can decrease transportation costs and cut down on travel time for working individuals. Local policy makers, as well as those at the regional and state levels have the responsibility to adopt or amend current regulations to encourage the development of housing near transit centers.

3.4.2 HOME VALUES AND TRANSIT

According to a study performed by the National Association of Realtors (NAR), housing next to public transportation increases home values. These neighborhoods have median



Figure 3.12 Connectivity for bicycles offers an affordable mode of transportation and recreation to an area.



\$2,500-\$4,400

in average yearly transportation savings of households living near public transit

Figure 3.13



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Figure 3.14 Walkable destinations are more attractive to visitors, businesses, and future residents.

sales prices 4-24 percent higher than those of neighborhoods farther away from public transit. Home price gains in these transit-oriented communities make sense because these areas typically are in high demand, where more businesses, restaurants, and opportunities tend to be located.

According to the same study by the NAR, homeowners also have flexibility when they live near public transit—1 in 4 homes were shown to not own a car. Additionally, average yearly transportation costs of households near transportation were between \$2,500

The Salt Lake Metro area saw a

19.3% increase

in walkable areas over car-dependant areas from 2012-2019

Figure 3.15 Walkable neighborhoods are expanding faster than car-dependent neighborhood in Utah.

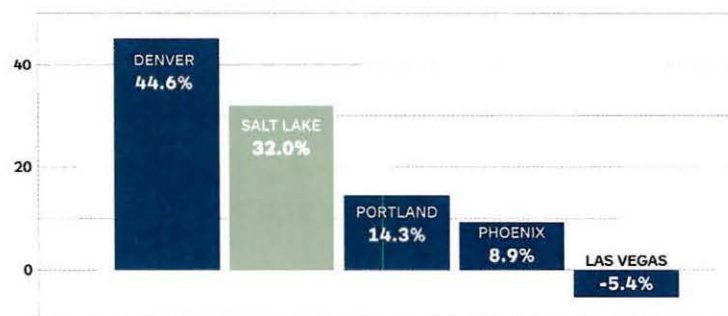


Figure 3.16 Walkability premium comparisons by major metropolitan area.

and \$4,400 less than those farther away. Living near transit services makes the most sense for anyone who needs easily accessible public transportation for daily work commuters, reducing driving costs and vehicle wear and tear.

3.4.3 WALKABILITY

A recent study completed by the real estate website Redfin, showed that in two-thirds of large metropolitan areas, walkable neighborhoods have higher home values than car dependent ones. Additionally, walkable neighborhoods appreciated faster than car-dependent ones in 44 of 51 large metro areas in the past seven years.

Houses with high levels of walkability (according to the website WalkScore) command a premium over otherwise similar homes in less walkable locations. Estimates are that a single additional point of WalkScore is worth \$3,500 in additional home value. As shown in the graph above, in Salt Lake County walkable home prices are 32 percent higher than car-dependent homes. Additionally, walkable homes have increased in price 19.3 percent faster than car-dependent homes.

The walkability premium is a clear market signal of the significant and



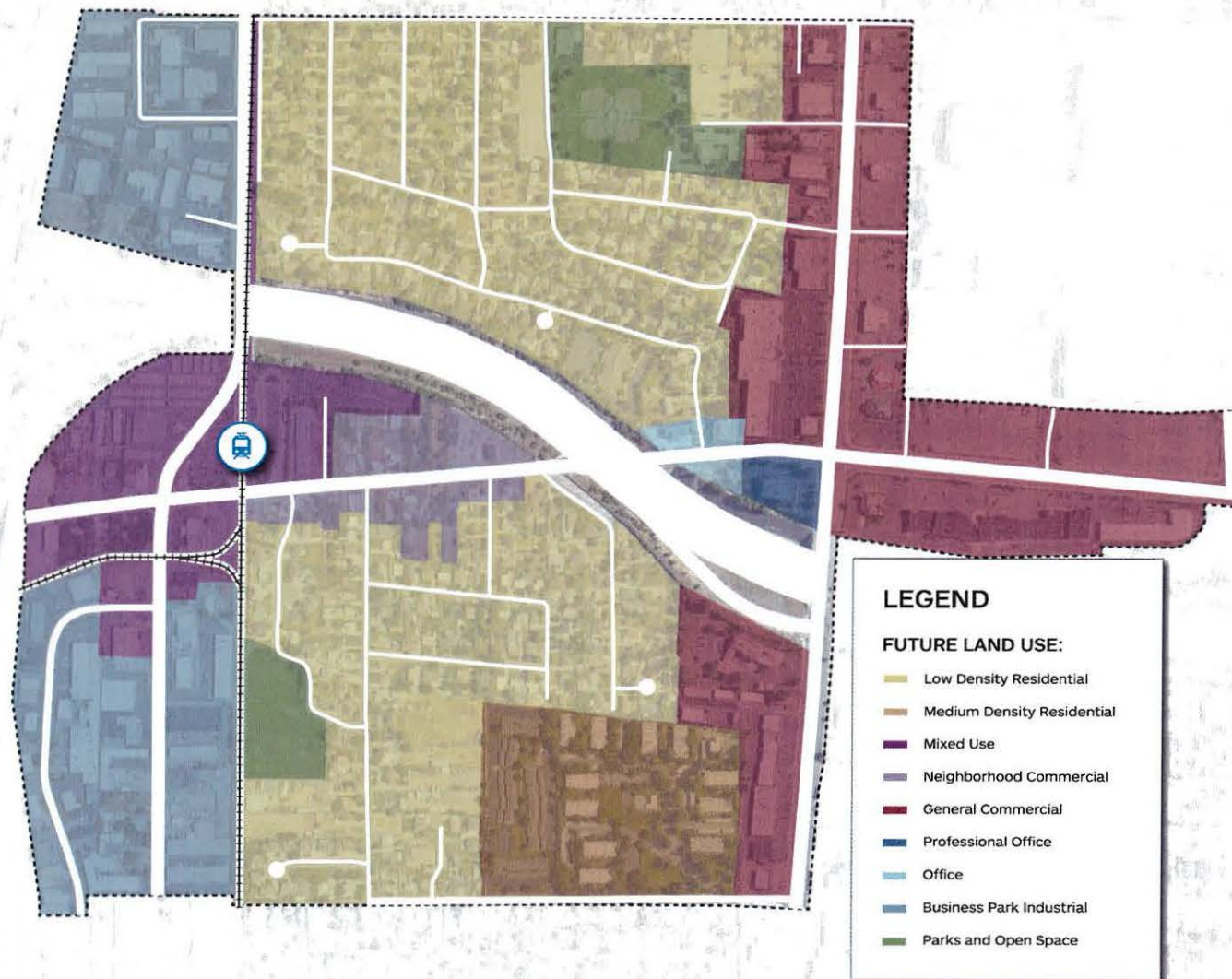


Figure 3.17 Murray City Future Land Use Map as determined by the 2017 General Plan.



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growing value Americans attach to walkability. It is also an indication that we have a shortage of walkable urban centers to meet the demand of walkable urban-style places. We have not been building new walkable neighborhoods in large enough numbers to meet demand; nor have we been adding housing in the walkable neighborhoods we already have fast enough to house all those who would like to live in them.

3.4.4 15-MINUTE NEIGHBORHOODS

The “15-minute neighborhood” principle is aimed to make each neighborhood a place where residents can live as locally as possible. The concept stems from sustainable community planning work done in Melbourne, Australia by the Department of Environment, Land, Water and Planning in the state of Victoria.

A 15-minute neighborhood must be safe, accessible and well connected for pedestrians and cyclists in order to optimize active transportation. This neighborhood should offer open space, services and destinations, access to public transit, diverse housing options, and be able to facilitate thriving local economies. Neighborhood activity centers are fundamental to the “15-minute” principle and will provide residents with services and destinations.

More than anything, living as locally as possible cuts down on transportation costs, which has a positive effect on improving the affordability of housing in the neighborhood. This principle is important for the residents in Murray’s Fashion Place West neighborhood that may have limited resources for housing and transportation. A 15-minute neighborhood gives residents access to their daily needs within a 15-minute walk, cycle, or public transit trip to and from their place of residence. 15-minutes is considered a “walkable” distance for pedestrians to meet most of their needs, including employment, housing, parks, education, transit and other daily needs.

3.5 MURRAY POLICY

Of the approximately 245 acres and 777 parcels that make up the Fashion Place West study area, 577 or 74 percent of those are residential land uses. The remaining 200 parcels make up the other 26 percent of the parcels and are occupied by non-residential land uses.

The existing housing stock in the Fashion Place West study area is aging. Most of the single-family homes were built in the 1960s with one smaller development built in the 1990s. There are also two multi-family developments within the study area. The South 67 Condos were built in the 1970s, and are an individually owned townhome style development.

The existing single-family residential homes along Winchester Street are not a complementary use, given the speed and frequency of traffic on the road. New residential construction should complement the area in massing, while offering a variety and differentiated housing types than what currently exists. Overall, the housing stock within the Fashion Place West neighborhood lacks diversity. The area is primarily market-rate single-family homes with one apartment development, and one condominium development.

3.5.1 2017 GENERAL PLAN AND HOUSING

The recommendations and strategies in the Housing section are built on the City’s goals from the 2017 Murray General Plan. The Future Land Use Map above gives a visual representation of the General Plan, which discusses the concept of preserving existing housing and expanding housing choice throughout the City. Due to the current housing shortage in the state, housing is a key issue to be addressed.



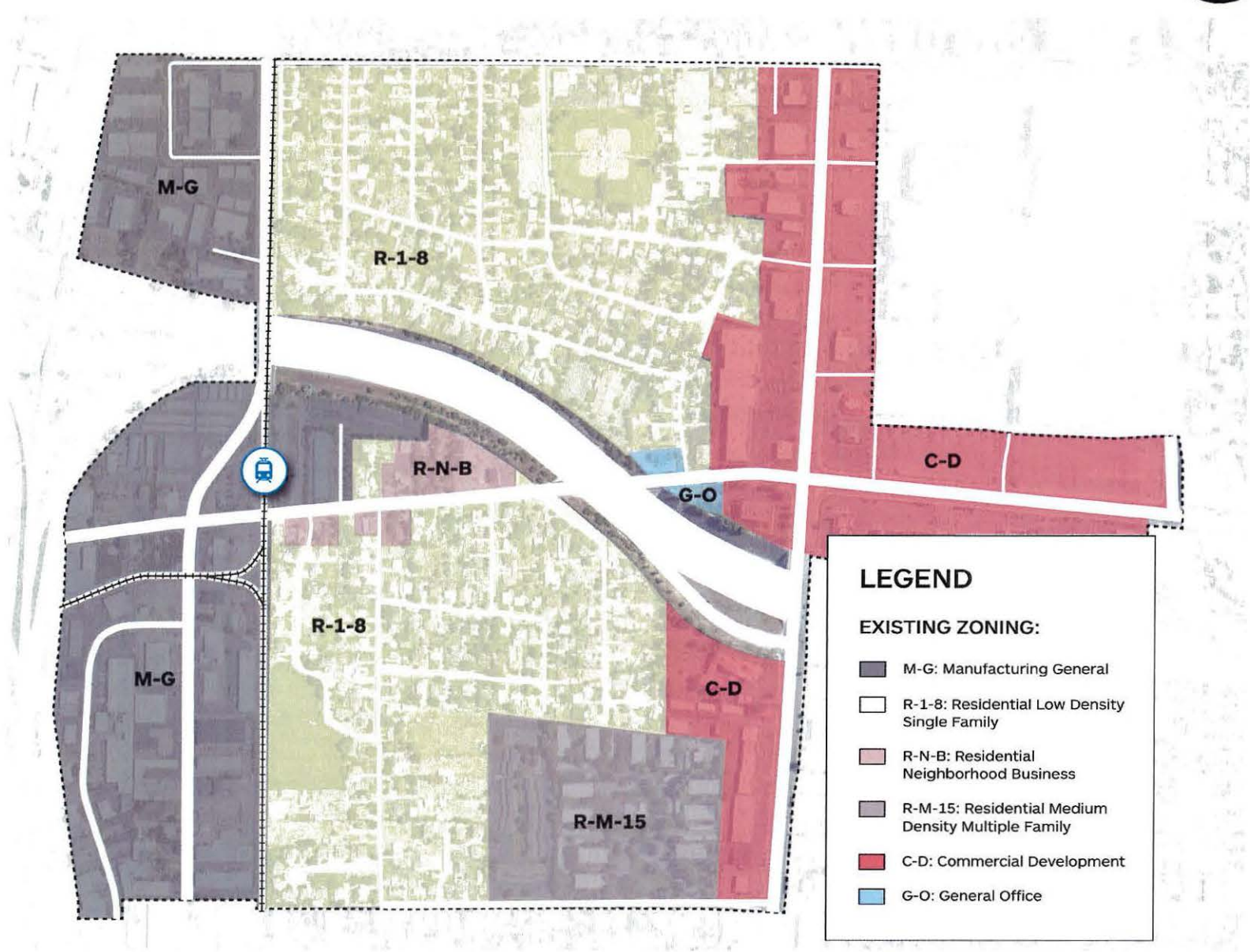


Figure 3.18 Map of existing zoning designations. Future zoning changes should be based on achieving the goals for the future of the Fashion Place West area.



3 HOUSING

3.5.1.1 KEY INITIATIVE #3

Initiative #3 in the General Plan is based around creating Livable and Vibrant Neighborhoods.

In order to create success around this General Plan Initiative, corresponding land use and zoning regulations must be amended in order to provide more opportunities for life cycle housing within residential areas. Life cycle housing can include many different types, but diversity in housing means providing a variety of housing types that are accessible to all income levels. Single-family homes, town homes, duplex and triplex units, apartments, and ADUs, (such as mother-in-law apartments) are examples of the many different housing styles that the neighborhood could utilize.

3.5.1.2 CHAPTER 5: LAND USE AND URBAN DESIGN

Chapter 5 of the General Plan describes general recommendations for future land uses and urban design. Objectives that support this goal as it relates to housing include providing a mix of housing options and residential zones to meet a diverse range of needs related to lifestyle and demographics, including age, household size, and income.

3.5.1.3 CHAPTER 8: NEIGHBORHOODS AND HOUSING

The Neighborhoods and Housing section of the General Plan prescribes various methods to plan for the future of Murray's residential neighborhoods. The goal of this section is to "provide a diversity of housing through a range of types and development patterns to expand the moderate-income housing options available to existing and future residents."

3.6 CURRENT ZONING

The zoning map, when it was adopted, predominately mirrored pre-existing land uses. The current zoning in the study area allows for residential but predominately very low density.

The R-1-8, R-M-15, and R-N-B zones are the only zones in the study area that allow residential development. The R-1-8 designation is applied to all single-family homes within the study area, both north and south of I-215.

The **Single-Family Residential (R-1-8)** adjacent to Winchester Street should transition to a zoning designation that is more conducive to the major arterial that is Winchester Street. Residential land uses that are appropriate for parcels along major thoroughfares include higher density residential and a mix of uses. The single-family housing stock in the study area fills a need in the housing market and should largely be left preserved with the existing zoning.

The **Multi-Family (R-M-15)** zoning designation includes the apartment complex as the condominium complex within the study area. The density and height should be increased for those parcels that are adjacent to the Commercial Development (C-D) zone. The parcels that are adjacent to single-family homes should increase in density but be required to be a lower height or density at the property line and step up to the maximum density as the buildings near the Commercial District zone.

The zoning in the study area does not allow a mix of uses. In a successful transit-oriented development, a mix of uses is encouraged. This mix usually is in reference to ground floor active commercial uses with residential units above. These residential units can be a for-sale or for-rent product and of varying sizes.



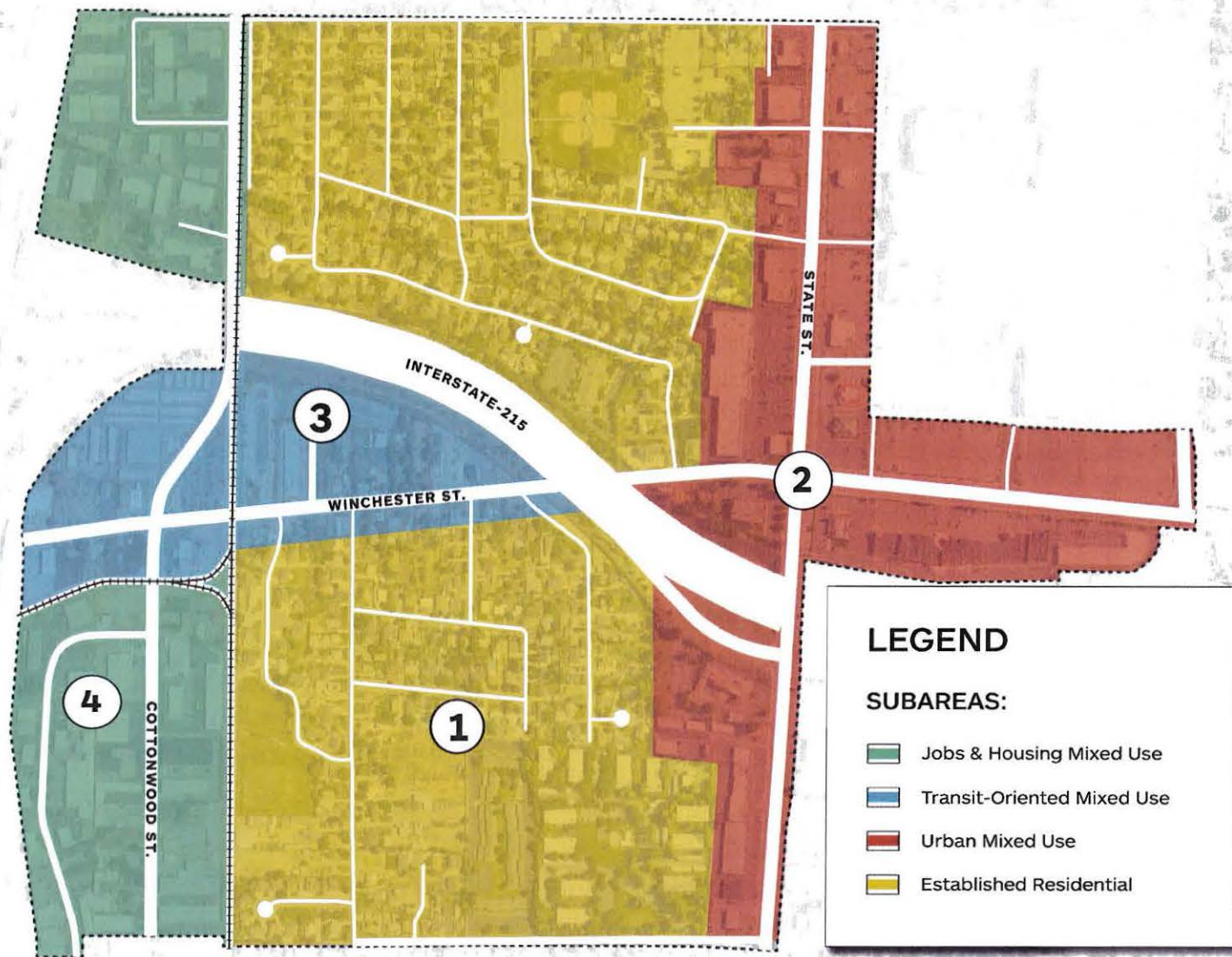


Figure 3.19 Map of sub-areas within the Fashion Place West study area. Residential use recommendations vary by sub-area.

3 HOUSING

The **Manufacturing (M-G)** designation is applied to a majority of the parcels that surround the TRAX station as well as the western portion of the study area along I-15.

This area has natural breaks from the single-family homes with the rail line, I-15, and I-215. Those facts make this an ideal location to transition to four to seven story residential towers in the future.

Residential uses around transit stations and adjacent to freeways should include much higher densities as they are not adjacent to single-family or lower density homes. The highest residential densities should be concentrated at points closest to the Fashion Place West TRAX station and the areas between the rail lines and I-15.

The **Commercial District (C-D)** zone is the eastern portion of the Fashion Place West study area. This zone includes Fashion Place Mall and the east and west sides of State Street within the study area. While the C-D zone has height allowances that are somewhat favorable for this area, residential is not currently allowed in this zone.

While the **Mixed-Use Development (M-U)** zone is not applied to parcels within the Fashion Place Study area, the M-U zone is versatile enough that it could be applied in and around the Fashion Place West Station area. Higher density uses including residential are encouraged, and single-family homes and duplexes are not permitted.

Text amendments are necessary in order to encourage and incentivize more housing in the area. Increased densities are necessary given the geographic location, housing demand in the region and throughout the state, and proximity to the TRAX station.

3.7 HOUSING SUMMARY AND RECOMMENDATIONS

In order for transit-oriented development to be successful, it is important for advocates to also be strong supporters of new housing development. The demand for walkable living across varying demographic groups is quite positive for most communities, particularly those that can provide good transit service and access to job centers and recreation, like the Fashion Place West neighborhood.

One of the key strategies of the The Wasatch Front Regional Council's (WFRC) Regional Transportation Plan (RTP) is to focus growth around multi-modal transportation neighborhood centers. These centers are created using community input and are reflective of the desires of the local population. These centers can become the focus of a strong market for moderately priced and life cycle housing for all income levels, as well as accessible jobs and services.

Unfortunately, many communities struggle to build more housing choices, often due to public misconception. Public and political resistance to increased residential densities often needed in order for projects to be viable, often prolong the development process several months, if not years, making a community far less attractive to developers.

With the current optimism and excitement apparent in Murray, it is vital that new housing growth be seen as a positive rather than a negative. The support of City officials is critical for the successful growth and development of context-sensitive housing. In addition, Murray's Fashion Place West neighborhood has a great deal of under-utilized land that could be converted to more productive and active uses, such as housing and mixed-use projects.



SUB-AREA 1: ESTABLISHED RESIDENTIAL

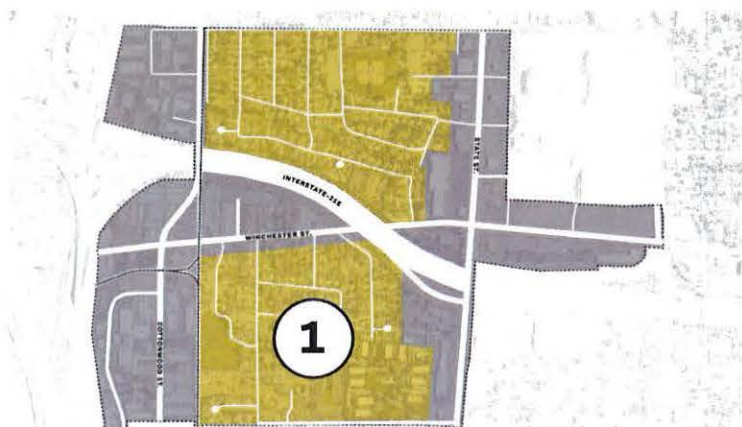


Figure 3.20 The single-unit neighborhoods within the Fashion Place West study area are well established and are an asset of great value to the City. These neighborhoods should be preserved, with the exception of infill development where underdeveloped parcels exist within the neighborhoods. Using development along Winchester to buffer this neighborhood can also create a wider range of housing choice within the area.

DUPLEX DEVELOPMENTS



Figure 3.22 Duplex units are an appropriate housing type in this sub-area. Duplexes provide the benefit of adding housing units to the neighborhood, increase density, all while maintaining the aesthetic of the area.

ACCESSORY DWELLING UNITS (ADU)



Figure 3.21 Accessory Dwelling Units (ADUs) constructed over a single-family home's garage is an example of a way to provide life cycle housing in the study area. This dwelling type is encouraged to allow more people to live in the neighborhood without greatly impacting the look and feel of it.

SINGLE-FAMILY RESIDENTIAL



Figure 3.23 This sub-area is primarily a single-family neighborhood. This type of housing remains an appropriate housing type in order to maintain the character of the neighborhood.



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SUB-AREA 2: URBAN MIXED-USE

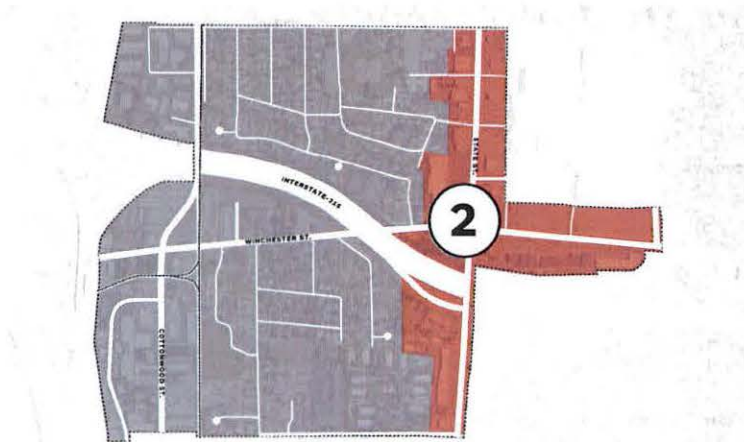


Figure 3.24 The area along State Street including Fashion Place Mall may densify over time. With State Street accommodating such a large volume of cars each day, as well as the proximity to both I-15 and I-215, there will be a great demand for this area to transition to a more urban style of development. Properties currently adjacent to State Street are ripe for redevelopment, where taller buildings could be constructed to address State Street to create a more urban environment. These types of developments could support the higher costs of taller construction methods.

2-4 STORY RESIDENTIAL WITH MIXED-USE



Figure 3.26 Middle density residential with a mix of ground floor uses are appropriate in this sub-area, due to the current land uses and densities, as well as nearby public transit.

HIGH DENSITY RESIDENTIAL WITH GROUND FLOOR MIXED-USE



Figure 3.25 Given the urban and commercial nature of the Street corridor, higher density residential uses are appropriate. The most dense projects should be located along main thoroughfares such as State Street and 6800 South.

4-6 STORY MULTI-FAMILY RESIDENTIAL



Figure 3.27 Four to six story residential development is recommended in the Urban Mixed-Use sub-area where buildings are not adjacent to major thoroughfares or single-family residential.



SUB-AREA 3: TRANSIT-ORIENTED MIXED-USE

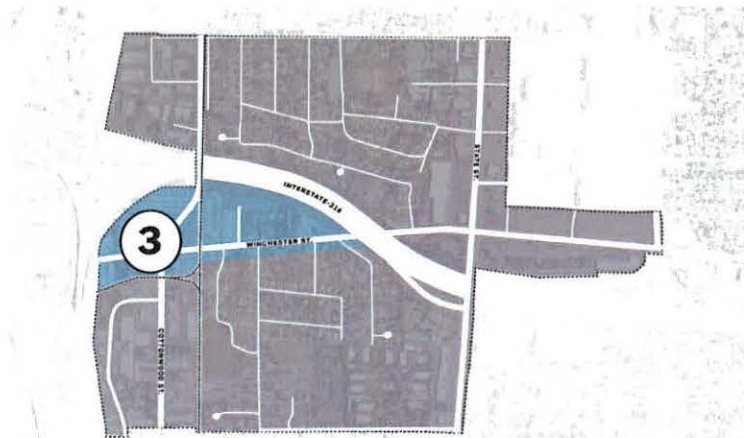


Figure 3.28 Over time the area adjacent to the TRAX station will become even more valuable given its proximity to transit service. New development will be more dense than current land uses and will be primarily residential uses and commercial uses including service related uses, restaurants, as well as other types of uses that support and are supported by the proximity to the TRAX station.

2-4 STORY RESIDENTIAL WITH MIXED-USE



Figure 3.30 Middle density residential with a mix of ground floor uses are appropriate in this sub-area, due to the current land uses and densities, as well as nearby public transit.

ROW HOMES



Figure 3.29 Row homes are a more dense housing type than town homes. Row homes are an appropriate housing type in sections of the sub-area where parcel sizes are smaller and do not allow for development as dense as multi-story multi-family residential.

TOWN HOMES



Figure 3.31 The TRAX station area is immediately adjacent to single-family homes in some areas, and adjacent to freeways, rail lines, and industrial in others. Town homes are an appropriate housing type that can be co-located near single-family homes in this sub-area.



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SUB-AREA 4: JOBS AND HOUSING MIXED-USE

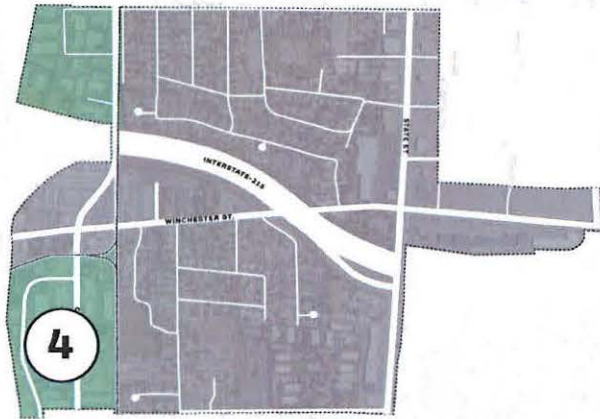


Figure 3.32 In the long term, as the valley increases in population, this area will increase in value and eventually transition from its current land uses to a densified jobs center that incorporates residential components.

OFFICE MIXED-USE DEVELOPMENT



Figure 3.33 With great proximity to transportation networks and other job centers, this area may transition to more office and mixed-use type developments.

2-4 STORY RESIDENTIAL WITH MIXED-USE



Figure 3.34 Middle density residential with a mix of ground floor uses are appropriate in this sub-area, due to the current land uses and densities, as well as nearby public transit.

4+ MIXED-USE DEVELOPMENT



Figure 3.35 As part of the job and housing mixed-use sub-area, condominium developments are an appropriate housing type. Condos provide a for-sale option to home buyers while providing more housing near transit service.



3.8 HOUSING IMPLEMENTATION STRATEGY

This implementation strategy weighs current market conditions, regulations, and best practices. These important factors help to identify and outline clear priorities and policy amendments that will improve housing development and opportunity within the study area.

3.8.1 HOUSING PRIORITIES

In order to expand housing choice in the study area, the following priorities have been identified:

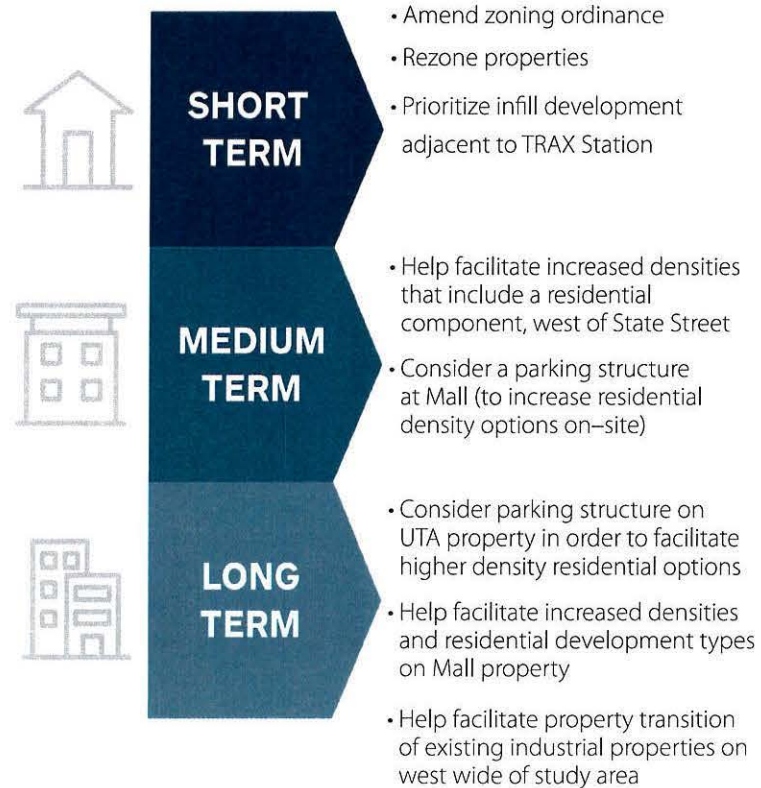
1. Offer services and amenities near housing.
2. Provide housing for all stages of life.
3. Create a walkable neighborhood.
4. Increase residential allowable densities for development along and adjacent to the Fashion Place West TRAX station, I-15, and State Street, by increasing parking densities using structured parking in conjunction with mixed-use developments.
5. Address established residential neighborhoods by creating responsible transitions between existing residential and new, higher density developments.
6. Incorporate a mix of uses into new residential developments as well as existing single-use zone districts.

3.8.2 POLICY UPDATES AND LAND USE AMENDMENTS

Policy changes the City can implement will begin the process of change for the study area, including the following:

1. Create new Fashion Place West zone district (FPW) modeled off existing TOD zone with the following revisions:
 - (a) Parking
 - (i) Include shared parking provision.

- (ii) Reduce residential requirements contingent upon proximity to TRAX station, shared parking calculation, etc.
 - (iii) Implement parking maximums.
 - (b) Reduce front yard setback from 15 feet and 25 feet, to 0 feet
 - (c) Implement maximum setback requirements.
 - (d) Consider a decrease of open space percentage requirements from 20 percent to 10 percent.
 - (e) Ground floor activation, requirements, and language.
2. Re-zone areas within the study area per recommendations of the General Plan.



4 CONNECTIVITY



4.1 INTRODUCTION

The study area is home to the Fashion Place West TRAX station, a major transit hub located over half a mile from Fashion Place Mall. Transporting people, especially to and from the TRAX station to the mall, is key to the area's continued economic vitality. While motorized vehicular infrastructure is well-established, pedestrian, bicycle, and transit infrastructure are not consistent through the area, and safety features could be added.

4.2 PREVIOUS PLANNING EFFORTS

4.2.1 2008 LIFE ON STATE

In 2008, the Life on State project established a shared vision for the future of the valley's 17-mile-long central corridor, State Street. The project was a collaborative effort between all six cities along State Street, Wasatch Front Regional Council (WFRC), Utah Department of Transportation (UDOT), Utah Transit Authority (UTA), Salt Lake County (SLCo), Salt Lake Chamber, Murray Chamber of Commerce, and the Downtown Alliance.

The vision for State Street was built on broad involvement with residents and stakeholders, and was detailed in the document. The belief was that this collaborative effort would create a safe environment for private investment consistent with the vision. The concept was that moving in a new direction was not as risky a proposition if it is backed by a strong, enduring commitment from the partnership.

4.2.2 MURRAY CITY GENERAL PLAN

The Murray City General Plan emphasizes the City's desire to improve accessibility for pedestrians, bicyclists, and public transit riders in the corridor between I-15 and State Street to provide adequate infrastructure for existing and planned commercial development.



Figure 4.0 Walkable and human scale nodes create a more inviting place.

4.3 BEST PRACTICES

4.3.1 CONNECTIVITY

Establishing better connections and improving the street grid between commercial areas, public transit, and surrounding neighborhoods begins by identifying locations, such as Fashion Place Mall, where the established street grid is not maintained, and establishing a plan to extend the grid when new development or redevelopment occurs. This will increase connectivity and diminish the island effect that is commonly created by these types of commercial land uses.

Designing and planning to implement more human-scale building design standards and improved streetscapes will help to guarantee that future development follows the grid with street design, building massing, and connectivity.

4.3.2 WALKABILITY

The experience of an individual on foot in an urban place can have lasting impacts on how a person feels about their community. Walkability is influenced by many factors, many of which are the degree to which human-scale design concepts are addressed. Slowing auto



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traffic, encouraging ground-floor activation of buildings, improving streetscapes, incorporating public art elements, and shortening distances between destinations can create more walkable places. According to *Foot Traffic Ahead*, published in 2019 by the George Washington University School of Business and Smart Growth America, retail space in well connected walkable commercial areas can rent for 121 percent (over two times) over drivable suburban commercial space.

Walkable places are increasingly valued by potential residents, visitors, business owners, developers, and property owners. Findings in a recent report show that walkable urban places are also extremely economically beneficial to the local municipalities in which they reside, with properties in these areas also highly valued. Walkable urban office space has a 105 percent rent per square foot over drivable suburban space.

4.3.3 PLANNING FOR FUTURE TRAFFIC

With projected growth and development in and around the study area, traffic is likely to increase. The following measures offer a variety of ways to mitigate traffic and plan for future growth.

4.3.4 TRAFFIC ANALYSIS AND MITIGATION

Personal vehicles are a primary mode of transportation in Murray, leading to congestion on certain roadway segments during peak hours. Signals throughout the study area should be optimized and synchronized as an inexpensive and quick way to mitigate congestion. If signal timing adjustments do not alleviate the congestion—turn bays might need to be added or lengthened. Adding lanes should be a last resort in alleviating traffic congestion as implementation is expensive, occupies valuable right-of-way, increases the number of conflicts, and increases crossing distances for pedestrians and bicyclists.

The oncoming development around Winchester Street and 700 West



Figure 4.1 Successful connectivity includes consideration of active transportation.

will increase traffic along Winchester, likely impacting the study area. The intersection should be properly adjusted using the above techniques to ensure a satisfactory level of service. Additionally, the signal at Winchester Street and Cottonwood Street should be synchronized with the signal at Winchester Street and 700 West to prevent backups and delay. As of 2016, Winchester Street had 11,000 annual average daily traffic (AADT) of its 16,000 AADT capacity. Winchester still has 5,000 AADT capacity to absorb additional traffic from new development.

4.3.5 LEVEL OF SERVICE AS A MEASURE

Level of Service (LOS) has been the standard method to evaluate the operational efficiency of an intersection for vehicles and for determining vehicular impact from developments. LOS is a calculation of delay per vehicle at a given intersection, ranging from A (least amount of delay) to F (worst amount of delay). It is not until recently that communities have begun to revise their measures of intersection quality and development



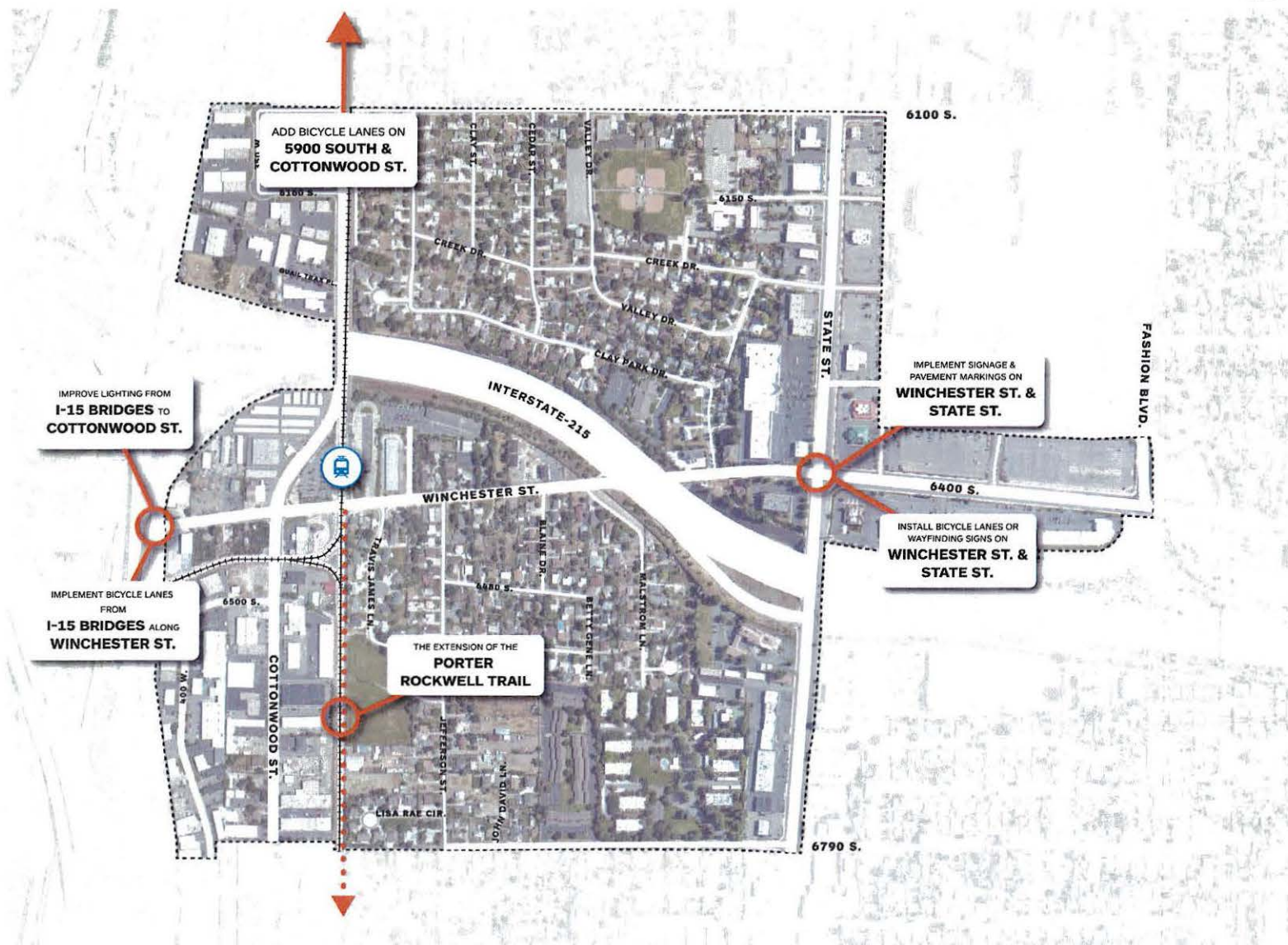


Figure 4.2 The Fashion Place West neighborhood lacks adequate bicycle infrastructure. The map above shows suggested future improvements that would increase overall bicycle connectivity within the area and to the rest of the neighborhood

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impact. The state of California adopted Vehicle–Miles Traveled (VMT), a method that measures the total distance traveled by individual roadway users along a corridor or in a network, as the new method for roadway flow evaluation, replacing LOS under SB-743. This new method analyzes traffic along with land use to reduce necessary trips and accounts for all users of a roadway network whereas LOS only analyzes the flow of motorized vehicles through an intersection. VMT was prioritized over LOS in California to report on the efficiency of a roadway network as well as describe the environmental effects associated with fuel consumption, emissions, and public health.

VMT is calculated by the Institute of Transportation Engineers (ITE) Trip Generation rate multiplied by the individual trip length. The further users are required to travel, the higher the VMT. Similarly, as the number of users required to travel increases, the VMT increases as well. Different land use scenarios affect VMT—integrating daily services within residential areas lowers the distance required to travel, thus lowering the VMT.

VMT projections are already included in the Wasatch Front Regional Council (WFRC) travel demand model and should be used when planning for future growth. This can be analyzed by an individual project (i.e., the trips to and from a new grocery store) or by the impact of an individual project on a network (i.e., the trips to and from a new grocery store would reduce VMT to and from existing grocery stores, thus decreasing the VMT for the greater area). While VMT does not have specific thresholds as LOS does, generally a reduction in overall network VMT is considered successful.

In addition to utilizing VMT as a metric, accepting a lower LOS (i.e. LOS E or F) is becoming more popular in the more urbanized areas throughout the western United States. The Sugar House neighborhood in Salt Lake City is a local example where priority has been given to all other modes



Figure 4.3 Traffic congestion along Winchester Street is a major community concern as expressed in a recent survey of residents in the area.

of transportation before motorized vehicles. This has helped keep the right-of-way at a manageable size for all modes of transportation and also encouraged more economic growth. This same approach can be taken throughout the Fashion Place study area, particularly along State Street, Winchester Street, and Cottonwood Street as they provide direct connections to major attractions and residential neighborhoods in the study area. Prioritizing VMT over LOS will encourage a more multi-modal and mixed-use environment, therefore reducing pollution and noise, making the area more enjoyable for both residents and roadway users. The entire study area itself has the potential to become a destination, rather than solely the pockets around popular attractions.

Recommendations for the Fashion Place West study area include considering VMT in evaluating the efficiency of traffic flow with the understanding that a low-ranking LOS at certain intersections might in fact promote other modes of transportation and move more vehicles through a corridor.



4.3.6 INDUCED DEMAND

Induced demand is the additional travel associated with a lower cost or lower time necessary to make a trip. These extra trips often occur due to the widening of an already congested roadway as additional lanes initially reduce travel time and fuel costs. However, the corridor soon reaches its capacity in a matter of years, as shown by a study done by Fehr & Peers in conjunction with Caltrans, U.C. Davis, and the California Governor's Office of Planning and Research. Induced demand also applies to the installation of walkways and bike lanes. Creating a safe space for these vulnerable users encourages an increase in non-motorized traffic. Induced demand explains both the idea that more lanes mean more traffic, and the notion that building infrastructure for alternative modes encourages people to use those modes. Overall, induced demand is the concept that proper infrastructure brings more users than existing conditions.

Implementation recommendations include safe pedestrian and bicycle infrastructure to encourage an increase in non-motorized users to reduce the amount of vehicular traffic on area roadways. Connections to the Fashion Place West TRAX station are particularly important as the station is a hub for pedestrians and cyclists. Implementing bike lanes and wide sidewalks along Winchester Street and Cottonwood Street would provide safe access for cyclists and pedestrians to the area's neighborhoods and to Fashion Place Mall. A crosswalk on Cottonwood Street at the northern side of the TRAX station would provide convenient pedestrian access to jobs and homes on the northern side of I-215. Providing safe and convenient infrastructure to non-motorized users, particularly at this transit hub, offers a competitive alternative to driving a car. This will in turn induce a higher use of active transportation modes which activates spaces and increases the vibrancy of the area. Adding more lanes to roadways in the study area should be avoided where possible as this will encourage more vehicles on these already high-volume roadways.



Figure 4.4 15-minute neighborhoods provide all necessary services and conveniences within a 15-minute walk from home.

4.3.7 15-MINUTE NEIGHBORHOODS

The concept of the 15-minute neighborhood entails mixing land uses and optimizing transportation networks so that daily needs—from work, to shopping, to recreation—are within 15 minutes of the home by foot or by bike.

Proper transportation infrastructure increases the reach of this 15-minute neighborhood. Implementing pedestrian and bicycle infrastructure in and around key destinations such as grocery stores, office centers, and parks is an effective way to achieve this concept. Pedestrian and bicycle infrastructure should not only be installed wherever possible, but also designed as a fluid connection from one destination to another. Increasing the number of daily trips that can be

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made without a car will in turn reduce roadway congestion.

Implementing the 15-minute neighborhood concept in the Fashion Place West neighborhood would improve public health and well-being, create more diversity in access to services, and better place-based design.

More than anything, living as locally as possible cuts down on transportation costs, which has a positive effect on improving the affordability of housing in the neighborhood. This principle is important for the residents in Murray's Fashion Place West neighborhood who may have limited resources for housing and transportation.

The Fashion Place West study area is well-suited for a 15-minute neighborhood. Fashion Place Mall is a central service hub, providing several daily needs in a single location. Furthermore, the Fashion Place West TRAX station provides access to other major urban nodes in the Salt Lake City metropolitan area.

Implementing safe and consistent infrastructure—wide, well-lit buffered sidewalks, well-maintained crosswalks, and dedicated bike lanes—will greatly increase non-motorized access to daily services.

To complete the 15-minute neighborhood concept, first and last mile connections will require similar bicycle and pedestrian infrastructure. Sidewalks and bike lanes should be improved within Fashion Place Mall parking lot, connecting users from the street to the mall doors. Likewise, residential areas should feature trails and bikeways to connect users directly to their home.

This concept aligns with many of the Fashion Place West Small Area Plan's stated goals for improved transit and active transportation use, improving connectivity and improving overall neighborhood quality.

4.3.8 ACCESS TO OPPORTUNITIES (ATO)

Access to Opportunities (ATO), is a way to measure how well people can connect to basic needs and amenities including jobs, schools, grocery, retail, parks, community centers, and entertainment. On a broad scale, ATO metrics quantify how well current and future transportation networks and infrastructure coordinate with land uses in order to assist local economies and communities to thrive.

Increased accessibility can have significant impacts on overall community livability while improving residents' connections to the services necessary to promote upward mobility such as education, employment, healthcare, social services, and other basic amenities. ATO could also serve as a guide for Murray City to pursue the best possible transportation planning and land use decisions in support of community choice and economic vitality.

4.3.8.1 UNDERSTANDING NEEDS OF VULNERABLE COMMUNITIES THROUGH ATO

The Federal Highway Administration (FHWA) defines under-served individuals as those that are low Income, a minority, elderly, a child, have limited English proficiency, or those with disabilities. Vulnerable Communities are those census block groups where any of the following conditions is met:

- Greater than 25 percent lower income populations are highlighted, as a lack of access to reliable and efficient transportation can be a major barrier to economic mobility
- Greater than 40 percent minority populations are included in this definition, as many land use and transportation investments in the U.S. have, historically, adversely impacted racial and ethnic groups. WFRC strives to prevent future projects from having a similar disproportionate impact



- Greater than 10 percent zero-car households are included, as these are populations which include those with disabilities, depend more on transit, paratransit, walking, and bicycling to reach employment and other destinations

ATO can help communities understand the separation of residents from employment opportunities and other basic needs, at a neighborhood level. This is especially crucial for under-served populations that would benefit most from alternative modes of transportation to access daily services.

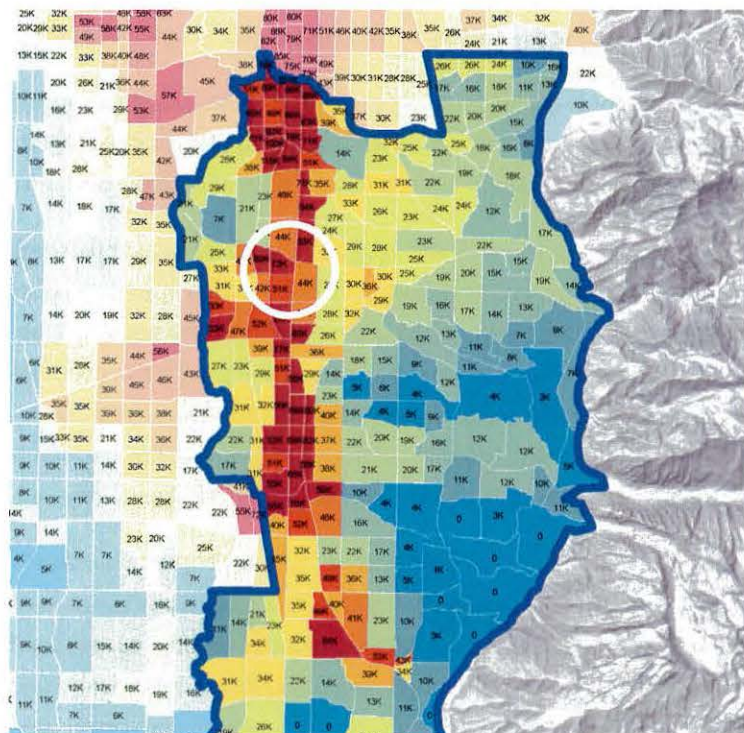


Figure 4.5 Household Access to Jobs: Transit For each traffic analysis zone (TAZ), colors indicate household accessibility to jobs, within a typical transit commute, relative to the average score for the highlighted area. The labels indicate the number of jobs accessible to each TAZ's households within a typical transit commute. (84K = 84,000 jobs) More info: <https://bit.ly/2QRt9gO>

4.3.8.2 STRATEGIES FOR INCREASING ACCESS

An Access to Opportunities measure can facilitate decision-making for and beyond transportation planning, in supporting upward socioeconomic mobility. Cities and developers can improve access to opportunity in a myriad of ways by mixing uses and clustering growth near high speed and high frequency transit.

Land use solutions that improve Access to Opportunities include:

- Growth centers near high-capacity transportation,
- Higher density development between 2-6 stories depending on location,
- Intermixing homes and jobs, and
- Street design that encourages local investment along the street.

Transportation solutions that can improve Access to Opportunities include:

- Reduced congestion,
- A more connected street network,
- Increased transit frequency and coverage,
- Bicycle and pedestrian connections, and
- Higher travel speeds on key commuter (non-neighborhood) routes.

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4.3.9 WAYFINDING

Wayfinding can be a low-cost high-impact tool to increase mobility and promote commercial retail throughout the study area. Including informational signage at popular area destinations that direct pedestrians and bicyclists towards appropriate facilities will improve the convenience and safety of all roadway users.

The TRAX station should feature signs indicating the direction and distance of key areas such as Fashion Place Mall, Clark Cushion Senior Recreation Center, and Grant Park. Likewise, the mall should display informational signs at entrances and exits to direct shoppers and workers to the various transportation options available: parking areas, bicycle infrastructure, walkways, micromobility parking locations, bus stops, and preferred route to the TRAX station.

Furthermore, signage should be implemented along these alternative mode routes to reaffirm the route and encourage economic travel towards the commercial retail centers throughout the study area.

Winchester Street is in particular need of wayfinding as it directly connects the TRAX Station with Fashion Place Mall. Signs should be located at the exits of both the station and the mall to guide users.

Wayfinding signs should also be placed at the intersection of State Street and Winchester Street where cyclists must begin to turn into the mall property. Directing cyclists to the most robust bike infrastructure network can increase comfort and confidence of users.

Other locations that would benefit from wayfinding include Liberty Elementary School, Grant Park, Jefferson Park, as well as the future Porter Rockwell Trail extension.

4.4 STREETS AND BLOCKS

4.4.1 FREEWAY INFRASTRUCTURE

There are two freeway overpass bridges in the study area, one on Winchester Street and one on Cottonwood Street. Both bridges are in need of active transportation improvements due to narrow and cluttered sidewalks. The Cottonwood Street bridge has limited space due to the TRAX rails and only features a sidewalk on the west side of the bridge. This sidewalk is narrow (4 feet wide) and does not connect with the sidewalk on the north side of the bridge. While the Winchester Street bridge features sidewalks on both sides of the roadway, these sidewalks are also narrow (4 feet wide), covered with garbage, are in close proximity to traffic, and only separated by a chain-link fence from the freeway traffic below. It is an uncomfortable experience for the pedestrian and bicyclists. Recommendations include removing the two-way left-turn lane to make space for a wider sidewalk with a buffer when the bridge undergoes repair.

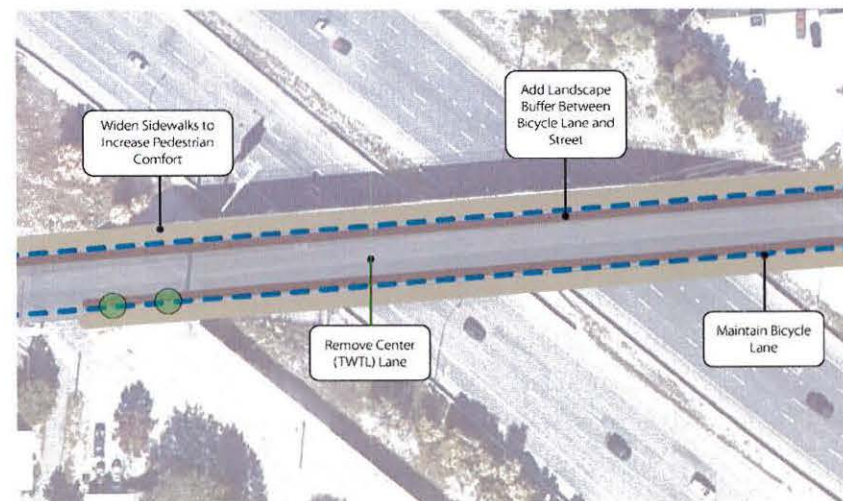


Figure 4.6 Future streetscape improvements along the Winchester Bridge would enhance the pedestrian experience and encourage use of more active transportation methods.



A second innovative option is to reconstruct the Winchester Street bridge with a wider structure to provide space for small shops to be located along the roadway. This would be a first-of-its-kind feature for Murray City and the State of Utah as the nation's first multi-use freeway overpass. A mixed-use environment would also create a lower-stress route for pedestrians to include a buffer between the below freeway vehicles and the vehicles on Winchester Street. This type of project would require heavy involvement from and coordination with UDOT.

4.4.2 INTERSECTION IMPROVEMENTS

The intersection of State Street and Creek Drive had 14 collisions from 2017-2019, 11 of which were making left-turn movements, primarily from State Street northbound onto Creek Drive and from the mall entrance westbound onto State Street. This intersection is located roughly 900 feet from the intersections at 6100 South and 6400 South, well under the threshold of the required 2,640 feet for UDOT signal spacing for this roadway. Restricting left-turn movements from either or both roadways would reduce the number of potential conflicts, increasing safety for the intersection.

The intersection of Winchester Street and 700 West is surrounded by developing property and will experience a growth in traffic volumes in the coming years. This growth will likely cause an increase in traffic towards local destinations such as the TRAX station and Fashion Place Mall, both of which are located along Winchester Street, likely causing an increase in traffic along the corridor.



Figure 4.7 The intersection of State Street and Winchester Street currently lacks a safe bicycle experience. Future improvement recommendations include better bicycle lane signage as well as sidewalk improvements.



Figure 4.8 The existing intersection at Creek Road and State Street is lacking pedestrian amenities. Improving this intersection will increase connectivity from the neighborhood to Fashion Place Mall.

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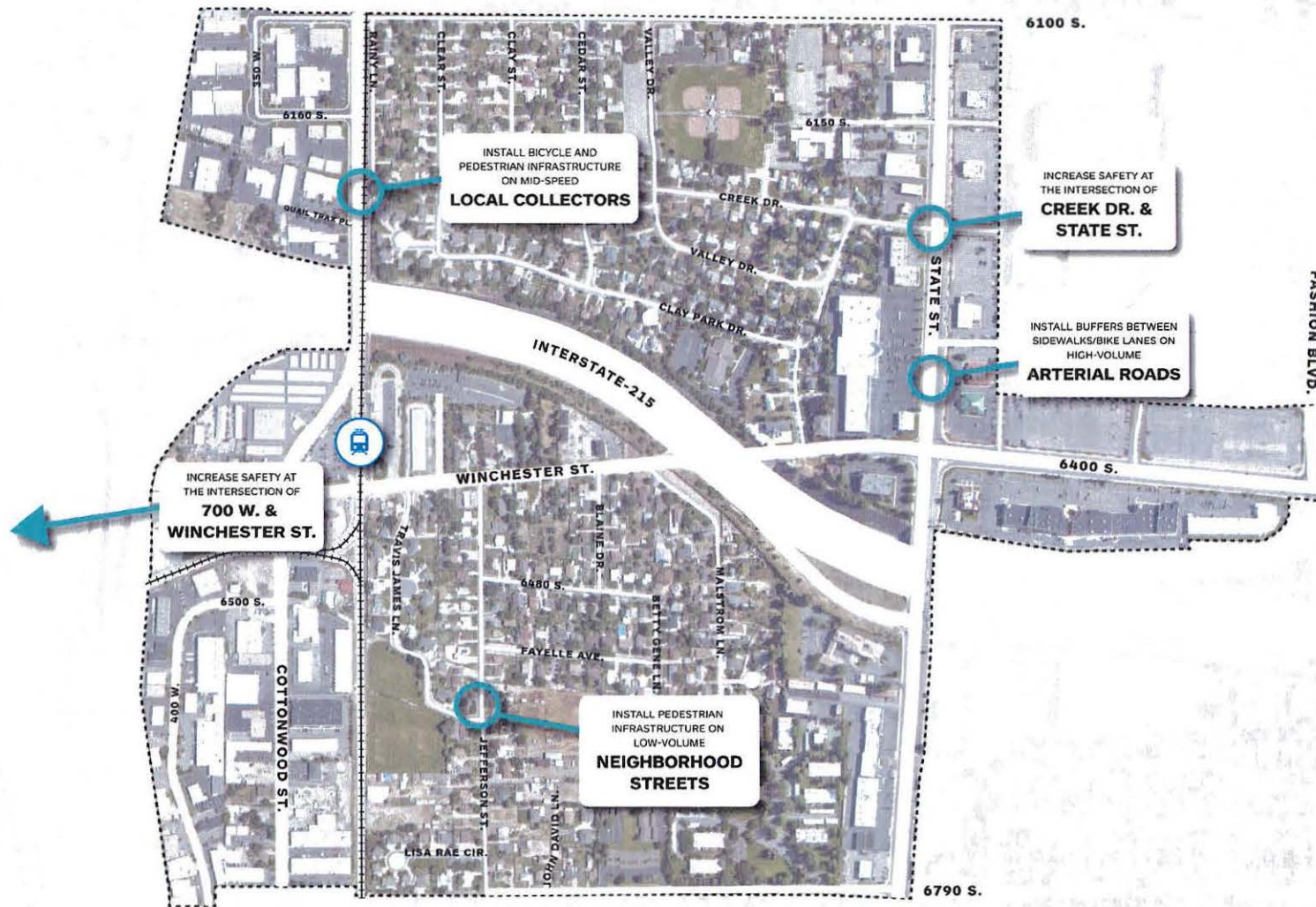


Figure 4.9 The map above illustrates suggested future improvements to the road network.



4.4.3 ROADWAY IMPROVEMENTS

Arterials: High-volume and wide roadways often are accompanied with higher speeds. It is very important to install buffers between sidewalks and bike lanes and the roadway to provide a lower level of stress and better sense of safety to non-motorized users. State Street experiences the highest level of vehicular traffic and has the highest speed limit in the study area. It is very important to implement proper pedestrian infrastructure to ensure the safety of all roadway users. Adding a buffer will increase pedestrian safety and decrease chances of vehicle-pedestrian collisions. All signals along the State Street corridor should be synchronized.

Collectors: These mid-speed roadways with great connectivity are very suitable for bicycle and pedestrian infrastructure. Cottonwood Street and Winchester Street are connectors in the study area. Both roadways should feature continuous bike lanes and sidewalks at least 6 feet wide with a buffer between the roadway.

Neighborhood Streets: These roadways operate at a low speed and volume and are typically safer for cyclists to ride in the roadway. Several neighborhood streets in the study area currently have no pedestrian infrastructure. Pedestrian infrastructure is vital to connecting homes to the larger mobility network. Recommendations include installing sidewalks and advisory shoulders—dashed lanes at the edge of the roadway reserved for non-motorist roadway users—where possible on all neighborhood roadways.

4.5 ACTIVE TRANSPORTATION

According to comments received during the public input process of the 2017 Murray General Plan, citizens would like to walk and bike more but do not feel safe to do so. Implementing the following recommendations can improve a user's comfort when using active transportation infrastructure.

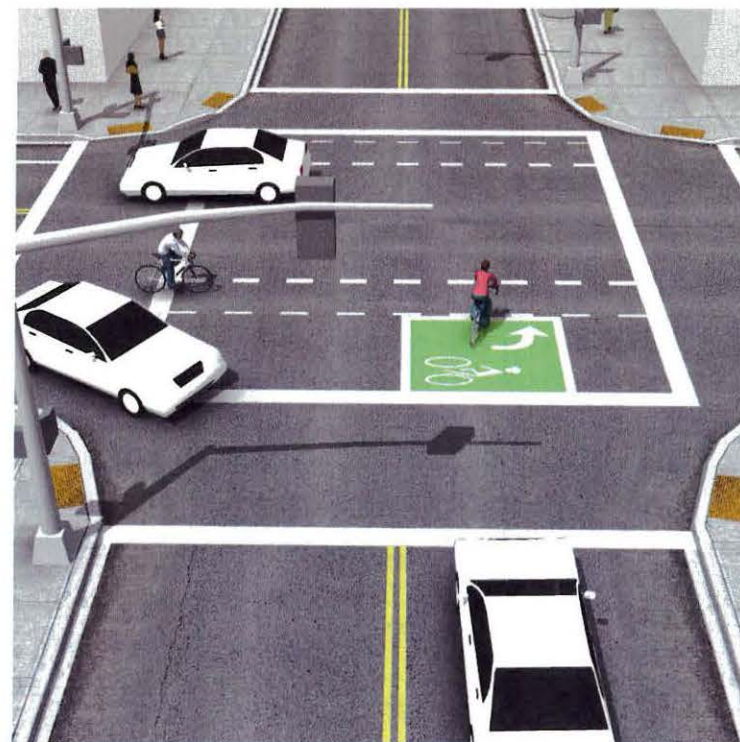


Figure 4.10 The FHWA outlines a two-stage bicycle turn box design similar to the ones implemented along 200 West in Salt Lake City which could be implemented at the Winchester and State intersection (image source: NACTO).

4.5.1 CYCLING ENHANCEMENTS

Incorporating bicycle friendly elements into the Fashion Place West neighborhood can take shape in many forms, including the addition of bicycle amenities, as well as supporting infrastructure improvements. Examples include bike racks, covered or indoor storage, and service stations for quick tune-ups or to fill flat tires.

Finally, bike lanes should be added to Cottonwood Street and 5900 South to provide a bypass for Winchester Street to the neighborhoods north of I-215 and to Fashion Place Mall.



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Figure 4.11 Effective bicycle connectivity within the Fashion Place West neighborhood has the capacity to increase activity in the area and reduce vehicular traffic.

4.5.2 BIKING RECOMMENDATIONS

The Fashion Place area experiences a fair level of bicycle activity as shown by Strava bicycle data, primarily along Winchester Street and Cottonwood Street. Beginning on the western edge of the study area, bike lanes and improved lighting should be installed along Winchester on the Interstate bridges to increase a rider's sense of safety. Further along Winchester Street, between Jefferson Street and Malstrom Lane, the bicycle infrastructure switches from a dedicated bike lane to sharrows back to dedicated bike lane to preserve on-street parking for certain residences. This on-street parking in the public right-of-way should be converted to dedicated bike lanes to reduce chances of conflict between motorized vehicles and cyclists.

Connecting the bicycle infrastructure from Winchester Street to Fashion Place Mall is of particular importance. A two-stage bicycle turn box at the intersection of Winchester Street and State Street, leading to a dedicated northbound bike lane (or sharrows if a bike lane is not possible) on State Street should be implemented to guide cyclists through this intersection that experiences the greatest number of cyclist crashes out of any other intersection in the study area.

In addition to these signs and pavement markings, signs warning motorists of cyclists should also be installed to increase awareness of the multi-modal intersection. From here, sharrows should be implemented from State Street through Fashion Place Mall parking lot to the mall entrance. Sharrows should similarly be installed from the doors of the mall through the parking lot to Winchester Street. Dedicated bike lanes should be implemented along Winchester Street connecting the infrastructure west of the intersection with State Street.

4.5.3 PEDESTRIAN RECOMMENDATIONS

Pedestrian infrastructure throughout the study area needs to be improved, particularly along State Street. The sidewalks along State Street should include a landscaped buffer at least 5 feet wide from the busy roadway to enhance the feeling of safety for users. Furthermore, drainage issues should be repaired at the intersections along State Street. Many crosswalks enter into a pool of leftover storm water making it very difficult for pedestrians to safely cross. Additionally, a Pedestrian Hybrid Beacon (PHB) or a full traffic signal should be implemented at State Street and Creek Drive. Currently, residents around Grant Park must divert up to a third of a mile through either the signal at State Street and Winchester Street or State Street and 5900 South to reach the edge of Fashion Place Mall parking lot. Installing a PHB signal or a full traffic signal would give residents directly west of Fashion Place Mall a convenient, direct and likely safer access point to the mall. It should be noted that under UDOT's current guidelines, a new signal would closer than the allowable standard of 2,650 feet between lights to both



existing State Street signals at 6100 South, as well as at Winchester Street. The current method for determining an appropriate exception for a PHB signal along a roadway such as State Street requires a study of the number of jaywalking pedestrians in a given period of time. Jaywalking across this roadway is unsafe and alternative thresholds should be explored with UDOT. While exceptions in signal spacing are not common, an example currently exists along State Street at Williams Street in Salt Lake City, as shown below.



Figure 4.12 Example of Pedestrian Hybrid Beacon (PHB) signal on State Street.

Outside of the State Street corridor, a sidewalk and crosswalk should be installed on the northern end of the TRAX station westward across Cottonwood Street. Public input indicates that this pattern is already a common route for pedestrians originating north of I-215.

Additionally, pedestrian infrastructure needs to be improved throughout Fashion Place Mall parking lot. Currently, no sidewalks or pathways exist connecting the City sidewalks to the mall entrances. This causes an unclear, uncomfortable, and unattractive experience for mall patrons traveling by foot. Providing a clear and welcoming walkway for pedestrians will increase comfort and attractiveness of walking to the mall.

Finally, general sidewalk conditions throughout the study area need to be improved. Sidewalks should be level, clear of vegetation and debris, at least 6 feet wide where possible, and should include a buffer between the walkway and the roadway. This is particularly important on Winchester Street and Cottonwood street to provide comfortable north-south and east-west access to the study area for TRAX riders who often begin and end their trip on foot.

4.5.4 CONNECTIONS TO SURROUNDING DEVELOPMENT

In order to create a true network of mobility, infrastructure must consistently connect destinations to destinations. All vehicular, transit, pedestrian, and bicycle infrastructure implemented should be designed with connectivity in mind, both inside and outside the study area. The planned extension of the Porter Rockwell Trail will be a key connection to other communities, requiring a robust bicycle and pedestrian network in the study area to encourage trail users to stop in the Fashion Place West neighborhood. Other key destinations to connect to include Murray City Center and the upcoming development at Winchester Street and 700 West.

4.5.5 PARKING LOT PEDESTRIAN IMPROVEMENTS

Currently, Fashion Place Mall parking lots feature no bicycle or pedestrian improvements. These connections are vital for the first/last mile portion of any mall trip. By providing wide walkways and bikeways from mall entrances directly to the adjacent roadways and transit stops, non-motorist users will feel more comfortable and encouraged to travel to/from the mall utilizing alternative modes of transportation.



66 FASHION PLACE WEST SMALL AREA PLAN



Figure 4.13 Future improvements to the transit system within the Fashion Place West neighborhood would increase ridership and improve the rider experience and quality.

What is Shared Micromobility?

Shared Micromobility encompasses all shared-use fleets of small, fully or partially human-powered vehicles such as bikes, e-bikes, and e-scooters.



Station-based bike share
(including e-bikes)



Dockless bike share
(including e-bikes)



Scooter share

4.5.6 MICROMOBILITY

Micromobility is an emerging mode of transportation bringing publicly or privately operated e-scooters, bikes (including bikeshare), and other shared mobile lightweight devices to a community. Micromobility can offer a convenient last-mile connection between the TRAX station and Fashion Place Mall, especially once complete cycling infrastructure is implemented along Winchester Street.

To avoid clashes with future installations of micromobility, Murray City should develop policies around micromobility before companies enter the market. Policies should address topics such as fleet caps, service area and distribution, fees and pricing, equity, maintenance and safety, data sharing, community engagement, and parking.

Fleets should be capped by a revocable permit system based on a dynamic rate such as number of residents or operational performance. A cost analysis should be conducted to determine the true costs of administering the program. Dynamic pricing offers the most potential for revenue, and parking fees can generate extra cash while encouraging riders to comply with parking policies. Implementing



Figure 4.14 Micromobility such as scooter and bike share programs offer communities a low cost/ high value option to increase connectivity where it is currently lacking.

pricing policies can help prevent abrupt price changes from operators.

Maintenance and safety guidelines should outline collection of incident reports and inspection requirements. Data sharing is important for infrastructure planning and the permitting process. It is recommended to share data in either the General Bikeshare Feed Specification (GBFS) or Mobility Data Specification (MDS) formats, depending on the preferred level of detail and user privacy.

The City should also develop communication and education policies to ensure operators are engaging with the community in an equitable manner to minimize the burden of micromobility adoption on the City.

Finally, parking policies should detail strategies to enforce parking rules, compliance with ADA requirements, and no parking at loading zones. Infrastructure for micromobility includes parking zones and riding infrastructure. Dedicated parking zones should be located near (but not block) entrances to popular area destinations, such as the TRAX station, Fashion Place Mall, and Grant Park. These parking locations should be easily accessible from riding infrastructure. Bicycle infrastructure should



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be used as micromobility infrastructure to discourage riding on the sidewalk where possible in order to avoid conflict with pedestrians and maintain an ADA-friendly environment. Improving bicycle infrastructure therefore improves micromobility infrastructure. Ideally, bike lanes should include a buffer to physically restrict conflict with motor vehicles. This buffer can also provide space for micromobility parking if no extra sidewalk space is available. Other enhancements can improve the non-motorized user experience as outlined in the figure below. Any of these enhancements would be particularly useful along Winchester Street which connects two of the area's destinations—the TRAX station and Fashion Place Mall—along with the continuation of the bike lanes between Jefferson Street and Malstrom Lane as a particularly helpful improvement.

4.5.7 TRAX STATION IMPROVEMENTS

Signage and information about the bike, pedestrian, and transit options could be installed to assist riders in accessing Fashion Place Mall from the TRAX station.

The 209 bus in particular should be utilized as a circulator bus between TRAX and Fashion Place Mall. Furthermore, the City in partnership with UTA should consider redesigning the crossing arms so as not to block access to the sidewalk causing pedestrians to back up onto the tracks. The current crossing configuration also prohibits individuals with mobility needs from crossing the TRAX rail. The following images show an example of improved crossing arm configuration at Central Pointe Station and 2100 South in Salt Lake City. As illustrated, the sidewalk is rerouted to ensure no conflict between the ADA and pedestrian route with the crossing arm or the sidewalk.



Figure 4.15 Example of improved crossing arm configuration at Central Pointe Station and 2100 South. The sidewalk is rerouted to ensure no conflict between pedestrian route and crossing arm.



Figure 4.16 Current crossing arm configuration at the TRAX Station and Winchester Street. Note the conflict of the ADA truncated plate.

Other improvements that should be considered at the Fashion Place West TRAX station include:

- Implementing a crosswalk connecting TRAX to Cottonwood Street
- Creating a connection from TRAX to new sidewalk on the west side of Cottonwood Street
- Including landscape buffers, at least five feet wide from busy roadways adjacent to station
- Widen sidewalks adjacent to the station to improve pedestrian comfort
- Ensure consistency in bike lanes to/from the station
- Improve existing sidewalk conditions along Winchester Street



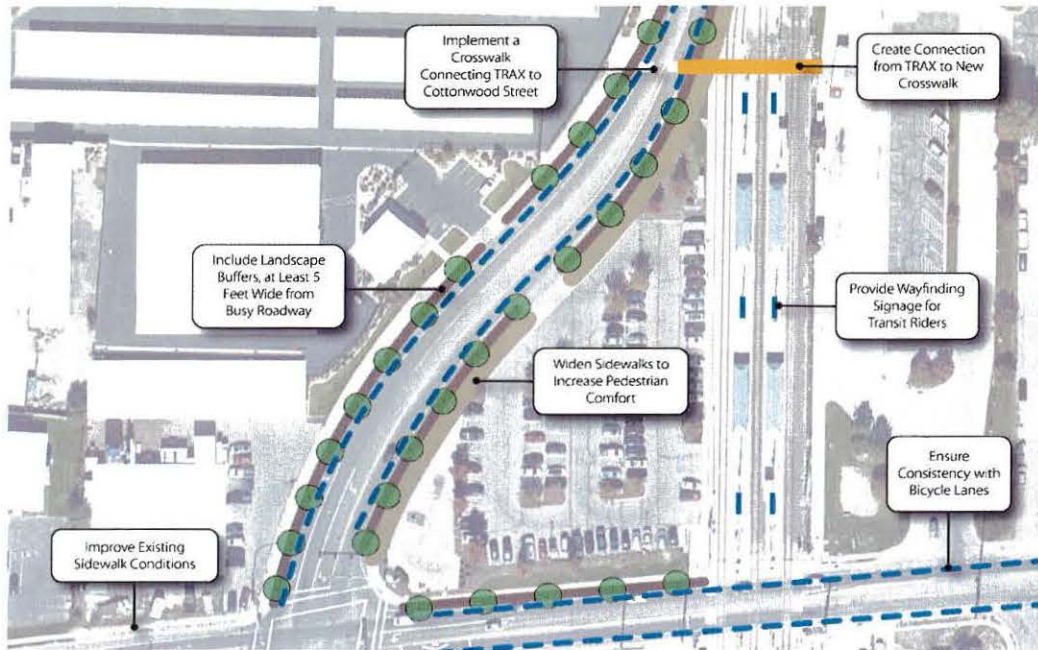


Figure 4.17 The existing access to the Fashion Place West TRAX station is inadequate. Future improvements to the station should include improving access from Cottonwood Street as well as across Winchester Street.



Figure 4.18 Location where sidewalk and crosswalk are needed on Cottonwood Street with access to the TRAX station.

4.5.8 BUS STOP IMPROVEMENTS

Consider rerouting the Route 209 bus route to stop directly in front of a mall entrance. The current mid-block stop location on Winchester Street forces riders to take a long route to reach the mall without proper sidewalk infrastructure through the parking lots. A direct route for riders improves pedestrian safety by decreasing chances of vehicular conflicts.

According to the UTA Bus Stop Master Plan, bus stops along Winchester Street and State Street can also be improved to feature additional amenities depending on frequency and ridership. The 209 bus currently runs on 15-minute headways, and the Route 201 bus and the Route 62 bus both operate on headways that are greater than 15 minutes. In the case that ridership does not meet the desired threshold for a

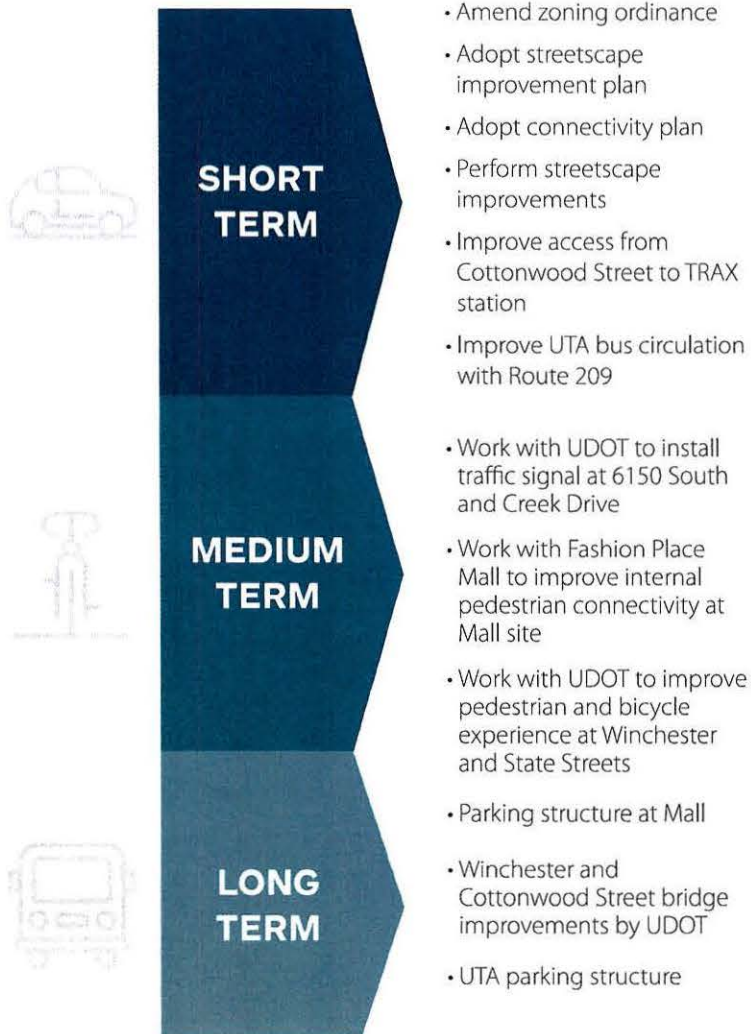
station improvement, Murray City can partner with UTA to fund the implementation of the amenity. Increasing amenities at bus stops makes the system more attractive and can increase comfort and safety of users.



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4.7 CONNECTIVITY SUMMARY AND IMPLEMENTATION STRATEGY

The Connectivity section of the Small Area Plan considers current transportation and mobility in the study area, planned improvements, and best practices. These factors were used to identify and outline clear priorities and policy amendments to improve future transportation within the study area.



4.7.1. CONNECTIVITY PRIORITIES

1. Improve overall active transportation connectivity between residential neighborhoods, TRAX station, and Fashion Place Mall
2. Modify UTA Bus route 209 to be a circulator between the TRAX station and Fashion Place Mall
3. Develop parking strategy
4. Adopt streetscape improvement plan to ensure future connectivity in key areas:
 - (a) Winchester
 - (b) Cottonwood
 - (c) Intersections
 - (d) Fashion Place Mall access

4.7.2. POLICY UPDATES AND LAND USE AMENDMENTS

1. Create new Fashion Place West zone district modeled off of existing TOD zone with the following revisions:
 - (a) Parking
 - (i) Include shared parking provision
 - (ii) Reduce residential requirements contingent upon proximity to TRAX station, shared parking calculation, etc.
 - (iii) Implement parking maximums
 - (b) Reduce front yard setback from 15 feet to 25 feet, to 0 feet
 - (c) Implement maximum setback requirements
 - (d) Decrease open space percentage requirements from 20 percent to 10 percent
 - (e) Ground floor activation, requirements, and language
2. Re-zone areas within the study area per recommendations of the General Plan



5 DESIGN GUIDELINES



5 DESIGN GUIDELINES

5.1 DESIGN GUIDELINES INTENT

5.1.1 DESIGN VISION

The Fashion Place West study area is located in the southwest corner of the City of Murray. The scale of development ranges from single-family and small scale multifamily to single-story industrial, to Fashion Place Mall. The vision for new development is to create a walkable, transit-oriented neighborhood. This type of development in the study area will foster small scale infill projects as well as allow for context sensitive larger scale mixed-use projects that will provide a wide range of housing choices, and an incubator for commercial spaces that serve the neighborhood.

5.1.2 PURPOSE

The purpose of this section is to serve as a design guide for development in the Fashion Place West study area. The guidelines in this section are directly related to achieving the key design objectives for the district.

5.1.2.1 KEY DESIGN OBJECTIVES:

- Context-sensitive solutions for infill development projects in the study area
- Emphasis on mixed-use, pedestrian-oriented developments and streetscapes that promote active use of the streets, sidewalks and public spaces
- Ensure availability of a range of transportation choices including; walking, bicycling, transit, and motor vehicles
- Apply principles of long-term economic, social, and environmental sustainability in the design of infrastructure, site, and building development

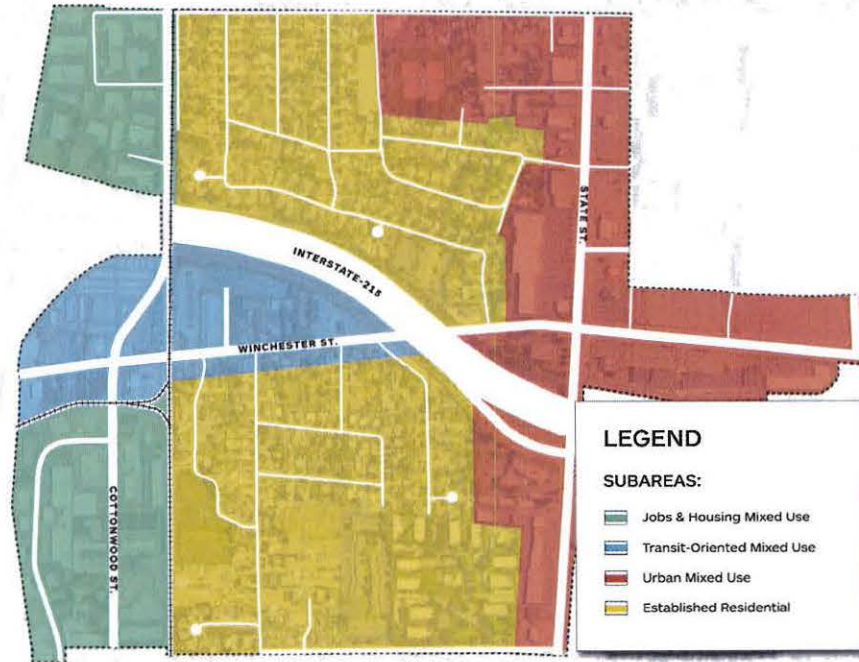


Figure 5.0 Map of future subarea recommendations in the Fashion Place West neighborhood. Design Guidelines will be applied in manner sensitive to the context of the neighborhood and each subarea.

- Provide the Fashion Place West study area with a distinct character

Each guideline includes an intent statement that explains the purpose of the directive to achieve one or more of these overall design objectives. In many cases, alternative solutions to the guidelines may be suggested by the developer, designer, or applicant, as long the solution meets the intent statement.



5.2 DESIGN GUIDELINES STRUCTURE

Design Guidelines for the Fashion Place West study area contain two sections—Site Design and Building Design. Site Design focuses on how the parcel or piece of property is designed, while Building Design is the concept of elements of the building itself. Each section includes the following guidelines:

5.2.1. SITE DESIGN

- **Building Placement** addresses setbacks for buildings, landscaping, and accessory units.
- **Parking Lot Design and Landscaping** guides applicants with the location of parking lots on a site, as well as the use of landscaping to screen parking.
- **Lighting** suggests lighting types and locations for the public realm such as sidewalks, parking lots, and public space.
- **Pedestrian Connections** recommends types such as crosswalks, walking paths, and sidewalks, as well as appropriate features.
- **Corner Sites** explains the importance of corner buildings to a streetscape, and how they should be situated on the lot.
- **Treatment of Outdoor Storage and Equipment** establishes location and screening guidelines for items such as dumpsters and mechanical equipment.
- **Accessory Dwelling Unit (ADU)** guidelines determine the location of the ADU as well as the size in comparison to the size of the site.

5.2.2. BUILDING DESIGN

- **Ground Floor Details** specify what types of features the ground floor of street-facing buildings should have.
- **Ground Floor Transparency** recommends various percentages of ground floor buildings that should be windows, doors, or otherwise



Figure 5.1 Public space with the appropriate location of amenities and landscaping attract people and invite them to stay longer.

transparent.

- **Prominent Entrances** describes the design of building entrances so that they stand out and create an inviting space.
- **Treatment of Blank Walls** advises that blank walls fronting the street or sidewalk are not desirable, and if needed, should be treated with landscape or art features, as examples.
- **Articulation** refers to the variation in materials, height, and general shape that buildings should be designed with.
- **Transition of Scale** addresses the need for new development to consider existing development in terms of height and density. Development adjacent to single-family homes should consider the scale to which the development is near.
- **Sign Design** establishes guidelines for various types of signs in different situation, in terms of materials, size, and location.

5 DESIGN GUIDELINES

5.3 SITE DESIGN

5.3.1. BUILDING PLACEMENT

To support and encourage pedestrian comfort, convenience and activity, buildings should create a sense of enclosure within the street corridor, by establishing a direct relationship between buildings and sidewalks.

- Commercial and mixed-use buildings should be built along the back of the sidewalk on all Type I and Type II streets, adjacent to any public plaza, courtyard, seating area, or other space intended for public use.
- Multi-family buildings may include a modest front setback (3-7 feet) to create a transition area between the public and private space. Street wall reinforcing elements are encouraged to occupy in this setback, such as:
 - Porches and stoops
 - Landscaping
 - 3 foot maximum fence height
- Single-family and lower density residential structures on Type III streets may have a front setback of 20-25 feet (or average of two adjacent properties) to maintain the existing character.
- Detached accessory residential structures, such as accessory dwelling units or detached garages should be set 0-10 feet from the back lot line.



Figure 5.2 Commercial buildings within the Fashion Place West study area should be built along front property lines and sidewalks to encourage pedestrian activity and a sense of enclosure, whereby creating a sense of place.

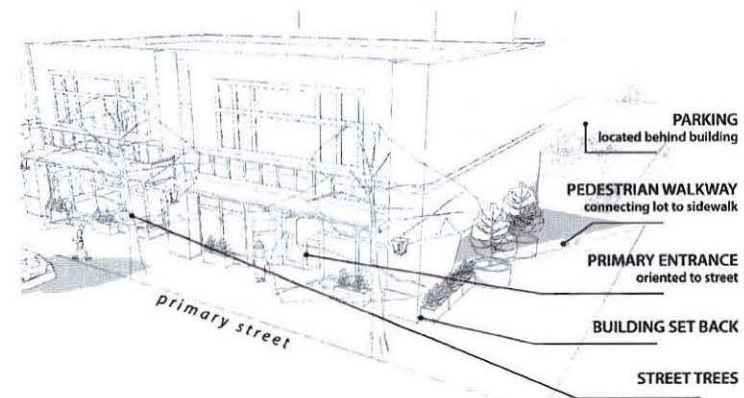


Figure 5.3 The diagram above illustrates the ideal placement of buildings so to maximize the lot as well as addressing the street.



5.3.2. PARKING LOT SCREENING AND LANDSCAPING

To diminish the amount of impervious surface and visual impact of parked cars, parking lots should be buffered from other uses, to offer shade to otherwise bare paved areas, and to visually soften expanses of parking.

- Parking lots should integrate main drive aisles to appear more like streets, and should include sidewalks, landscaping including trees, and pedestrian scaled lighting.
- Masonry walls and other structural screening features should be used only for corner accents or where screening of headlights is necessary, and should not be used as a substitute for landscaping.
- Parking aisles should be organized to create a central pedestrian



Figure 5.4 Parking lots should not be located along the primary frontage but rather along the secondary or at the rear of a building. Parking lots should be screened from sidewalks and streetscape but still remain comfortable for access by pedestrians.

access to building entries. Outer parking aisles may incorporate drainage swales between parking rows.

- Trees should be distributed throughout the parking area to provide ample shading and visually soften the parking area, roughly 1 tree for every 8 parking stalls. Adjacent to single-family residential uses, 1 tree for every 5 stalls should be planted.
- In addition to trees, shrubs and perennials should be planted as understory at the base of tree planting beds.
- Grouping trees may be allowed to accommodate natural features, so long as the equivalent number of trees are planted and so long as the grouping is within the parking area. Curbs or other methods of preventing vehicles from damaging the trees should be installed.
- Retaining existing trees in parking lots is encouraged.

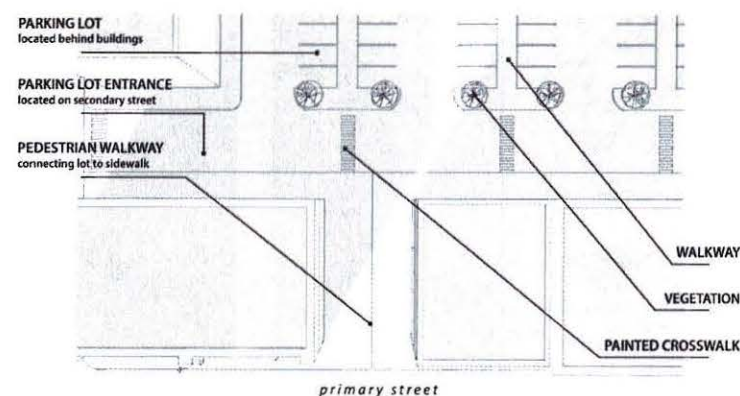


Figure 5.5 Parking lots should be landscaped to soften the hardscape to offer a more aesthetically pleasing environment. Landscaping should break up parking aisles as well as provide paths for pedestrians.



5 DESIGN GUIDELINES

5.3.3. LIGHTING

Lighting should ensure a contribution to the character and safety of the streetscape and public spaces, but not disturb adjacent developments and residences.

- Use City-approved standardized fixtures for sidewalk lighting. Fixtures should be consistent with adopted light fixture for the study area.
- Lighting elements throughout and surrounding the site should be complementary, including pedestrian pathway, accent, parking lot lighting, lighting of adjacent developments, and the public right-of-way.
- All lighting should be shielded from the sky and adjacent properties and structures, either through exterior full cut-off shields or through optics within the fixture.
- Lighting used in parking lots should not exceed a maximum of 30 feet in height. Pedestrian-scale lighting should be a maximum of 16 feet in height.
- Parking lot lighting should be appropriate to create adequate visibility at night and evenly distributed to increase security.



Figure 5.6 Street lighting and lighting within public spaces should adhere to character and identity established for the Fashion Place West study area and be at a pedestrian scale. Additionally, this lighting should be down cast lighting to minimize light spillage.



5.3.4. PEDESTRIAN CONNECTIONS

Safe pedestrian passage should be provided through any large blocks or parking lots to provide convenient and direct pedestrian connections, and to provide neighborhood-scale open space.

- Formalized mid-block pedestrian corridors or connections between public rights-of-way through the blocks and redevelopment sites on 300-350 foot intervals are highly encouraged, with at least one through-block connection for any block face longer than 600 feet.
- All non-motorized corridors and connections should include:
 - A 5 foot minimum building setback on either side of the connection, which could include landscaping, lighting, and other pedestrian amenities,
 - A 6 foot 7 inch minimum walkway, and
 - Appropriately scaled pedestrian lighting.
- Walkways should be paved with a differentiated pavement surface treatment to alert drivers to the pedestrian right-of-way and potential presence of pedestrians. Speed tables may be installed as appropriate to further calm vehicular traffic.
- Alternate building entrances are encouraged to be located on pedestrian connections and alley ways to provide a building face along such pathways.
- Access from the street should include wayfinding signage to notify pedestrians of the facility.



Figure 5.7 The Fashion Place West study area lacks infrastructure for pedestrians. Being bisected by two freeways limits the walkability capacity in the neighborhood. Improving pedestrian connections between commercial and residential developments can greatly increase walkability in nearby areas.



Figure 5.8 Midblock pedestrian crossings are lacking in the Fashion Place West study area. Crosswalks exist mainly at traffic signals. Future streetscape improvements should incorporate safe and highly visible midblock crosswalks along Winchester Street and Cottonwood Street.

5.3.5. CORNER SITES

Corner sites and buildings provide an enhanced pedestrian experience by creating visual gateways, public plazas, courtyards and other gathering spaces.

- Key intersections should be marked with setbacks that allow for public spaces. Rather than meeting the corner, new buildings should incorporate forecourts, plazas, or gardens that welcome the public and offer a dramatic statement at the corner.
- Major entrances should also be located at the corners and highlighted by elements such as higher or more expressive canopies, higher bays, larger windows and doors, projections, different window designs, or other physical features.
- If potential views to noteworthy natural features and points of interest exist, (either nearby or in the distance exist from the development site), entrances and publicly accessible open spaces should be located and oriented to take advantage of this view.



Figure 5.9 Corner sites should be developed to encourage interaction with pedestrians by allowing and requiring specific setbacks that allow for plazas and inviting entrances.

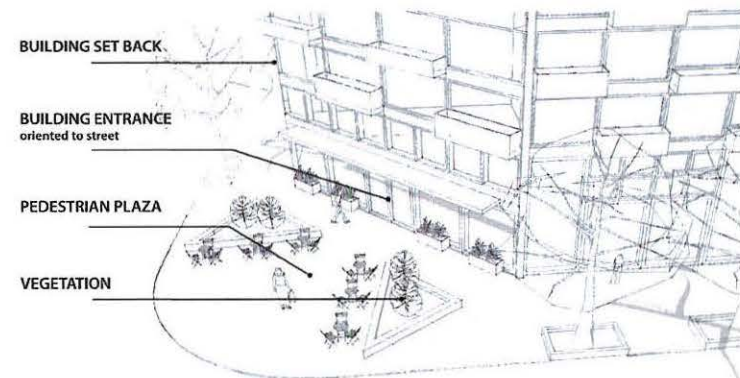


Figure 5.10 Principal buildings on corner sites should have a grand entrance from the sidewalk and offer a public space.



5.3.6. ACCESSORY DWELLING UNITS (ADU)

The City recognizes that accessory dwelling units (ADUs) in single-family residential zones can be an important tool in the overall housing plan for the City. The purposes of the ADU recommendations are to:

- Allow opportunities for property owners to provide social or personal support for family members where independent living is desirable;
- Provide for affordable housing opportunities;
- Make housing units available to moderate income households that might otherwise have difficulty finding homes within the City;
- Provide opportunities for additional income to offset rising housing costs;
- Develop housing units in single-family neighborhoods that are appropriate for people at a variety of stages in the life cycle; and
- Preserve the character of single-family neighborhoods by providing



Figure 5.11 Accessory Dwelling Units (ADUs) can be designed to be stand alone dwelling units that are completely separate from the primary dwelling unit.



Figure 5.12 Accessory Dwelling Units (ADUs) can be constructed as units attached to the principal dwelling unit, but have their own private entrance and yard.

standards governing development of ADUs. (Ord. 09-23 § 2)

5.3.6.1. EXISTING MURRAY CITY ADU STANDARDS

1. Accessory dwelling units (ADUs) are allowed within single-family zones in the City, on lots that are a minimum of 12,000 square feet.
2. The property owner, must occupy either the principal unit or the ADU, but not both, as their permanent residence and at no time receive rent for the owner occupied unit.
3. Only one ADU may be created per lot or property in single-family zones.
4. A separate entrance to the ADU shall not be allowed on the front or corner lot side yard. Any separate entrance shall be located to the side or rear of the principal residence.



5 DESIGN GUIDELINES



Figure 5.13 Accessory Dwelling Units (ADUs) constructed over a single-family garage is an example of a way to provide life-cycle housing in the study area. This dwelling type is encouraged also as a way to increase density.

5. The total area of an attached ADU shall be less than 40 percent of the square footage of the primary residence and in no case shall exceed 1,000 square feet.
6. ADUs shall not contain more than two (2) bedrooms.
7. ADUs shall be occupied by no more than two (2) related or unrelated adults and their children.
8. Two (2) off street parking spaces shall be provided.
9. Detached ADUs shall not be located in a front or corner lot side yard and shall meet the same setbacks as required for the primary residence in the zone.
10. A detached ADU shall not exceed the allowable lot or rear yard coverage standard for the underlying zone or encroach into the required setbacks.
11. Detached ADUs shall be compatible with the exterior color and materials of the principal dwelling.

12. The maximum height for detached ADUs is limited to one story and to 20 feet or the height of the principal structure, whichever is less.
13. The total floor area of a detached structure containing an ADU shall not exceed 1,000 square feet.
14. Conversion of existing accessory buildings (such as detached garages) may only occur where the existing accessory building meets the setback requirements for a primary residence in the zone and meets the applicable building code.
15. The planning commission may place other appropriate or more stringent conditions deemed necessary in approving ADUs to protect the public safety, welfare and single-family character of the neighborhood. (Ord. 09-23 § 2)

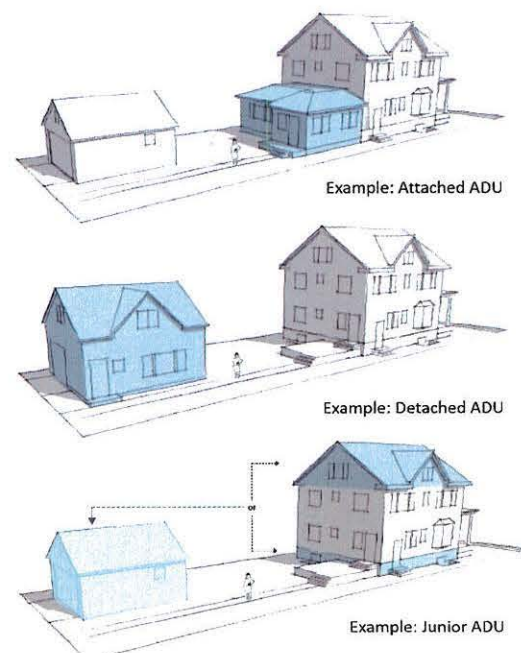


Figure 5.14 Various types of Accessory Dwelling Units that are permitted in most residential areas of Murray.



5.3.7. TREATMENT OF OUTDOOR STORAGE, AND EQUIPMENT

Enclosures and screening should be used to reduce the visual impacts of storage, trash, and service areas.

- The total area allowed for outdoor storage or merchandise display should be less than twenty-five percent (25 percent) of the total gross square footage of building occupied by the use; provided, however, that such area may exceed twenty-five (25 percent) percent if it is fenced and screened. This standard does not apply to temporary uses such as material storage during construction or street vendors.
- Any storage, service and truck loading areas, utility structures, storage tanks, elevator and mechanical equipment on the ground or roof should be screened from public view.
- Trash collection and outdoor storage tank areas should be located within enclosed structures constructed of similar materials and quality of the associated buildings, with a gate that can be closed. The gate should be similarly treated or located in an area not visible from the street.



Figure 5.15 Waste containers and dumpsters should be shielded from view using permanent materials. This screening should decrease the visibility and visual impacts of these types of areas.



Figure 5.16 Buildings that require utilities or mechanical equipment to be positioned on the roof, should be screened from public view using materials complementary to the building facade and the surrounding neighborhood.

5 DESIGN GUIDELINES

5.4 BUILDING DESIGN

5.4.1. GROUND FLOOR DETAILS

Ground Floor Details reinforce the character of the streetscape and provide pedestrian amenities.

- The first floor level should be at least 12 feet in height as measured from the floor to the interior ceiling to provide for a generous space for retailing, services, and restaurant functions.
- Facades of commercial and mixed-use buildings that face the street should be designed to be pedestrian friendly through the inclusion of at least three of the following elements:
 - Kick plates for storefront windows
 - Projecting window sills
 - Pedestrian-scale signage
 - Exterior lighting sconces
 - Containers for seasonal plantings
 - Window box planters
 - Benches and seat walls along 30 percent of the length of the façade
 - Decorative paving in the sidewalk
 - Decorative brick, tile or stone work on the ground floor façade
 - A feature not on the list that meets the intent of the guideline.



Figure 5.17 New construction of residential and commercial buildings should be designed with ground floors that address the street and are built on a pedestrian scale. Elements include landscaping, scaled windows and entrances as well as furnishings.

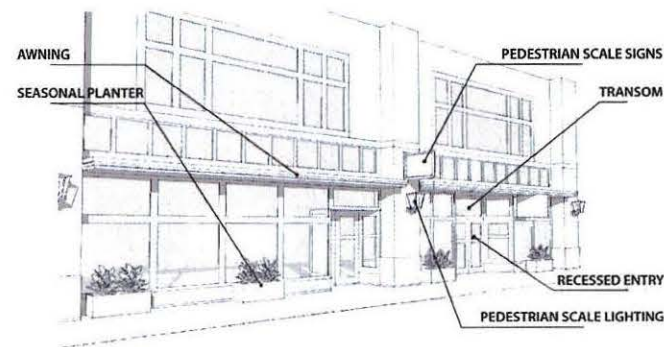


Figure 5.18 The diagram above illustrates specific elements that should be incorporated into the design of new buildings within the Fashion Place West area. Collectively, these elements create a sense of place and create an aesthetically pleasing environment for the pedestrian.



5.4.2. GROUND FLOOR TRANSPARENCY

Ground Floor Transparency should utilize building façades to provide safe and comfortable waiting areas for transit and provide visual connections between activities inside and out.

- All commercial buildings should include windows with clear vision glass on at least 50 percent of the area between two and twelve feet above grade for all ground floor building facades that are visible from an adjacent street.
- Street-facing, ground-floor facades of commercial and mixed-use buildings should incorporate generous amounts of glass in storefront-like windows. Amounts of clear, transparent glass should meet or exceed the following:
 - 60 percent along primary streets
 - 50 percent along secondary streets



Figure 5.19 Whenever possible, the ground floor of buildings along primary frontages should have facades that interact with the pedestrian and the street it sits on. This should be accomplished with the use of windows and clear vision glass that allow for 60 percent transparency along primary streets and 50 percent transparency along secondary streets.

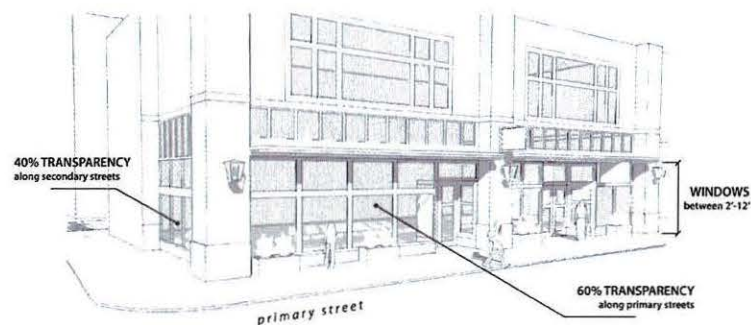


Figure 5.20 The diagram above visually illustrates transparency requirements. Height, width, and location of windows largely contributes to appropriate percentages of transparency.

5 DESIGN GUIDELINES

5.4.3. PROMINENT ENTRANCES

Building entrances should be designed to readily inform people of their access and use.

- The primary (front) building façade and main entry of nonresidential buildings should be well-marked, articulated and oriented and facing the primary public street.
- Consider placing the main building entrance at a street corner.
- Entries should be lighted and protected from weather.
- Entries facing public streets should be made visually prominent and receive architectural emphasis. A variety of techniques to accomplish this include:
 - Recessed entries
 - Projecting entries
 - Elevated entries with stairways for residential uses
 - Entry-related cover or roof line articulation (such as canopy articulation; parapet-roof articulation)
 - Arched entries
 - Decorative lintels or molding above doorways
 - Landscape treatment and emphasis
 - Surface treatment (such as paver or tiles)
 - Entry courtyard
 - Transom windows
 - Signage
 - Other techniques as appropriate



Figure 5.21 Entrances to buildings within the Fashion Place West study area should be well-marked and oriented toward the primary frontage. Signage should be implemented on a human scale and facades should include a well-articulated entrance.



Figure 5.22 Corner buildings should be constructed as the main building of new development, and should display a prominent entrance on the corner. Entrances can include decorative awnings, stone facade treatments, and stairs that are prominent and address the street.



5.4.4. TREATMENT OF BLANK WALLS

Blank Wall Treatments ensure that buildings do not display blank, unattractive walls to the abutting street or public areas.

- Use vegetation, such as trees, shrubs, ground cover or vines adjacent to the wall surface. Green walls are strongly encouraged to manage stormwater runoff.
- The use of façade articulation such as expressing the structural bays of the building with pilasters or other detailing should be used to help animate an otherwise blank area of wall.
- Use artwork, such as bas-relief sculpture, murals or trellis structures. Use seating areas with special paving.
- Use architectural detailing, reveals, and contrasting materials.



Figure 5.23 In cases where blank walls cannot be avoided, or are on secondary frontages, treatments should be applied to these surfaces. Post-construction applications can include landscaping such as a trellis structure, shown above.



Figure 5.24 Newly constructed buildings that contain blank walls should include architectural detailing, articulation, or artwork, like the building above displays.

5 DESIGN GUIDELINES

5.4.5. ARTICULATION

Building Articulation should reduce the apparent bulk and maintain a human scale proportion in multi-story or large buildings.

- Buildings should incorporate varied articulation on all sides. The street-facing side(s) should receive the greatest amount of attention with respect to richness of forms, details, materials, and craft.
- Elements such as sun shades, terraces, and rain water harvesting features can be used to compose and articulate the building's façade.
- Varied frontages. Building frontages should be divided into relatively small units with storefronts, bays, recesses, offsets, balconies, a varied and rich color palette, and other elements to avoid long, monolithic facades.



Figure 5.25 A key component of good urban design and creating a desirable place to visit is to construct buildings whose facades offer varied materials and articulation. This articulation should vary on all sides that have street frontage.



Figure 5.26 New construction should look to historic buildings for inspiration regarding facade articulation and materials. Historic buildings, like the ones above, often are of timeless architecture styles, and succeed at creating inviting destinations.



5.4.6. TRANSITION OF SCALE

Transition of Scale can be achieved by incorporating additional features into higher density development when located adjacent to properties with lower density single-family use to enhance the compatibility between uses.

- Multi-family and mixed-use development located adjacent to existing single-family residential should incorporate three or more of the following architectural features:

- Recessed entry
- Dormers
- Higher quality material
- Pitched roof forms

- Upper level balconies
- Upper level step backs
- Gables
- Window patterns
- Flat, blank walls should not be visible from the street or common areas.
- Tree retention or additional vegetative screening along neighboring properties is encouraged.
- Building Height



Figure 5.27 The practice of using transition of scale helps municipalities include a mix of uses in a single area while remaining sensitive to lower density uses.

5 DESIGN GUIDELINES

5.4.7. SIGN DESIGN

5.4.7.1 PEDESTRIAN ORIENTATION

Signs will complement and strengthen the pedestrian realm

- Pedestrian signs include projecting signs (blade signs), window signs (painted on glass or hung behind glass), logo signs (symbols, shapes), wall signs over entrance, and monument signs.

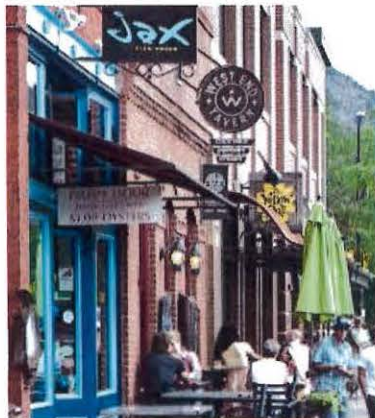


Figure 5.28 Pedestrian scale signage.

5.4.7.2. CREATIVITY AND UNIQUE EXPRESSION

Signage should be interesting, creative, and unique approached to the design of signs.

- The design of signs are encouraged to use color, graphics, and handcrafted elements.



Figure 5.29 Creative signage with a design unique to the business.

5.4.7.3. INTEGRATION WITH ARCHITECTURE

Signage should be part of the overall design approach to a project and not added as an afterthought element.

- The design of buildings and sites shall identify location and sizes for future signs. As tenants install signs, it is expected that such signs shall be in conformance with an overall sign program that allows for advertising which fits the architectural character, proportions, and details of the development.



Figure 5.30 Signage integrated into a brick building's architecture.

5.4.7.4 COORDINATED WAYFINDING

Public signage should reflect and enhance the character of the area.

- The City should implement a coordinated neighborhood identity program in the design of wayfinding signage.



Figure 5.31 Wayfinding signage should have a theme and be consistent throughout the area.



6 APPENDIX



6 APPENDIX

6.1 PUBLIC ENGAGEMENT

6.1.1 OPEN HOUSE

On February 12, 2020 Murray City along with the consultant team, held a public open house at the Clark Cushing Senior Center, located within the northern portion of the study area. The objective of the open house was to educate the public about existing conditions in the area and the goals of the Fashion Place West Small Area Plan, as well as to gain feedback and insight from the participants about many key components. A series of ten boards and individual questionnaires were used to inform, and gather feedback.

Among the approximately 35 individuals that participated, half said that they lived in the study area, and the other half were commuters or Murray residents. Most participants had positive reactions to the planning process, while also expressing their desire for better connectivity in the area, which aligns well with the City's vision for the Small Area Plan.

The most frequently asked question from participants was, "What development is being proposed?" Staff and the consultant team educated residents about the need for a long range plan for this area, even though there was no development proposed, or on the horizon.

When participants were asked which of Murray's five key initiatives (established in the General Plan) seem most related to this neighborhood, many felt that Livable and Vibrant Neighborhoods and Multi-Modality were most applicable.

The questionnaire asked respondents about their impressions of the study area and what they have experienced, and would like to see changed.



Approximately 35 individuals participated in the Open House at the Cushing Senior Center.

When asked what types of destinations they wished were in the neighborhood, the most common answers were:

- Public space/parks
- Dining
- Grocery/market

When asked what type of housing they would occupy in the next phase of life, the majority of respondents answered:

- Single-Family Home
- Townhome
- Accessory Dwelling Unit (ADU)



6.1.2 SURVEY

While originally scheduled to hold a second open house, due to safety concerns related to the COVID-19 pandemic, City staff and the consultant team conducted an online survey from May 20th through June 20th. Residents, commuters, shoppers, and other interested parties were invited to participate by answering a series of 18 questions. The survey was advertised through social media channels and received over 130 responses.

The goal of the survey was to gauge respondents' understanding of the components of the Small Area Plan, and aptitude for more specific recommendations dealing with connectivity expansion, housing options, and design guidelines.

A number of survey questions stood out as good indicators of concerns that residents have and what they would like to see more of. Those included:

- What four words would you use to describe the attributes of the Fashion Place West neighborhood?
- What is your primary destination when you visit the neighborhood?
- What do you see as challenges facing the neighborhood?
- What types of housing do you wish were available?
- What housing issues do you feel exist in the neighborhood?

A majority of respondents appreciate the convenient and central location of the Fashion Place West neighborhood. When asked questions regarding access for bicycles and pedestrians, many respondents expressed desire for better sidewalks and more bicycle lanes. A common concern throughout the survey responses was around traffic in the Fashion Place West neighborhood, and the area becoming busier. Because of this concern, staff and the consultant team felt it important to address the effects of future growth on traffic, as well as ways to mitigate current and future traffic increases.



Question 4: What four words would you use to describe the attributes of the Fashion Place West neighborhood?

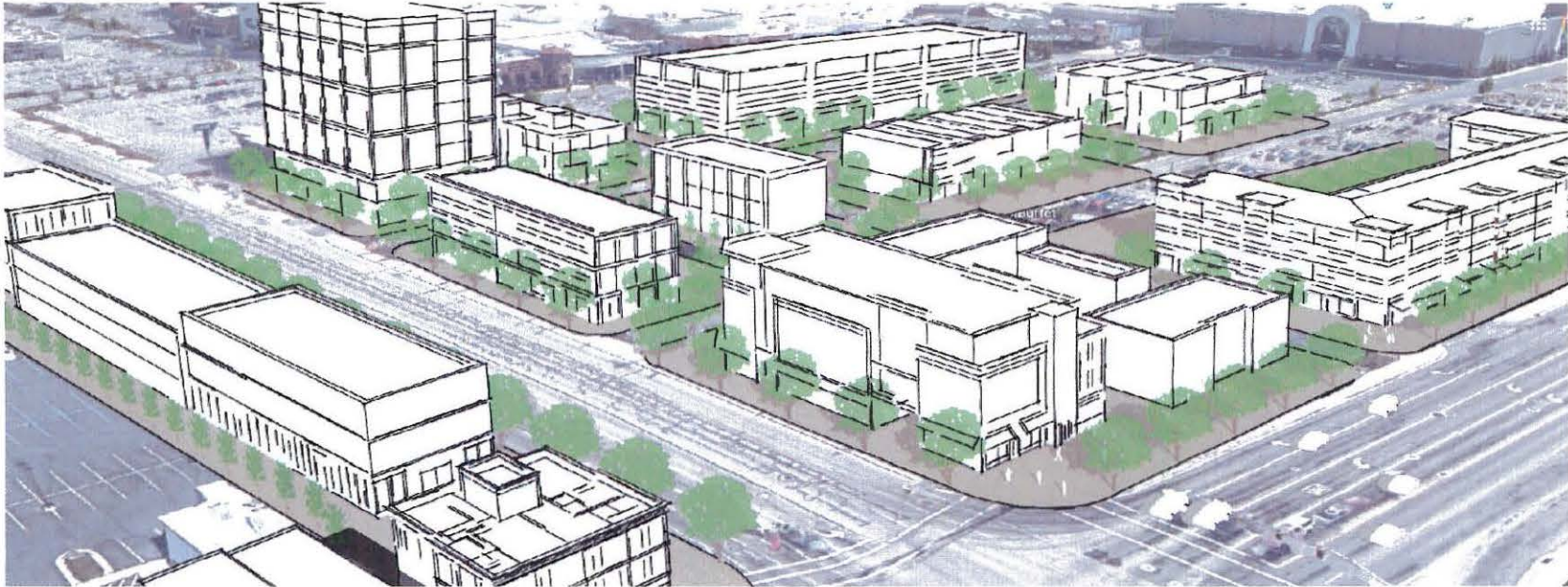
When respondents were asked about the types of housing that they wished were in the neighborhood, many felt that mid-density housing types such as cottage clusters, ADUs, and duplex/triplex units would make a good addition. When asked about housing issues they felt the study area faced, many respondents expressed the need for more housing affordability, and construction quality.

Overall, the survey was a key component to the public engagement approach, giving residents a safe and healthy avenue to express their concerns and ideas about the future of the neighborhood.



6 APPENDIX

6.2.1. CATALYTIC PROJECT: STATE STREET/ WINCHESTER INTERSECTION



The future success of Fashion Place Mall and the surrounding area hinges on the ability to develop more densely where properties meet State Street (and Winchester Street). To make this future development possible the following regulations should be reviewed and revised:

1. Create new Fashion Place West zone district (FPW) that includes:

- Shared parking provision
- Implementation of parking maximums
- Reduced front yard setback
- Ground Floor activation recommendations

SHORT TERM

- Amend zoning ordinance, rezone properties
- Prioritize residential and office use infill development adjacent to State and Winchester Streets

- Perform streetscape improvements

MEDIUM TERM

- Work with Fashion Place Mall to improve internal pedestrian connectivity and pedestrian access to mall site
- Work with UDOT to improve pedestrian and bicycle experience at Winchester and State Street intersection
- Parking structure at mall

LONG TERM

- Help facilitate increased densities and residential development types within mall property, especially adjacent to State Street and 6400 South.



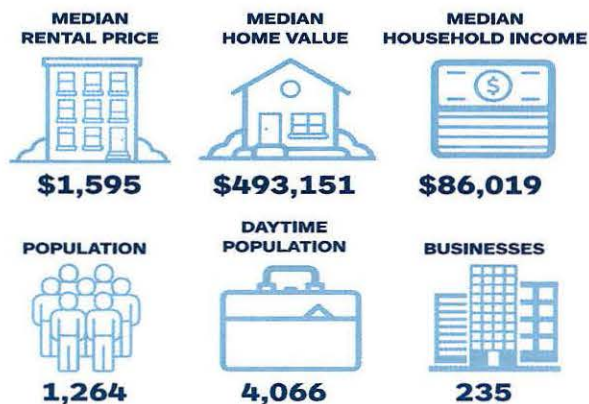
6.2.2. CASE STUDY: BELMAR LAKEWOOD, COLORADO

After more than 15 years of proactive redevelopment efforts by the City of Lakewood and private developers, Belmar is considered to be the new downtown. The total amount of retail area was reduced considerably, but the developers added housing, office, lodging, and healthcare to the mix, to create a mixed-use place.

In 1966, the Villa Italia, a regional mall was built. It was a 104-acre site with 1.2 million square feet of commercial space. The mall closed in 2001 due to increasing competition and changing retail formats. The City of Lakewood began to re-envision how Villa Italia could be renovated or redeveloped.



NEIGHBORHOOD DATA



The plaza in the warmer months is home to festivals and markets.



The Hyatt House Hotel in the Belmar area.

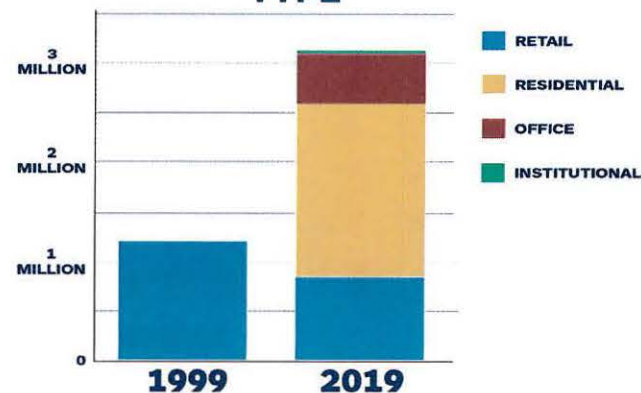


Many of the retail spaces are located on the ground floor, with residential on the upper floors.



Housing is the most abundant new type of development in Belmar.

SQUARE FEET OF DEVELOPMENT BY TYPE

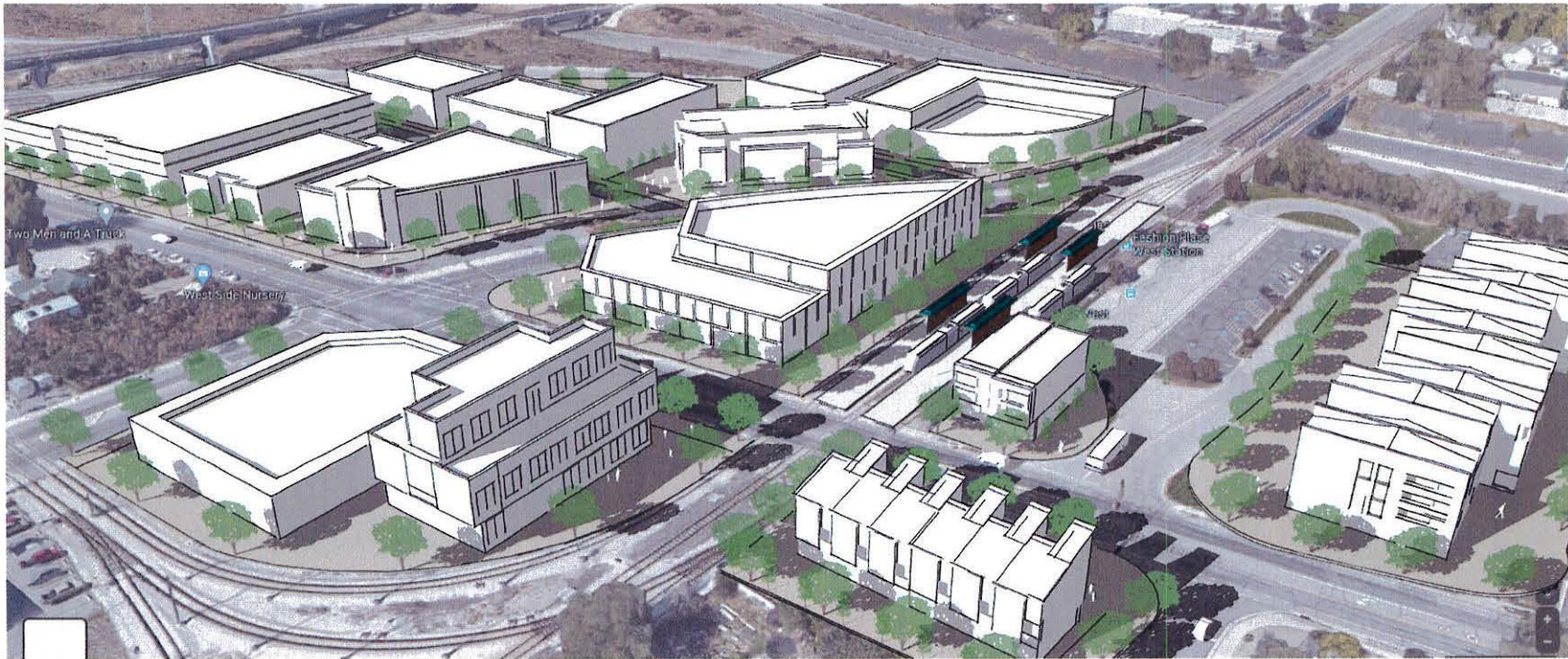


The largest land use after redevelopment of the Belmar area is residential, with a reduction in the overall amount of retail square footage.



6 APPENDIX

6.2.3. CATALYTIC PROJECT: TRAX STATION AREA REDEVELOPMENT



The area around the Fashion Place West TRAX station is ripe for redevelopment over the next 20 years.

In order to encourage this increase in density of uses such as commercial, residential, and office, the area must be rezoned to decrease parking requirements and increase density allowances.

SHORT TERM

- Amend zoning ordinance, rezone properties
- Improve access from Cottonwood Street to TRAX station with Cottonwood bridge reconstruction
- Prioritize residential infill development adjacent to TRAX station

- Perform streetscape improvements
- Improve UTA bus circulation and frequency with Route 209.

MEDIUM TERM

- Help facilitate increased densities that includes residential and office uses

LONG TERM

- UTA Parking structure
- Help facilitate property transition of existing industrial properties on west side of study area.



6.2.4. CASE STUDY: MEADOWBROOK 188 WEST 3900 SOUTH, SOUTH SALT LAKE

The Meadowbrook station is located in an older industrial area near the center of the Salt Lake Valley. Upon the construction of TRAX, the surrounding parcels were primarily industrial and underutilized parcels. Some office space, Harmony Park, and single-family homes inhabited the area, as well.

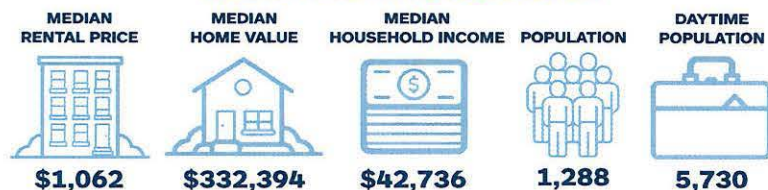
Once the Meadowbrook station was built, the surrounding community leveraged Envision Utah and the Wasatch Choice 2040 toolkit for future development around the station. South Salt Lake and Salt Lake County have employed a Form Based Code and other policies to remove barriers and encourage the kind of growth the community envisions.



TIME TO GET TO... via TRAX



NEIGHBORHOOD DATA



Waverly Station Townhomes and Condominiums, built in 2007.
Plymouth Towns Townhomes, built in 2012.



Construction of the Hub of Opportunity, expected completion in 2020. The Hub will have a total of 156 residential units. 110 income restricted, 46 units market-rate. Another 16 units will be reserved as live/work units as part of Columbus' NextWork Hub that will provide vocational training space to young adults with autism to help them transition to community living and employment.

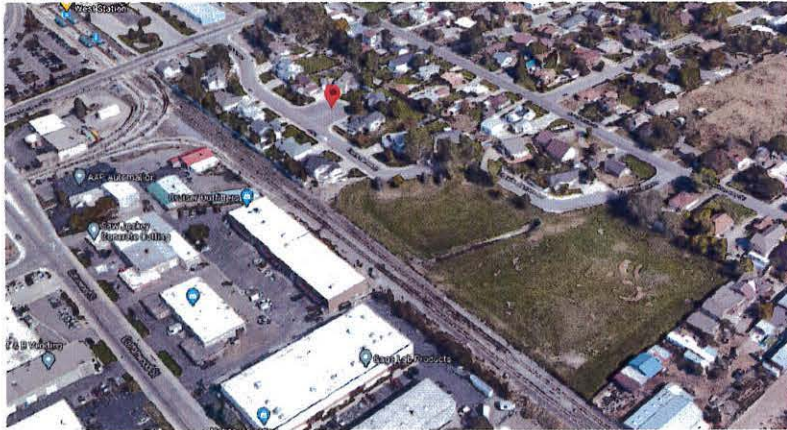


A rendering of the new Hub of Opportunity, located on the corner of 3900 S. West Temple. The Hub is an innovative project and is a community-based, mixed-use development that will bring together a unique combination of community services, workforce development opportunities, and community living for individuals with disabilities.



6 APPENDIX

6.2.5. CATALYTIC PROJECT: JEFFERSON PARK



The detention basin on Travis James Lane is a significant opportunity to provide the neighborhood with a unique green space, but also play a role in modernizing elements of the Salt Lake County Flood Control network.

The Jefferson Detention Basin is a large, undeveloped green space that is dry for most of the year. Hillside amenities and facilities that can withstand water when flooded will substantially improve recreation opportunities for nearby neighbors.



The property on Travis James Lane is owned and maintained by Murray City but is under the jurisdiction and control of Salt Lake County as part of their Flood Control Master Plan. Any changes to this property would require permitting through Salt Lake County.



Rendering of potential future improvements to the Jefferson Detention Basin including a looping pedestrian/tricycle path, climbing rocks, a turf play field, and off-leash dog area. Native plantings could naturalize part of the basin to create a green oasis.

FUTURE PLANS

When Murray City adopted its Parks and Recreation Master Plan in April of 2020, the Jefferson Detention Basin park project was identified as an upcoming project. Development opportunities include creating a terraced hill, with seating at the top and a hill slide or hill climber extending down into the basin.

- Jefferson Detention Basin Development: 3-5 years
- Developed Acres Added: 4.3
- Project Description: Add nature play elements, walking paths, and family gathering space to meet the needs of nearby neighbors.
- Project Type: Park Enhancement/ Expansion
- Estimated Capital Cost: \$500,000
- Standard level annual maintenance cost: \$35,000



6.2.6. CASE STUDY: TANNER SPRINGS PARK PORTLAND, OREGON

Tanner Springs Park was designed in 2003 by Atelier Dreiseitl (Germany), Green Works, P.C. (Portland), Portland Parks and Recreation, Portland Development Commission, and a project steering committee of public and private stakeholders.

A series of community workshops were held between January and June 2003 and the park was named Tanner Springs Park in April 2005. The goal was to transform contaminated city blocks (.92 acres) into a healthy urban green space for contemplation and connecting with nature.

A recirculating bioswale was designed to reference the historical wetlands of Couch Lake and Tanner Creek from the years prior to industrial development. The artistic and synergistic design incorporated sustainability and historical reclamation to make the project unique to Portland and give a strong sense of place.



Sustainability: The impervious surfaces of the urban environment produce excessive precipitation runoff with pollutants and heavy metals. To mitigate this, the park collects stormwater from the sidewalks and streets surrounding it. The park is a large bioswale designed to absorb this runoff. It is a closed system so no pollutants enter the storm water system.



Maintenance: Portland Parks and Recreation maintains the park with a focus on sustainability with an adaptive management approach. Friends of Tanner Springs Park is a community group that grew out of the need for more community support for park maintenance and use. They collaborate with Portland Park and Recreation.



Historical Reclamation: Located in the Willamette Valley, the park was designed to echo the habitat that existed prior to settlement that is now endangered oak savanna and upland prairie. The naturally sloping characteristics of the park mimic the sloping of the Willamette Valley foothills.



6.2.7. CATALYTIC PROJECT: WINCHESTER BRIDGE



Future streetscape improvements along the Winchester Bridge would enhance the pedestrian experience and encourage use of more active transportation methods.

Current conditions on the Winchester Street bridge create a disconnect between the TRAX station and Fashion Place Mall for pedestrians and bicyclists. Improving this bridge in two separate phases would improve overall connectivity as well as access to the TRAX station.

FUTURE PLANS

UDOT and Murray City have determined that the reconstruction of the Cottonwood and Winchester Street bridges are scheduled to occur by 2034.

1. Phase I could include the reconstruction of the current bridge including improved pedestrian and bicycle access, sidewalks, planted park strip, and painted bike lanes.
2. Phase II could entail the construction of an adjacent bridge on the west side of the current bridge. This second bridge could link the two neighborhoods with buildings and storefronts on a single level—creating a pedestrian and bicycle friendly experience across the bridge, and to the TRAX station.



6.2.8. CASE STUDY: RIALTO BRIDGE

COLUMBUS, OHIO

BACKGROUND

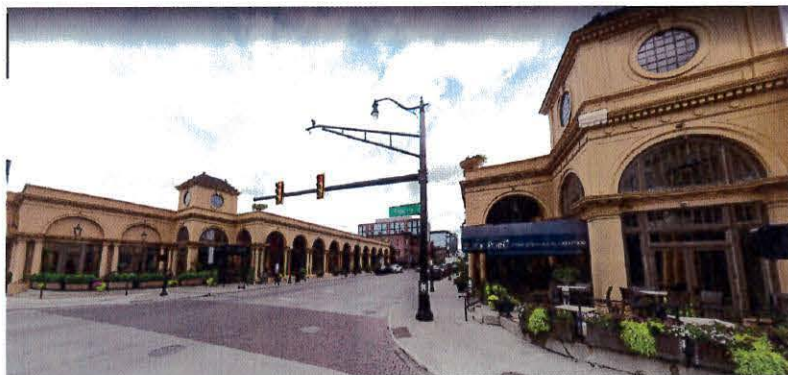
The Cap at Union Station in Columbus, Ohio demonstrates how governments can partner with the private sector to create and share value in highway-related investments.

Before the construction of The Cap at Union Station Project, a 200 foot-long, chainlink-fence bordered walkway spanned the busy highway below, creating a no man's land.

To heal the scar created by the interstate, the solution was be to build a hard cap over the expressway. The objective of the Cap was to create pedestrian and retail space. A local developer, approached the City and expressed interest in investing in the Project. The company signed a memorandum of understanding with the City in 1999 to jointly develop a cap.

Under the terms of the Memorandum between the developer and the City:

- The City would pursue clear title to the air rights above the highway and obtained permission from Ohio Department of Transportation (ODOT) and the Federal Highway Administration (FHWA) to construct the Cap



platforms

- Once the above was achieved, the developer would enter a ground lease for the platforms and construct the buildings.
- Developer would reimburse the City for up to \$75,000 in architectural fees for work that was necessary prior to construction of the buildings on the Cap.

The Project was composed of three separate bridges: one for through-traffic across the highway, and one on either side for the retail structures. Construction of the Cap structures began in 2002, with the developer beginning work on the buildings in April 2003.

REGULATORY HURDLES

AIR RIGHTS

Gaining air rights over the development proved to be a hurdle. When the original interstate was constructed, the state acquired only ground rights. The process required two years to find the owners of the air rights and for the City to procure clear title to the Project site.

PERMITS FROM FHWA

The FHWA places restrictions on use of highway easements for commercial use. It requires that in order for an easement to be granted, fair market rent must be charged to the developer for use of the Cap platforms. This proved challenging for several reasons. Ultimately, the City was able to negotiate an alternative arrangement whereby the City would share in 10 percent of the ongoing profits of the development in lieu of paying rent (the platforms were leased to the developer for a nominal \$1 per year).

MARKET CONSIDERATIONS

Key to the economic viability of the Project was the developer's ability to secure long-term, above market leases for the new buildings. In advance of securing financing, the developer secured tenants willing to pay rents that were approximately 20 to 30 percent higher than those in the surrounding



6 APPENDIX

6.2.8. CASE STUDY: RIALTO BRIDGE COLUMBUS, OHIO

area. The higher rents were enabled because tenants valued the cachet of the new location, and proximity to nearby attractions. The developer also took care to ensure a mix of day and night tenants to keep the space as active as possible. The space currently features a wine bar, a clothing store, an apparel and gift shop, and a few smaller specialty food stores.

KEY PARTNERSHIPS

CITY-DEVELOPER

The City worked with the Developer on the difficult task of extending utilities to the Project across a bridge.

FHWA-CITY

Since the FHWA funded the original construction of the expressway, the alternative use of the highway easement required FHWA approval and buy-in.

ODOT-CITY

Similarly, since ODOT would be operating the highway, all of the design elements of the Project required close coordination with and sign off from ODOT.

FUNDING DESIGN

The City spent \$115,000 on the preliminary design needed to secure the necessary regulatory approvals. The developer reimbursed the City \$75,000 of this cost.



CONSTRUCTION OF THE CAP AND BRIDGES

ODOT agreed to pay \$1.3 million for the construction of the three bridges. The City paid an additional \$325,000 required to extend utilities to the platform via the concrete bay.

CONSTRUCTION OF THE RETAIL BUILDING

The developer assumed the entire cost of the improvements on top of the cap. To finance the construction, the developer originally used conventional loan options and an equity contribution for the \$7 million dollar price tag. The developer also received a ten-year, 100 percent tax abatement on the property for the City, improving the Projects' economics.

TAKEWAYS

- The Project shows an innovative partnership between a private developer, a City, a state DOT and FHWA to support urban development.
- The project demonstrates how Interstate widening projects can contribute to urban renewal with limited incremental cost to government.



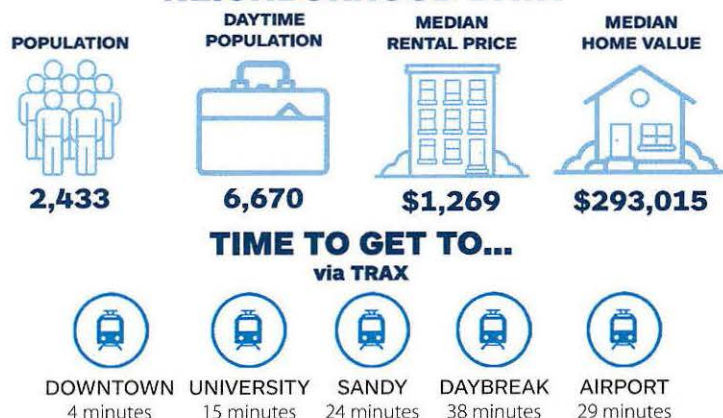
6.2.9. CASE STUDY: CENTRAL NINTH 850 S 200 W, SALT LAKE CITY

The Central Ninth area has a robust, and growing, neighborhood business district on 900 South and is surrounded by an eclectic mix of multi-family developments, single-family homes, and a growing number of small-scale commercial and office buildings. The highlighted building footprints are the newest developments in the area.

Before the construction of the 900 South TRAX station, the neighborhood was mainly industrial with a cluster of single-family homes. According to the Census, there were only 463 housing units within 1/2 mile of the future station in 2000. By 2010, the housing units had more than doubled (1,093 units). The TRAX station was built in 2005.



NEIGHBORHOOD DATA



The Market Building, located on 900 South and in between West Temple and 200 West. The building is home to a market,



restaurants, and a bar. Next door to the Market Building is a smaller coffee development with office space and a coffee shop.



The Alinea Lofts townhomes were built in 2018. The development includes some groundfloor commercial space on 900 South.



The Jefferson Walkway development (above) includes six cottage-like townhomes and public pedestrian walkway that connects two streets.



Above is a rendering of the new Spy Hop Youth Media Arts Center, located on the corner of 900 South and 200 West.



MURRAY
CITY COUNCIL

Public Hearing #2



MURRAY


Power Department

Electric Vehicle (EV) Charger Rate Discussion

Council Action Request

Council Meeting

Meeting Date: February 16, 2021

Department Director Blaine Haacke	Purpose of Proposal Discussion of proposed Electric Vehicle (EV) charger rate
Phone # 801-264-2715	Action Requested Consideration of ordinance adopting Electric Vehicle (EV) charger rate.
Presenters Blaine Haacke Matt Youngs	Attachments Summary of rate proposal and ordinance
	Budget Impact A new rate would be adopted for use of public access to Electric Vehicle (EV) chargers. The revenue amount realized is not known. Rate is not meant to be a huge revenue source.
Required Time for Presentation 20 Minutes	Description of this Item Three EV charger units have been installed at The Park Center for public access. Each unit has two charger "cords". The city was awarded \$157,000 from a VW settlement and the Power Department opted to use the award to install the chargers. A use rate has to be implemented before we deem the chargers operational. Our discussion will review the history of this project and will also propose a rate that has been examined by a third-party consultant. The Attorney's office and Mayor's office have also been involved with this rate formation.
Is This Time Sensitive no	
Mayor's Approval 	
Date February 3, 2021	

Murray City Power Public Electric Vehicle (EV) Charging Rate Proposal

- Volkswagen Settlement – The State of Utah is beneficiary of over \$35 million from the Volkswagen (VW) Environmental Mitigation Trust, part of a settlement with VW for violations of the Clean Air Act. The Governor designated the Utah Department of Environmental Quality (DEQ) as the lead agency to administer this funding, including the development of an Environmental Mitigation Plan. Utah is funding through the settlement to reduce the excess nitrogen oxide (NOx) emissions from the VW, Audi, and Porsche vehicles that were not in compliance with the Clean Air Act.
 - Approximately 7,000 vehicles in Utah were affected by the emissions cheat device on VW vehicles.
 - Utah's total allocation from the settlement is \$35,177,506
 - Majority of these funds are allocated to reducing NOx emissions from Class 4-8 local freight trucks, and school, shuttle, and transit buses.
 - 11% of funds allocated for light duty zero-emissions-vehicle supply equipment (EVSE)
 - Targets government-owned facilities, providing double benefits to taxpayers
 - Prioritizes facilities in nonattainment areas, near major transportation corridors, and allows public access.
- VW Settlement Application - Murray applied for VW settlement funding and was awarded funding up to \$157,608.24 in December 2019 to install one (1) DC Fast Charger and two (2) Level 2 Chargers at the Park Center in Murray. When the project is complete, we will submit paperwork for reimbursement from the State.
 - The application required that Murray follow the City's competitive bid process to select a vendor for the project. Murray solicited bids from five vendors on the State's approved vendor list and the bid was awarded to LilyPad EV out of Kansas City Missouri.
 - ChargePoint, the manufacturer of the installed EV chargers, was chosen by the State of Utah through a selection process to be used in VW EVSC projects.
- EV Charging Proposed Rates / Cost-of-Service Study
 - Power Department contracted with Dave Berg Consulting to:
 - Use our latest cost-of-service study (compiled by Dave Berg Consulting) to determine recommended EV rates for L2 and DC fast chargers, and
 - Provide justification for the recommended EV rates
 - Dave Berg Consulting Recommended EV Rate Letter: Proposed EV charging fees were derived using data from Murray's latest cost-of-service study. This study details the actual cost of serving a customer with electricity based on power supply, transmission, distribution, and administrative costs. In addition to the cost-of-service data, the proposed fees also take into consideration ChargePoint fees and operation and maintenance of the EV chargers.
 - Rates
 - \$0.20 / kilowatt hour – Level 2 Charger (two Level 2 Chargers at Park Center)
 - \$0.30 / kilowatt hour – DC Fast Charger (one DC Fast Charger at Park Center)
 - Fees / Penalties
 - 4 hour parking limit
 - \$10 fee for violation of 4 hour limit – assessed at hour 5

- EV users make payment via ChargePoint account / App
 - Similar to parking payment Apps
 - ChargePoint administrative fee is 10% of all rates / fees collected
- Revenue
 - Murray's VW Award includes cost of EV chargers and a 5 year maintenance / warranty for equipment
 - Propose that rate / fee revenue go the Power Fund, towards cost of power, system, and future EV charger maintenance and infrastructure.
- Local EV Public Charging Rates
 - The proposed rate also takes into consideration the variety of public EV charging rates in Salt Lake County. Some cities give power away for free for a limited amount of time and some charge a flat dollar amount per hour. Some charge a connection fee in addition to a kilowatt hour rate.
 - Murray's proposed EV rate is designed so that the cost of EV charging is borne by the EV customers and does not create a subsidy for all rate payers.
- Sample charging costs and battery % for a 1-hour charging session at proposed rates:
 - Level 2 Chargers (\$0.20/kilowatt hour)
 - Chevy Bolt - \$1.48, 11% charge
 - Tesla Model S - \$2.17, 9% charge
 - DC Fast Charger (\$0.30/kilowatt hour)
 - Chevy Bolt – \$13.33, 58% charge
 - Tesla Model S – \$13.33, 38% charge

Salt Lake County Public EV Charging Rate Comparison

- **Draper:** Level 2 chargers. \$1/hour for first 4 hours. \$2/hour thereafter. \$20 maximum.
- **Sandy:** Level 2 chargers: \$0.20/kilowatt hour. DC Fast Chargers: \$0.30/kilowatt hour. Parking fee: free for two hours, \$10.00 thereafter.
- **Salt Lake City:** Level 2 chargers: free with two-hour parking/charging limit. DC fast charger: \$1.00 connection fee and \$0.21/kilowatt hour. \$75 fee for parking limit violation.
- **West Jordan:** Level 2 chargers: free. DC fast chargers: \$1.50 connection fee, \$0.20/kilowatt hour. \$5.00/hour after two hours.
- **West Valley:** Level 2 chargers: free for 4 hours. \$3.00/hour thereafter.

ORDINANCE NO. _____

AN ORDINANCE ENACTING SECTION 15.20.145 OF THE MURRAY CITY MUNICIPAL CODE RELATING TO THE ESTABLISHMENT OF PUBLIC ELECTRIC VEHICLE CHARGING STATIONS AND RATES AND AMENDING SECTION 10.08.020 OF THE MURRAY CITY MUNICIPAL CODE RELATING TO THE REGULATION OF PARKING AT AN ELECTRIC VEHICLE CHARGING STATION.

Section 1. Purpose. The purpose of this ordinance is to enact Section 15.20.145 and amend Section 10.08.020 of the Murray City Municipal Code relating to the establishment of Public Electric Vehicle Charging Stations and Rates and the regulation of parking at an Electric Vehicle Charging Station.

Section 2. Enactment of Section 15.20.145 of the Murray City Municipal Code. Section 15.20.145 of the Murray City Municipal Code relating to the establishment of Public Electric Vehicle Charging Stations and Rates shall be enacted to read as follows:

15.20.145 PUBLIC ELECTRIC VEHICLE CHARGING STATIONS AND RATES

A. PURPOSE: To set the policies and procedures for charging station rates relating to the use of public Electric Vehicle ("EV") Charging Station Facilities ("Charging Stations") owned and operated by Murray City Corporation (the "City") and managed by the Murray City Power Department (the "Power Department").

B. TERMS AND RULES FOR CHARGING STATION USE

- 1. RATES ESTABLISHED:** The rates and charges for EV Charging Stations furnished by the City to users and consumers shall be established as set forth in this Chapter.
- 2. FINANCIAL TRANSACTIONS:** All Charging Station financial transactions and payments for vehicles charging at a City Charging Station shall be wholly managed and handled by a third-party vendor as posted at each Charging Station. The third-party vender(s) shall receive a percentage of all fees collected at the individual Charging Stations.

C. CHARGING STATION ACCESS:

- 1.** Charging Stations may be used by any member of the public.

2. Charging Stations located within City Parks shall be unavailable for use during the hours of 11:00 PM to 6:00 AM when the parks are closed for public use.
 3. Vehicles parked at a Charging Station must be connected to the Charging Station and in the process of charging.
- D. CHARGING STATION TIME LIMITS AND FEES:** Charging Stations may have limits on the length of time a Charging Customer ("Customer") may charge an EV in a certain parking space. Each parking space at a Charging Station shall have charging requirements clearly posted indicating the charging time limits for that specific Charging Station. It is the responsibility of the Customer to adhere to the charging requirements and time limits as posted.
1. **FEES:** If the charging and time requirements for a Charging Station are violated, Customers may be subject to a fee (assessed at the time of use) for non-compliance with the posted requirements.
- E. EV CHARGING STATION RATES:** Charging Station rates assessed by the City shall be cost-based and designed to recoup the capital and operating costs of the charging equipment, plus the cost of electricity as determined by the relevant retail rate, plus appropriate taxes and overhead costs. The total rate charged shall include the appropriate electricity charges, operating costs, and any applicable third-party vendor fees. Charging Station rates shall be clearly posted on each Charging Station and shall be broken down in detail.
1. **EV CHARGING STATION RATE SCHEDULE:** Charging Station Customers will be charged per kWh for electricity consumed based on the following rates:

EV Charging Station Rates	
DC Fast Chargers	\$0.30/kWh
Level 2 Chargers	\$0.20/kWh

Section 3. Amendment of Section 10.08.020 of the Murray City Municipal Code.
Section 10.08.020 of the Murray City Municipal Code relating to the regulation of parking at Electric Vehicle Charging Stations shall be amended to read as follows:

10.08.020: PROHIBITED PARKING

...

- F. A person may not park a vehicle at an Electric Vehicle Charging Station:
1. unless the vehicle is connected to the Electric Vehicle Charging Station and in the process of charging; or
 2. beyond the charging time limit requirements posted at the Charging Station.

Section 4. Effective date. This Ordinance shall take effect upon first publication.

PASSED, APPROVED AND ADOPTED by the Murray City Municipal Council on
this ____ day of _____, 2021.

MURRAY CITY MUNICIPAL COUNCIL

Diane Turner, Chair

ATTEST:

Brooke Smith, City Recorder

MAYOR'S ACTION: Approved

DATED this ____ day of _____, 2021.

D. Blair Camp, Mayor

ATTEST:

Brooke Smith, City Recorder

CERTIFICATE OF PUBLICATION

I hereby certify that this Ordinance, or a summary hereof, was published
according to law on the ____ day of _____, 2021.

Brooke Smith, City Recorder

Murray City Corporation

NOTICE OF PUBLIC HEARING

NOTICE IS HEREBY GIVEN that on the 16th day of February 2021, at 6:30 p.m., the Murray City Municipal Council will hold and conduct a public hearing. The purpose of the public hearing is to receive public comment concerning a proposed ordinance enacting electric vehicle ("EV") charging rates at City-owned electric vehicle charging stations.

The public hearing will be held electronically as authorized by Utah Code §52-4-207(4) and by City Council Resolution No. 20-13 adopted March 17, 2020. **No physical meeting location will be available.**

The public may view the hearing via the live stream at www.murraycitylive.com or <https://www.facebook.com/MurrayCityUtah/>.

Public hearing comments may be sent via email sent in advance or during the meeting to city.council@murray.utah.gov. Comments are limited to three minutes. Name and contact information should be included in the email. Emails will be read and become part of the public record.

A copy of the proposed ordinance amendments may be reviewed by interested persons by contacting the Murray City Recorder, Room 113, Murray City Center, Murray, Utah, (801) 264-2662 during normal business hours.

DATED this 4th day of February 2021.

MURRAY CITY CORPORATION

Brooke Smith
City Recorder

DATE OF PUBLICATION: February 7, 2021
PH21-08



MURRAY
CITY COUNCIL

Business Items



MURRAY
CITY COUNCIL

Business Item #1



MURRAY


Public Works/Water Division

Water Leak Abatement Policy

Council Action Request

Council Meeting

Meeting Date: February 16, 2021

Department Director Danny Astill Phone # 801-270-2404 Presenters Danny Astill, Cory Wells Required Time for Presentation Is This Time Sensitive No Mayor's Approval  Date February 3, 2021	Purpose of Proposal Consideration of a Water Leak Abatement Policy Action Requested Ordinance approval Attachments Water Leak Abatement Policy and Ordinance Budget Impact No direct budget impact Description of this Item <p>In 2017 Murray City hired a consultant to help the city comply with the Utah State Legislature (2016 General Session, SB 28), to have a conservation-based water rate structure. After an extensive and lengthy study, the city developed a tiered water rate system that contains five tiers. These tiers have allowed us to meet our financial needs and were set at levels to help encourage conservation-based watering. The tiered rates were set to cover the continuing operation and maintenance of the water system in a financially sound manner.</p> <p>In an effort to fairly handle the misfortune of a Murray City water customer who experiences a leak in their system, we have developed this policy which outlines a methodology to be used when a water leak occurs.</p>
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WATER LEAK ABATEMENT POLICY

Background:

In 2017, the City consulted with a local engineering firm and developed a Tiered Water Rate System ("Tiered System") made up of five (5) tiers which allows the City to encourage conservation-based watering while still meeting the financial requirements necessary to operate the City's water systems. Under the Tiered System, a City water customer ("Customer") is billed based on the volume of water used during a single billing cycle. The volume of water used by a Customer is measured in "units", with a single unit of water equaling 100 cubic feet. Customer water usage per billing cycle under the Tiered System is broken down as follows:

- Tier 1 = 0-8 Units (0-800 cubic feet)
- Tier 2 = 9-25 Units (900-2,500 cubic feet)
- Tier 3 = 26-49 Units (2,600-4,900 cubic feet)
- Tier 4 = 50-79 Units (5,000-7,900 cubic feet)
- Tier 5 = 80+ Units (8,000+ cubic feet)

Purpose:

The purpose of this Water Leak Abatement Policy ("Policy") is to establish procedures to be followed in the event that a Customer experiences a water leak on their property.

Policy:

1. **Leak Repair, Abatement and Payments:** The repair of leaks and service of plumbing on a Customer's side of the service connection to the City's water system is the responsibility of the Customer. Any water lost through a leak or open valve on the Customer's side of the service connection shall be paid for by the Customer. To the extent possible, payments for lost water shall be at the rates of the prevailing Tier of the Customer's normal water usage. However, no payments under this Policy for lost water shall be at a rate lower than Tier 3. The City may attempt to notify a Customer if a leak is suspected, but absence of notice from the City does not excuse a Customer of any obligation to be aware of a leak or to pay for the lost water.

- a. **Customer Responsibilities:**

- i. Section 13.08.010 of the Murray City Municipal Code (the "City Code") requires all Customers to "keep their service pipes, connections, and other apparatus in good repair and protected from frost at their own expense." In addition, Section 13.08.120 of the City Code requires Customers to remedy any leaks or to address other wasteful uses of City water once they are discovered.
- ii. Once a Customer is aware of a leak or a wasteful use, they must immediately take the appropriate actions necessary to adequately address and repair the problem.

b. City Responsibilities:

- i. The City shall verify that any leak or wasteful use has been adequately repaired by the Customer. Once the City has determined that the Customer has acted appropriately, the City will charge the Customer as outlined in this Policy for the total amount of water lost as a result of the leak or wasteful use.

2. **Customer Bill Adjustment:** A Customer may request an adjustment to their water bill from the City for water lost because of a leak using the form provided. The City, at its discretion, may adjust the Customer's bill by charging a rate determined by the Public Works Director or Designee contained within the Tiered System. This rate shall consider the cost associated with providing water (i.e. Pumping, Treatment, Storage, Transporting, Delivery, Monitoring, Repair, Replacement, Billing and Customer Service activities). Any approved adjustments will only be considered for amounts over and above a Customer's normal water usage during that billing period and will not be considered for more than two (2) billing periods.

The City shall not consider any adjustments to a Customer's bill until the Customer has presented sufficient proof to the City that the leak has been fully repaired.

a. Customer Requirements for Adjustment Eligibility:

- i. Requests for adjustments must be made within two (2) billing periods after the leak is repaired.
- ii. Adjustments may be available for leaks that are concealed or hidden from view or detection due to landscaping, concrete, structures or a leak inside the cavity of a wall.
- iii. Evidence must be provided to the satisfaction of the Public Works Department that the leak has been repaired (i.e. repair bills, invoice from a plumbing company, or receipts if self-repaired).
- iv. Customers will only be granted one (1) adjustment in a rolling 24-month period.
- v. Granting of an adjustment is at the sole discretion of the City.

Policy Adoption: The Murray City Water Leak Abatement Policy is hereby approved and adopted and shall be incorporated into the Public Works Department, Water Division policies.

Effective Date: This Murray City Water Leak Abatement Policy shall be effective immediately.

[Signature Page to Follow]

DATED this ____ day of _____, 2020.

D. Blair Camp, Mayor

ATTEST:

City Recorder

APPROVED AS TO CONTENT:

APPROVED AS TO CONTENT:

Danny Astill, Public Works Director

Brenda Moore, Finance Director

APPROVED AS TO FORM:

G.L. Critchfield, City Attorney

History.

- Adoption Date: _____, 2020

BILL ADJUSTMENT REQUEST FORM

COMPLETE THIS FORM TO REQUEST A BILL ADJUSTMENT FOR A LEAK

REQUIREMENTS FOR ELIGIBILITY

- Requests for adjustments must be made within two (2) billing periods after the leak is repaired.
- Adjustments may be available for leaks that are concealed or hidden from view or detection due to landscaping, concrete, structures or a leak inside the cavity of a wall.
- Evidence must be provided to the satisfaction of the Public Works Department that the leak has been repaired (i.e. repair bills, invoice from a plumbing company, or receipts if self-repaired).
- Customers will only be granted one (1) adjustments in a rolling 24-month period.
- Granting of an adjustment is at the sole discretion of the City.

CUSTOMER INFORMATION

Name _____

Account Number _____

Phone Number _____ Email _____

Service Address _____

City _____ State _____ Zip _____

LEAK AND REPAIR DETAILS

Date Leak First Noticed _____

Date Repaired _____

Where is the leak? ☐ Underground

☐ Under or within concrete

☐ Beneath a building

☐ Within a wall or cavity

☐ Other _____

Describe the leak and the actions taken to complete the repairs. (attach all receipts)

Customer Signature _____ Date _____

ORDINANCE NO. _____

AN ORDINANCE AMENDING SECTIONS 13.08.110 and 13.08.120 OF THE MURRAY CITY MUNICIPAL CODE RELATING TO CITY WATER USER'S RESPONSIBILITIES FOR SERVICE PIPE REPAIRS, AND TO ALLOW THE MAYOR TO DEVELOP GUIDELINES FOR ADJUSTMENTS TO HIGH WATER BILLS DUE TO WATER USER WASTE.

Section 1. Purpose. The purpose of this ordinance is to amend Sections 13.08.110 and 13.08.120 of the Murray City Municipal Code relating to City Water User's responsibilities for service pipe repairs, and to allow the Mayor to develop guidelines for adjustments to high water bills due to Water User waste.

Section 2. Amendment. Sections 13.08.110 and 13.08.120 of the Murray City Municipal Code shall be amended to read as follows:

13.08.110: SERVICE PIPES TO BE KEPT IN GOOD REPAIR; DIGGING IN STREETS OR SIDEWALKS FOR PURPOSE OF REPAIRING SERVICE PIPE:

~~A. All water users shall keep their service pipes, connections, and other apparatus in good repair and protected from frost at their own expense. No person shall dig into a street or sidewalk for the purpose of laying, removing or repairing any service pipe, unless such person has previously obtained: a) authorization and direction from the Public Works Director or designee, and b) all other required City approvals and necessary City permits.~~ Water users shall be responsible to investigate and monitor higher than expected usage and to make necessary property-side repairs to service pipes, connections and other apparatus at their own expense and to avoid letting water run to waste.

B. No person shall dig into a street or sidewalk for the purpose of laying, removing or repairing any service pipe, unless such person has previously obtained: (1) authorization and direction from the Public Works Director or designee, and (2) all other required City approvals and necessary permits.

13.08.120: WASTING WATER PROHIBITED:

A. It is unlawful for any water user to use water in violation of the rules and regulations promulgated by the City for controlling the water supply, in violation of any provisions of this chapter, or to waste water or allow it to be wasted by:

1. Imperfect stops, valves, leaky joints of pipes;

2. Allowing tanks or watering troughs to leak or overflow;
3. Wastefully running water from hydrants, faucets stops, basins, water closets, urinals, sinks or other apparatus; or
4. Using the water for purposes other than those for which the person has paid.

B. A violation of any of the provisions of subsection A of this section is a Class B misdemeanor.

C. The pressurized irrigation of landscapes between the hours of ten o'clock (10:00) A.M. and six o'clock (6:00) P.M. is prohibited. A violation of this provision is an infraction. (Ord. 18-31)

D. Adjustments. To help mitigate the financial burden of a significant water leak, the Mayor shall develop policies and guidelines to allow for possible adjustments to a water user's bill if, once the water user became aware of the leak or wasteful use on their property, they acted immediately to take appropriate actions to adequately address and repair the problem.

Section 3. Effective date. This Ordinance shall take effect upon first publication.

PASSED, APPROVED AND ADOPTED by the Murray City Municipal Council on
this ____ day of _____, 2021.

MURRAY CITY MUNICIPAL COUNCIL

Diane Turner, Chair

ATTEST:

Brooke Smith, City Recorder

MAYOR'S ACTION: Approved

DATED this ____ day of _____, 2021.

D. Blair Camp, Mayor

ATTEST:

Brooke Smith, City Recorder

CERTIFICATE OF PUBLICATION

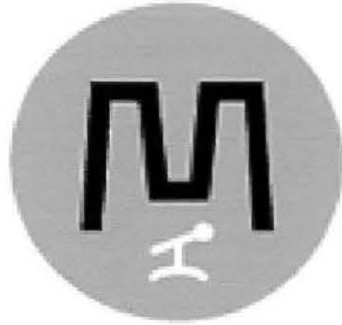
I hereby certify that this Ordinance, or a summary hereof, was published
according to law on the ____ day of _____, 2021.

Brooke Smith, City Recorder



MURRAY
CITY COUNCIL

Mayor's Report And Questions



MURRAY
CITY COUNCIL

Adjournment